



City of San Marcos

630 East Hopkins
San Marcos, TX 78666

Regular Meeting Agenda - Final Planning and Zoning Commission

Tuesday, April 12, 2016

6:00 PM

City Council Chambers

630 E. Hopkins

- I. Call To Order
- II. Roll Call
- III. Chairperson's Opening Remarks
- IV. 30 Minute Citizen Comment Period

CONSENT AGENDA

1. Consider approval of the minutes of the Regular Meeting on March 22, 2016.

PUBLIC HEARINGS

2. A-16-01 (Patton Street Abandonment) Hold a public hearing and consider a request by Joseph Sullivan, on behalf of Saurav Raj Pandey, to abandon an undeveloped portion of Patton Street. (W. Parrish)
3. SCW-16-01 (Advanced Auto Parts) Hold a public hearing and consider a request by Hartzog Holdings LLC, for a SmartCode Warrant to allow a frontage buildout width of less than 80% in a T5 Zoning District. (W. Parrish)
4. ZC-16-02 (1331 Old Ranch Road 12) Hold a public hearing and consider a request by Billie Jo Allen, on behalf of Carol L. Snodgrass and Janet Honig, for a zoning change from "SF-6" Single Family Residential to "MU" Mixed Use for approximately 0.58 acres, out of the T.J. Chambers, Abstract No. 2, located at 1331 Old Ranch Road 12. (A. Brake)
5. ZC-16-03 (202 Posey Road) Hold a public hearing and consider a request by Gunnerson Outdoor Advertising, on behalf of Bobbie Gilbert, for a zoning change from "AR" Agricultural Ranch to "GC" General Commercial for approximately 2.3 acres, more or less, out of the W. H. Van Horn Survey No. 107, Abstract No. 464, located west of IH-35 at 202 Posey Road. (A. Brake)

- 6. PDD-15-02 (Lindsey Hill Mixed Use) Hold a public hearing and consider a request by Guadalupe Re, LLC., for a zone change from "P" Public District to "PDD," Planned Development District with a base zoning of "MU" Mixed-Use, on Block 4, Lindsey and Harvey Addition (4.83 +/- acres). The property is generally located at the northwest corner of W. Hutchison Street and Moore Street. (B. Melland)
- 7. Hold a public hearing, staff presentation and discussion on the 10-Year Capital Improvements Program (FY 2017-2026)

NON-CONSENT AGENDA

V. Question and Answer Session with Press and Public.

VI. Adjournment

Notice of Assistance at the Public Meetings

I certify that the attached notice and agenda of items to be considered by the Planning and Zoning Commission was removed by me from the City Hall bulletin board on the _____ day of _____

_____ Title:



City of San Marcos

630 East Hopkins
San Marcos, TX 78666

Legislation Text

File #: ID#16-181, **Version:** 1

AGENDA CAPTION:

Consider approval of the minutes of the Regular Meeting on March 22, 2016.

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: n/a

Account Number: n/a

Funds Available: n/a

Account Name: n/a

CITY COUNCIL GOAL:

BACKGROUND:



Meeting Minutes
Planning and Zoning Commission

Tuesday, March 22, 2016

6:00 PM

City Council Chambers

630 E. Hopkins

I. Call To Order

With a quorum present the regular meeting of the San Marcos Planning and Zoning Commission was called to order by Chair Garber at 6:00 p.m. on Tuesday, March 22, 2016 in the City Council Chambers of the City of San Marcos, City Hall 630 E. Hopkins, San Marcos, Texas.

II. Roll Call

Present 9 - Commissioner Jim Garber, Commissioner Travis Kelsey, Commissioner Shawn Dupont, Commissioner Saul Gonzales, Commissioner Kate McCarty, Commissioner Douglas Beckett, Commissioner Betseygail Rand, Commissioner Lee Porterfield, and Commissioner Angie Ramirez

III. Chairperson's Opening Remarks

Chair Garber advised the Commission that the applicant of Item #4 PDD-15-02 (Lindsey Hill Mixed Use) has requested that the item be postponed to the April 12, 2016 Planning Commission Meeting.

IV. 30 Minute Citizen Comment Period

Dianne Phalen, resident of San Marcos for 30 years who resides at 721 Burleson for 15 years is opposed to Lindsey Hill project as presented. She said she was primarily against the project due to increase in traffic in the neighborhood. She added that the developers tried to address the concerns of the people but at last night's meeting they said more people will be taking Moore Street but feels that they will take shortcuts through the neighborhood. Ms. Phalen added that she is also concerned about a 300 occupant hotel and apartment building and what it will do to the neighborhood. She feels it will set a bad precedent for other developers. Ms. Phalen said she would like to see condominium or high end apartments instead of the proposed development.

Christine Brown said she is from Southern California. She explained that she chose San Marcos because of its small human scale and beauty and history. She said she has never lived in a place like San Marcos and expressed her excitement. Ms. Brown stated she knows that the city is growing and development has to happen but hopes that the proposed development helps the city retain its human scale and historical feel and look. She provided pre and post development photos of Huntington Beach and gave a brief overview. She explained that the development in the photo is similar to

Lindsey Hill. She asked the Commission if that is what they want contemporary development surrounded by a historic district. Ms. Brown hopes that the Commission will ask the developers to scale back and down and build something that integrates into the community.

Amy Meeks, Belvin Street, said she is not opposed to the project. She understands that development will happen on the Lamar property but has concerns with proposed development. Ms. Meeks said she is concerned about the density and how many things are going to be happening on the land because the property has always been used for educational purposes. She is wondering if it will make a positive or negative impact at that location. Ms. Meeks feels that the best scenario for her is that they stay in conversation with the developers. She added that she has found them to be willing to meet and explain their request. She is glad they pulled the agenda item and are willing to speak to neighbors. She said she hopes that they will consider some compromise with people in the Historic District. Ms. Meeks mentioned that it has been suggested that perhaps there are restrictions on the land and that the restrictions may continue. She hopes she will come to the next Planning Commission meeting with the developers and be able to support the development.

Tina Simek lives in the Lindsey Rogers Neighborhood one block away from the Lindsey Hill Development. She said she is worried about the traffic that will come through their neighborhood. She would like to see the entrance and exit moved off Burluson and on to Hutchison Street which currently has a traffic light and feels it could support the traffic. Ms. Simek is also concerned with the SmartCode parking not being adequate which means the neighborhood will be full of overflow parking from the University. She said that the proposed traffic light on Moore and Burluson is going to be a nightmare. Ms. Simek stated that she does not like the hotel, retail and the thought of all the people going through the neighborhood. She hopes they can keep that type of progress on the other side of Moore Street.

Camille Phillips said she appreciates the fact that the Lindsey Hill project was withdrawn from the agenda. She said that we need a lot of time to figure out what is really going on and digest all the issues.

Carina B. Pinales, 612 W. Hopkins and citizen of San Marcos for 27 years said this development is an opportunity for San Marcos to showcase a pilot option with community engagement with the developer. She said that the developers seem to be open minded and have a vision of a community oriented project. Ms. Pinales pointed out that the developer has been working on the project for 14 months and there were some citizens that heard about it at the joint meeting for the first time. She saw that people were not brought to the conversation that should have been. Ms. Pinales stated a Pilot Program for a vetting process that can be formed to work with the developers. She felt that their was a great voice presence at the meeting and the developers listening so that they can work together. Ms. Pinales feels they can set a standard for future developers to work with the community in a process and make sure all voices are heard.

Kama Davis said she has lived in San Marcos for 42 years and thanked the developers for an open dialogue. She added that she wished they would have been brought into the discussion a lot sooner. Ms. Davis spoke on behalf of her parents, Rev. Carl Brown and Dr. Karen Brown and sister who have owned a home on Hopkins Street for 38 years. She pointed out that they already have a lot of traffic problems. She stressed that the community was built with infrastructure and roads that are meant for small neighborhood use. She pointed out that they are not meant for Industrial Use. Ms. Davis added that Lindsey Hill will bring in hundreds of people as proposed and will use the infrastructure. She mentioned that it will add tax dollars and that the citizens will be paying for changes to the infrastructure to support the hotel, apartments and boutique shops. She said a solution is that developers pay an impact fee before construction begins. She feels that the multi use project is too much, she suggested getting rid of the hotel and add more apartment. Ms. Davis added that the Lindsey Hill Project is going to decrease neighborhood property values. She pointed out that the hotel will bring in new traffic and the Historic District will be glutted with traffic.

Matt Akins, 704 W. Hopkins read a letter he submitted to the Commission. Mr. Akins stated he is in full support of the Lindsey Hill Project.

Danny P. Putegnat resident at 546 Lindsey provided written comments. He asked the Commission to refrain from approving the currently proposed development.

James Baker of 727 Belvin said he appreciates the fact that the area is being developed. He pointed out that the Historic District is not a tourist destination it is a neighborhood. He said they would like to keep it as a neighborhood. Mr. Baker said in his opinion the hotel is going to be a big problem. They are not there to bring people from IH 35 to look at the Historic District. Mr. Baker pointed out that this is an area of stability and when they looked at the Denver model, some areas are area of stability and some areas are successful thriving neighborhoods. He also felt that there are too many things going on for their neighborhood. He said bars are going to be in the area and a precedent will be set. Mr. Baker reiterated that they want to keep their neighborhood a neighborhood. He said he would like to see instead of a hotel, townhouse, condominium or apartments. He said they want neighbors, people that will live in the neighborhood. They do not want transient people and commercial development in their neighborhood. He pointed out that his home has been here since before the University and hope that it will be there for many years after him.

Diane Wassenich said she will come to the Planning Commission meetings over the next few meetings. She told the Commission she will send them maps via email. Ms. Wassenich said she wants to explain her point that we have a clear river. She said other areas do not. She explained that we have a clear river because we have a vast swath of undeveloped land that our river flows from. She said the water is clear because it has vegetation on, not developed or paved. Ms. Wassenich gave a brief overview of a topographical map indicating the flow of water into Spring Lake.

Leanne Smith, 719 Belvin Street, and has lived there since 1992. She said she has

done a lot of improvements on the property and has seen so much come and go in the neighborhood. Ms. Smith stated she is appreciative to developers who seem to be very sensitive to their desires and want to collaborate. She said she agrees with many of her neighbors that the hotel is too much. She mentioned that she is worried about people that don't care about her neighborhood. Ms. Smith said she likes the idea of apartments and as a University Professor feels it would be ideal for young professionals. She added that just because it's possible doesn't mean it's desirable.

Cathy Dillon has a business at 321 W. Hopkins and resident at 1000 Burleson deferred her time to David Hale.

David Hale, spoke regarding the Lindsey Hill topic. He said that the Commission probably realized that they have something very special here. He stated there are not many cities in the state of Texas that have this type of expansive Historic Districts. He said that what they are seeing now in the 21st Century that cities that are seeing the revitalization are the cities that have saved their heritage. He told the Commission that they have a challenge to decide what do they want the future to look like. Mr. Hale continued that the time is now to set a precedent for the future. He spoke in support of saving the Historic District.

CONSENT AGENDA

1. Consider approval of the minutes of the Regular Meeting on March 8, 2016.

A motion was made by Commissioner Ramirez, seconded by Commissioner Kelsey, that the consent agenda be approved. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

PUBLIC HEARINGS

2. CUP-16-07 (Root Cellar Bakery) Hold a public hearing and consider a request by Kyle B. Mylius, on behalf of Root Cellar Bakery, for a new Restricted Conditional Use Permit to allow the sale of beer and wine for on-premise consumption at 142 North LBJ Drive. (A.Villalobos)

Chair Garber opened the public hearing.

Andrea Villalobos, Planning Tech gave an overview of the request.

There were no citizen comments and the public hearing was closed.

A motion was made by Commissioner Kelsey, seconded by Commissioner

Gonzales, that CUP-16-07 (Root Cellar Cafe) be approved with the conditions that the permit shall be valid for one (1) year, provided standards are met, subject to the point system; and the permit shall be posted in the same area and manner as the Certificate of Occupancy. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

3. CUP-16-08 (Provision Spirit Distillery) Hold a public hearing and consider a request by Tracy Borrel, on behalf of Provision Spirit Distillery, for a new Conditional Use Permit to allow the sale of mixed beverages for on-premise consumption at 110 East MLK Drive, Ste. 100. (A.Villalobos)

Chair Garber opened the public hearing.

Andrea Villalobos, Planning Tech gave an overview of the request.

Marsha Millam, 1506 W. 13th Street, Austin, owner and operator of the business introduced herself and stated she is in support of the request. She said she is looking forward to being in San Marcos.

There were no additional citizen comments and the public hearing was closed.

A motion was made by Commissioner Kelsey, seconded by Commissioner McCarty, that CUP-16-08 (Provision Spirit Distillery) be approved with the conditions that the permit shall be valid for one (1) year, provided standards are met, subject to the point system, the permit shall be effective upon the issuance of the Certificate of Occupancy. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

4. **PDD-15-02 (Lindsey Hill Mixed Use)** Hold a public hearing and consider a request by Guadalupe Re, LLC., for a zone change from "P" Public District to "PDD," Planned Development District with a base zoning of "MU" Mixed-Use, on Block 4, Lindsey and Harvey Addition (4.83 +/- acres). The property is generally located at the northwest corner of W. Hutchison Street and Moore Street. (B. Melland)

A motion was made by Commissioner Dupont, seconded by Commissioner Kelsey, that PDD-15-02 (Lindsey Hill Mixed Use) be postponed to the April 12, 2016 Planning and Zoning Commission meeting. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

5. LDC-16-01 Hold a public hearing and consider revisions to Subpart B of the City's Code of Ordinances (Land Development Code) updating Chapters 2, 4, 6, and 8 to clarify which standards apply to the Extraterritorial Jurisdiction, add Senior Housing as a land use, and amend zoning categories which permit Hospitals. (T. Carpenter)

Tory Carpenter, Staff Planner gave an overview of the request.

Chair Garber opened the public hearing.

There were no comments and the public hearing was closed.

A motion was made by Commissioner Ramirez, seconded by Commissioner Dupont that LDC-16-01 be approved as stated.

A motion was made by Commissioner Kelsey, seconded by Commissioner McCarty, that LDC-16-01 be amended to change the definition from 12 to 14 units per acre. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

Chair Garber called for a vote on the motion to approve LDC-16-01 with the amendment that the definition be changed from 12 to 14 units per acre. The motion carried by the following vote:

For: 9 - Commissioner Garber, Commissioner Kelsey, Commissioner Dupont, Commissioner Gonzales, Commissioner McCarty, Commissioner Beckett, Commissioner Rand, Commissioner Porterfield and Commissioner Ramirez

Against: 0

6. Presentation of the 3-year Capital Improvements Program.

Laurie Moyer, Director of Engineering/CIP gave an overview of the 3-year Capital Improvements Project List. She advised the Commission that a public

hearing, additional discussion on the 3 year and a review of the remaining 7 years will be held on the April 12th Planning Commission Meeting. In addition, a second public hearing will be held on the April 26th Planning Commission Meeting as well as discussion and action on recommendations to City Council.

NON-CONSENT AGENDA

7.

Development Services Report:

1. National APA Conference (April 2-5, Phoenix)
2. UT Land Use Conference (April 27-29, Austin)

Shannon Mattingly informed the Commission about the upcoming conferences. She stated that if anyone was interested in attending a conference to contact Planning staff.

V. Question and Answer Session with Press and Public.

There were no citizen comments.

VI. Adjournment

A motion was made that the meeting be adjourned at 7:55 p.m. The motion carried by a unanimous vote.

Jim Garber, Commission Chair

ATTEST:

Francis Serna, Recording Secretary

Notice of Assistance at the Public Meetings

I certify that the attached notice and agenda of items to be considered by the Planning and Zoning Commission was removed by me from the City Hall bulletin board on the _____ day of _____

_____ Title:



Legislation Text

File #: A-16-01, Version: 1

AGENDA CAPTION:

A-16-01 (Patton Street Abandonment) Hold a public hearing and consider a request by Joseph Sullivan, on behalf of Saurav Raj Pandey, to abandon an undeveloped portion of Patton Street. (W. Parrish)

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: NA

Account Number: NA

Funds Available: NA

Account Name: NA

CITY COUNCIL GOAL: Community Wellness/Strengthen the Middle Class

COMPREHENSIVE PLAN ELEMENT(s): NA

BACKGROUND:

The house that is located at 804 Gravel Street was built partially within the Patton Street ROW. Based on the submitted survey, the house extends approximately 3 feet into an undeveloped portion of Patton Street. The applicant is proposing to purchase a portion of the Right of Way so that the house is no longer located within the boundaries of Patton Street. There are existing utilities within this portion of Patton Street and an easement will be provided.

Staff finds the request consistent with the criteria in Section 74.087 (see below) and recommends **approval** of the abandonment of an 8' x 170' portion of the alley at 804 Gravel Street under the following conditions:

1. The applicant provides a Public Utility Easement over the entire portion of the purchased property, with the exception of the portion where the existing home is located;
2. The applicant is responsible for the relocation of all fences that are currently located within the existing Right of Way or future Public Utility Easement;
3. The applicant must plat the property into a legal lot after the purchase.

CoSM1 801
MLK DR



A-16-01
"Railroad"/Patton Street
804 Gravel Road
Map Date: 2/17/2016

 Site Location



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

CoSM1 801
MLK DR



A-16-01
"Railroad"/Patton Street
804 Gravel Road
Map Date: 2/17/2016

 Site Location



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A-16-01

Right of Way Abandonment

804 Gravel



Summary: Abandonment of an 8' by 170' portion of an undeveloped portion of Patton Street.

Applicant: **Joseph Sullivan**
PO Box 40231
Austin, TX 78704

Abutting Property Owners: **Saurav Raj Pandey** **Ruth Mata**
804 Gravel Street 756 Gravel Street
San Marcos, TX 78666 San Marcos, TX 78666

Notification: Personal notifications of the public hearing were mailed on Friday, April 1, 2016.

Property/Area Profile:

Location: An undeveloped 170' portion of Patton Street that dead ends into Dunbar Park.

Size: The applicant is requesting to purchase approximately .03 acres out of the approximately .10 acres of Right of Way.

Existing Utilities: A wastewater line and a storm sewer line exist within the Right of Way.

Existing Use of Property: Residential

Comments from other Departments:

The Public Services Department has stated that access to the wastewater line and the storm sewer line must be maintained. This can be done through a Public Utility Easement. Additionally, City Staff would like to maintain access to Dunbar Park through this right-of-way (ROW).

Background Information:

The house that is located at 804 Gravel Street was built partially within the Patton Street ROW. Based on the submitted survey, the house extends approximately three feet into Patton Street. This house is believed to have been constructed in the 1970s. Currently the ROW is used as a driveway by both 804 and 756 Gravel Street, and there are private fences that extend into the ROW.

804 Gravel Street was recently purchased by Saurav Pandey and is being remodeled by his agent Joseph Sullivan (the applicant). After the purchase, Mr. Sullivan discovered that the house extended into the ROW. Because the home extends into the ROW, the applicant is having difficulty selling the house. The applicant approached City Staff with a request to purchase a portion of the ROW to resolve this issue.

Planning Department Analysis

Typically a request for an abandonment must be for an entire segment (defined as entire width between adjacent intersecting streets) which requires the consent of all property owners abutting the length of the segment. In this case, the alley now dead-ends adjacent to the portion under consideration. The criteria for abandonment states that, “streets and alleys will be abandoned only in whole segments, except that a portion of a dead-end street or alley may be abandoned if the abandonment does not cause a part of the street or alley to become landlocked.”

After reviewing the situation, staff determined that the minimum amount of ROW that would be appropriate to sell the applicant would be 8 feet wide and 170 feet deep. A purchase of 8 feet in width would bring the property into conformance with the house being within the boundary of the lot and a five foot setback from the property line.

However, due to the fact that there are utilities within the ROW, Staff will require that the entire portion of the purchased ROW (with the exception of the house) be located within a Public Utility Easement, to allow for access to the utilities located within the ROW. Additionally, all fences located within the ROW must be removed, as they restrict access to the ROW and public utilities.

Staff finds the request consistent with the criteria in Section 74.087 (see below) and recommends **approval** of the abandonment of an 8’ x 170’ portion of the alley at 804 Gravel Street under the following conditions:

1. **The applicant provides a Public Utility Easement over the entire portion of the purchased property, with the exception of the portion where the existing home is located;**
2. **The applicant is responsible for the relocation of all fences that are currently located within the existing Right of Way or future Public Utility Easement;**
3. **The applicant must plat the property into a legal lot after the purchase.**

Evaluation		Abandonment Standards (Section 74.087 of the San Marcos City Code)
Consistent	Inconsistent	
X		Street and alleys will be abandoned only in whole segments, except that a portion of a dead-end street or alley may be abandoned if the abandonment does not cause a part of the street or alley to become landlocked.
X		A street or alley abandonment will not be approved if it causes substantial interference with access to any tract or parcel of property.
X		A street or alley containing public utility facilities will be abandoned only if the facilities are relocated out of the street or alley or if a public utility easement is recorded covering the area occupied by the facilities. Unless otherwise agreed by the owners of the utilities, the cost of relocating the facilities or preparing survey descriptions for easements will be borne by the owners of the property abutting the segment to be abandoned.
X		A street or alley abandonment will be approved only if the street or alley is not needed for future road or utility improvements.

Planning Department Recommendation:	
<input type="checkbox"/>	Approve as submitted
<input checked="" type="checkbox"/>	Approve with conditions or revisions as noted
<input type="checkbox"/>	Alternative
<input type="checkbox"/>	Denial

Prepared by:

Will Parrish

Planning Technician

March 30, 2016 2013

Name

Title

Date



**Street Abandonment
"Railroad"/Patton Street
804 Gravel Road
Map Date: 3/30/2016**

-  Proposed Purchase
-  Site Location



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

Joey Sullivan
804 Gravel St
San Marcos, TX 78666
JosephRobertSullivan@gmail.com
512.262.9998

1/12/2016

City of San Marcos
630 E. Hopkins
San Marcos, TX 78666

Dear Council Members, Mayor Guerrero and City Manager Miller,

I am writing this email to inquire if there might be any option for the City of San Marcos to abandon or mitigate any fees and costs involved with acquiring a small strip of City-owned land adjacent to our property.

In August of 2015 we purchased the property at 804 Gravel Street, with the intention of bringing it up to city code and remodeling it to create a beautiful and affordable home. We were able to help a distressed homeowner out of a difficult situation. The house was in terrible condition, with holes in the roof and no electricity. We are currently working with the permit office to complete substantial remodeling. We have had our fair share of surprises with this property, the largest being when we had a survey done (after we had already bought it), we found out that a small portion of the house is encroaching on an abandoned strip of city land (aka Rail Road Street which was platted in 1887).

You can find the survey, original plat, pictures and other information here - bit.ly/1OXRLRp

If you look at the pictures, you will notice some show the house all boarded up. The City of San Marcos' Marshal Office was responsible for this due to the fact that the property was vacant, unsecured and drawing in crime & trespassers. Unbeknownst to us when we bought the house that the City of San Marcos had put a \$1,612.52 lien on the house for cleaning up and securing the property (the reason our title company did not catch it was because the city had not filed the lien soon enough).

We are currently using an expensive bridge loan to help with the repair costs. The lender did not require a survey prior to our closing the loan as the legal description of the property did not suggest anything unusual – had there been any concern, they would have required us to get a survey. We are currently paying a **high interest-only loan** on this property and are hoping to refinance out of it into a 30 year Fannie Mae loan. Unfortunately, conventional lenders will not consider refinancing us due to the boundary issue.

Once we discovered the problem on our hands, we contacted the city planners and met with them to find a solution to this predicament. They were kind, very easy to work with and gave us an *Alley and Street Abandonment Application* and told us we needed to pay \$823 in application fees, order a survey

and appraisal of Rail Road Street (we estimate all these costs to be around \$1,700). After we get everything in order, we were told to submit the documents to the city and eventually stand before the city council to determine if we are to have the land platted to us through abandonment or sale.

I met with Mr. Ray Mata (Ruth's husband) next door to 804 Gravel St and adjacent to Rail Road Street. Mr. Mata stated that his wife's parents (or uncle, I do not recall) built both the properties surrounding that strip of land. The houses are believed to have been built in the early 1970's. Apparently, the property owners were the only ones using and maintaining the land for over 40 years. Today the land is used as driveways and/or as additional backyard acreage for both adjacent property owners.

Mr. Mata made clear that he and his family are not in a financial position to make any expenditures on the land (i.e. the filing fee, appraisal, survey and potential cost of the land). Concerning our project at 804 Gravel St, thus far we are north of \$10,000 over our budget in remodel costs.

We have worked tirelessly to bring this property up to its present condition, with remodeling not being limited to the roof and electrical. An appropriate air conditioner was installed. The entire pier-and-beam foundation was completely replaced and elevated to at least 10 inches high to mitigate any unforeseen flooding issues in the future. This is not including all the cosmetic updates, such as trash-haul-away, landscaping, new laminate & tile flooring and fresh interior & exterior two-tone paint.

In order for us to successfully continue and complete this project, we are asking for your help in reducing or eliminating the costs associated in the re-platting of the land and abandonment of the City right-of-way. As it currently stands, both lots of land are non-conforming to current zoning and development regulations. Would it be possible to have a portion or all of the land abandoned and re-platted to the adjacent property owners (which could then start accruing property tax to the city going forward)? We would be extremely grateful and it would solve the conundrum we are in.

I am aware of the City Council's work supporting **Affordable Housing for San Marcos**. Our home is a prime example of taking an abandoned, run down structure and providing a safe, secure home in the community. As you can see from the attached pictures, I believe the neighborhood will be safer and benefit from taking this structure from a nuisance to a real home. Through this project, we have taken concerted efforts in doing our part to make the San Marcos community a better place to live. The neighbors are grateful to us for building up their neighborhood and deterring the drugs, crime and squatters in the Gravel neighborhood. We are grateful for your consideration in this matter. Please do not hesitate to contact me if you have any questions or concerns.

Sincerely,



Joey Sullivan

City of San Marcos

ALLEY AND STREET ABANDONMENT APPLICATION

APPLICANT
 Name: JOSEPH SULLIVAN Phone No. 512.262.9998
 Mailing Address: PO BOX 40231 AUSTIN, TX 78704
If different from applicant:
 Property Owner's Name: SAURAV RAJ PANDEY Phone No. _____
 Address: 804 GRAVEL ST SAN MARCOS, TX 78666

PROPERTY
 Legal Description and Location of Abandonment: **OR A PORTION THEREOF**
 Lot 1 Block 4 Subdivision DAN MCKIE
NOTE: *Streets and alleys will be abandoned only in whole segments (between intersecting streets or terminus).*

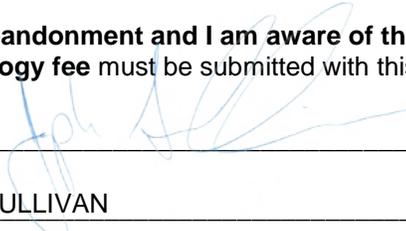
TYPE OF ABANDONMENT

Street: Length: _____ Width: _____
 Appraisal attached
 Check for appraisal attached

Alley: Length: _____ Width: _____
 (No appraisal required)

Documents indicating the consent to the abandonment by the owners of all property abutting the segment to be abandoned must be included with the application.

I understand the process of abandonment and I am aware of the cost and fees associated with this process. \$812 filing fee and \$11 technology fee must be submitted with this application form.

Signature of Applicant:  Date: 11/23/2015
 Printed Name: JOSEPH SULLIVAN

To be completed by Staff:

Meeting Date: _____ Application Deadline: _____
 Accepted by: _____ Date: _____



Legislation Text

File #: SCW-16-01, **Version:** 1

AGENDA CAPTION:

SCW-16-01 (Advanced Auto Parts) Hold a public hearing and consider a request by Hartzog Holdings LLC, for a SmartCode Warrant to allow a frontage buildout width of less than 80% in a T5 Zoning District. (W. Parrish)

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: NA

Account Number: NA

Funds Available: NA

Account Name: NA

CITY COUNCIL GOAL: NA

COMPREHENSIVE PLAN ELEMENT(s): NA

BACKGROUND:

This property is occupied by a recently vacated gas station and is located within the Downtown T5 SmartCode District. The development regulations for this district promote dense, mixed use, and pedestrian oriented development. Staff has worked extensively with the applicant to develop a retail building that meets both the applicant's and the City's requirements. The applicant has agreed to construct a one-story building that is a minimum 25 feet tall from finished floor to finished ceiling, with a shop front frontage and a public entrance facing Guadalupe Street.

However, due to the fact that this property only has frontage on an A-Grid Street, the applicant would like to request a Warrant to the frontage requirement in order to allow better vehicular access to the site. The SmartCode requires that in the T5 Downtown District, the frontage buildout at the front setback be a minimum of 80%. The applicant is proposing a 24 foot wide driveway to allow for two-way ingress and egress to the site, which would make the frontage buildout approximately 68%.

Staff provides this request to the Commission for your consideration and recommends **approval** of the SmartCode Warrant to allow a reduction of the frontage buildout requirement from 80% to 68% to allow for a two-way driveway to access the site, as the property does not have frontage on a B-Grid street or an alley.



SCW-16-01
Frontage Width
510 S Guadalupe St
Map Date: 3/25/2016

-  Site Location
-  Buffer



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.



SCW-16-01
Frontage Width
510 S Guadalupe St
Map Date: 3/25/2016

 Site Location



0 112.5 225 450
 Feet

This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

SCW-16-01 SmartCode Warrant Frontage Build out Requirement 510 South Guadalupe Street



Summary:

Applicant: Hartzog Holdings
506 Lavaca Street, # 1160
Elgin, South Carolina 29045

Property Owner: Hartzog Holdings
506 Lavaca Street, # 1160
Elgin, South Carolina 29045

Applicant Request: *Request for a SmartCode Warrant to seek a deviation from table 5.10 which requires a minimum 80% frontage buildout at the setback in a SmartCode-T5 Zoning District.*

Notification: Public hearing notification mailed April 1, 2016

Response: None as of date of report.

Property Area/Profile:

Location: 510 South Guadalupe Street

Legal Description: Lot 188 J.M. Veramendi Subdivision

Frontage On: South Guadalupe Street

Neighborhood: Downtown

Existing Zoning: T5

Future Land Use Map: Downtown – High Intensity

Existing Utilities: Adequate.

Existing Use of Property: Vacant Gas Station

Zoning and Land Use Pattern:

	Current Zoning	Existing Land Use
N of Property	T5	Retail/House
S of Property	CS	Southside Community Center
E of Property	CS / T5	Church/Retail
W of Property	T5	Vacant Parking Lot

Code Requirements:

A Warrant is a ruling that would permit a practice that is not consistent with a specific provision of the SmartCode, but is justified by the provisions of Section 1.3 Intent. The Warrant process requires personal notification of property owners within 200 feet and a public hearing before the Planning and Zoning Commission.

Frontage Buildout is regulated by the SmartCode within the Principal Building Setback section (Table 5.10). A minimum of 80% frontage buildout is required at the front setback line in a T5 zoning district and a reduction from this requirement requires a Warrant.

Comments from Other Departments:

Warrants are reviewed by the interdepartmental SmartCode Development Review Committee (DRC) which is comprised of members of Planning, Permitting, Building, Main Street, Engineering and Public Services. The DRC reviews and provides comments throughout the review process for building and site design issues.

Background:

This property is occupied by a recently vacated gas station. Southside Community Center occupies the property to the South and Car Quest operates a retail business to the North. The property is located within the Downtown T5 SmartCode District, and the development regulations for this district promote dense, mixed use, and pedestrian oriented development. This property is located on Guadalupe Street, which is identified as an A-Grid Street, which is defined as a street where development along its frontage is held to the highest standards prescribed by this Code. Staff has worked extensively with the applicant to develop a retail building that meets both the applicant's and the City's requirements. The applicant has agreed to construct a one-story building that is a minimum 25 feet tall from finished floor to finished ceiling, with a shop front frontage and a public entrance facing Guadalupe Street. However, due to the fact that this property only has frontage on an A-Grid Street, the applicant would like to request a Warrant to the frontage buildout requirement in order to allow better vehicular access to the site.

Planning Department Analysis:

The City of San Marcos SmartCode was adopted with the understanding that, in general, within urban areas, the form of a building and the environment that it fosters in the public realm is more important than the specific use, while recognizing that there are some outliers (industrial uses for example). Therefore the SmartCode zoning standards are generally more concerned with the location, height, and design of a structure, than the activity contained within.

The SmartCode requires that in the T5 Downtown District, the frontage buildout at the front setback be a minimum of 80% (table 5.10). This means that a building must be constructed along 80% of the width of the property along a street frontage. The property in question is 132 feet wide, and the proposed building is 90 feet wide. This would make the frontage buildout approximately 68%.

The T5 district requires that when a property has frontage on an alley or a B-Grid Street, access must be taken from either the alley or the B-Grid Street, rather than an A-Grid Street. This property however, only has access to the Guadalupe, which is an A-Grid Street. As the A-Grid Street is the only street from which access can be taken, the applicant is proposing a 24 foot wide driveway to allow for two way ingress and egress to the site. In addition to the 24 foot driveway, the applicant is proposing a 4 foot sidewalk between the driveway and the building to allow access from the street to the parking lot. The applicant is also setting the proposed driveway 10 feet away from the Southside Community Center driveway to provide clear delineation between the two driveways.

The proposed improvements are largely consistent with some of the Intent policies at the Regional, Community, and Transect levels listed below from Section 1.3.

1.3.2 Region

b. That growth strategies should encourage Infill and redevelopment in parity with New Communities.

1.3.3. The Community

a. That neighborhoods and Regional Centers should be compact, pedestrian-oriented and Mixed Use.
b. That neighborhoods and Regional Centers should be the preferred pattern of development and that Districts specializing in a single use should be the exception.

- c. That ordinary activities of daily living should occur within walking distance of most dwellings, allowing independence to those who do not drive.
- f. That appropriate building densities and land uses should be provided within walking distance of transit stops.
- g. That Civic, Institutional and Commercial activities should be embedded downtown, not isolated in remote single-use complexes.

1.3.5. The Transect

b. Shops mixed with townhouses, larger apartment houses, offices, workplace, and Civic buildings; predominantly attached buildings; trees within the public right-of-way; substantial pedestrian activity.

1.3.4. The Block and the Building

- a. That buildings and landscaping should contribute to the physical definition of Thoroughfares as Civic places.
- b. That development should adequately accommodate automobiles while respecting the pedestrian and the spatial form of public areas.
- c. That architecture and landscape design should grow from local climate, topography, history, and building practice.

Staff provides this request to the Commission for your consideration and recommends **approval** of the SmartCode Warrant to allow a reduction of the frontage buildout requirement from 80% to 68% to allow for a two-way driveway to access the site, as the property does not have frontage on a B-Grid street or an alley.

Planning Department Recommendation:	
X	Approve as submitted
	Denial
	Approve alternate recommendation

The Commission's Responsibility:

The Commission is required by law to hold a public hearing and receive public comment on this application. After considering the public input, the Commission is charged with making a decision to approve or deny the Warrant pursuant to regulations established with the exceptions of requests for additional height.

The Commission's decision is discretionary. In evaluating the impact of the proposed Warrant on surrounding properties, the Commission should consider the extent to which the practice:

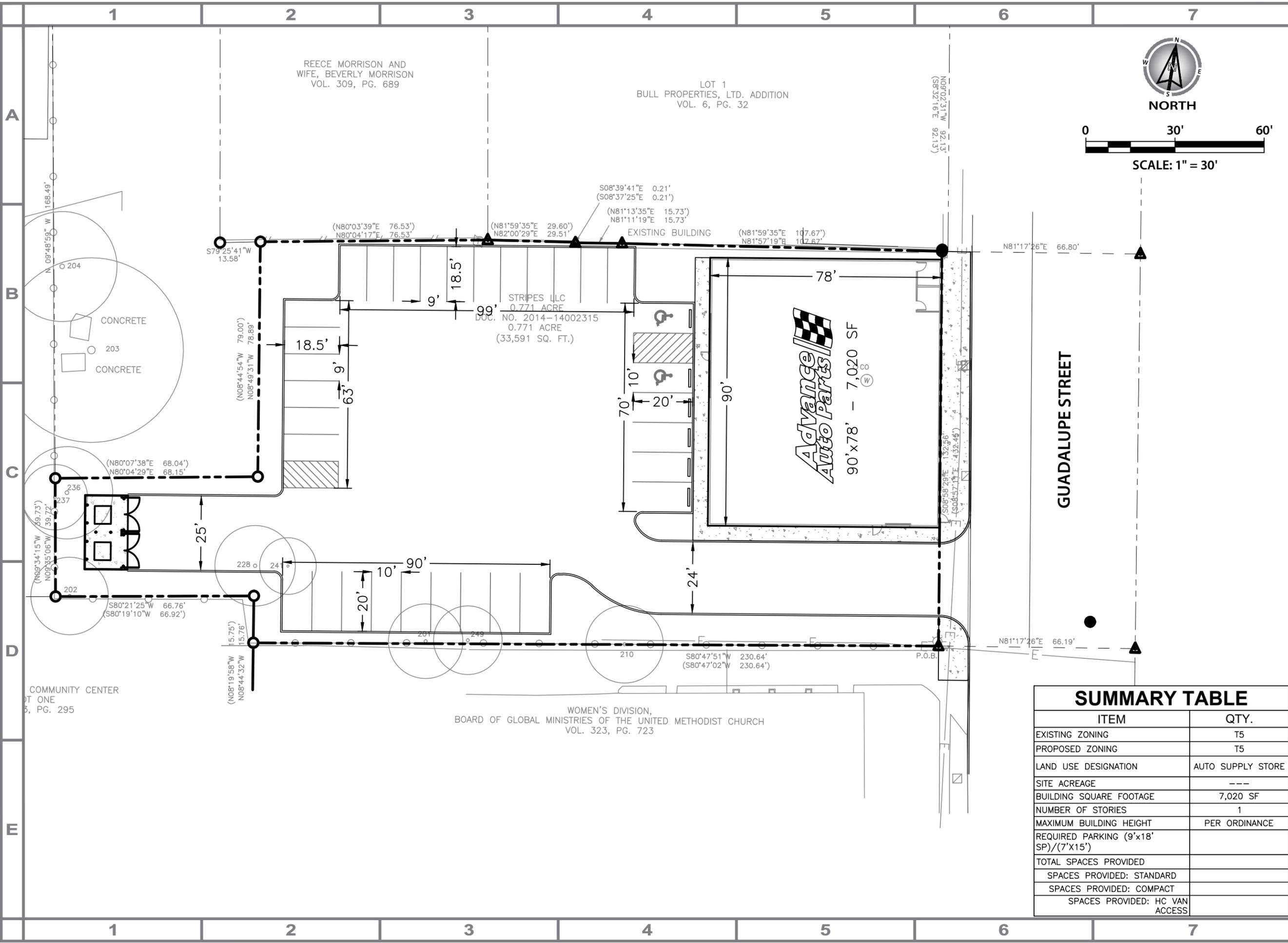
- enables, encourages and qualifies the implementation of the SmartCode policies on Intent;
- is consistent with policies of the Comprehensive Plan and Downtown Master Plan;
- is compatible with the character and integrity of adjacent developments and the general intent of the Transect.

The following standards are not available for Warrants:

- a. the maximum dimensions for traffic lanes;
- b. the required provision of Rear Alleys; and
- c. the Base Residential Densities.

Will Parrish	Planning Technician	March 29, 2016
Name	Title	Date

FILE: K:\PROJECTS\259 - ADVANCE AUTO PARTS\008 - SAN MARCOS\CAD\EXHIBITS\CONCEPTUAL PLAN #13 - AAP SM 2016.3.29.DWG PLOTTED BY: RUSH YANN PLOTTED ON: 03/29/16 3:32:58 PM



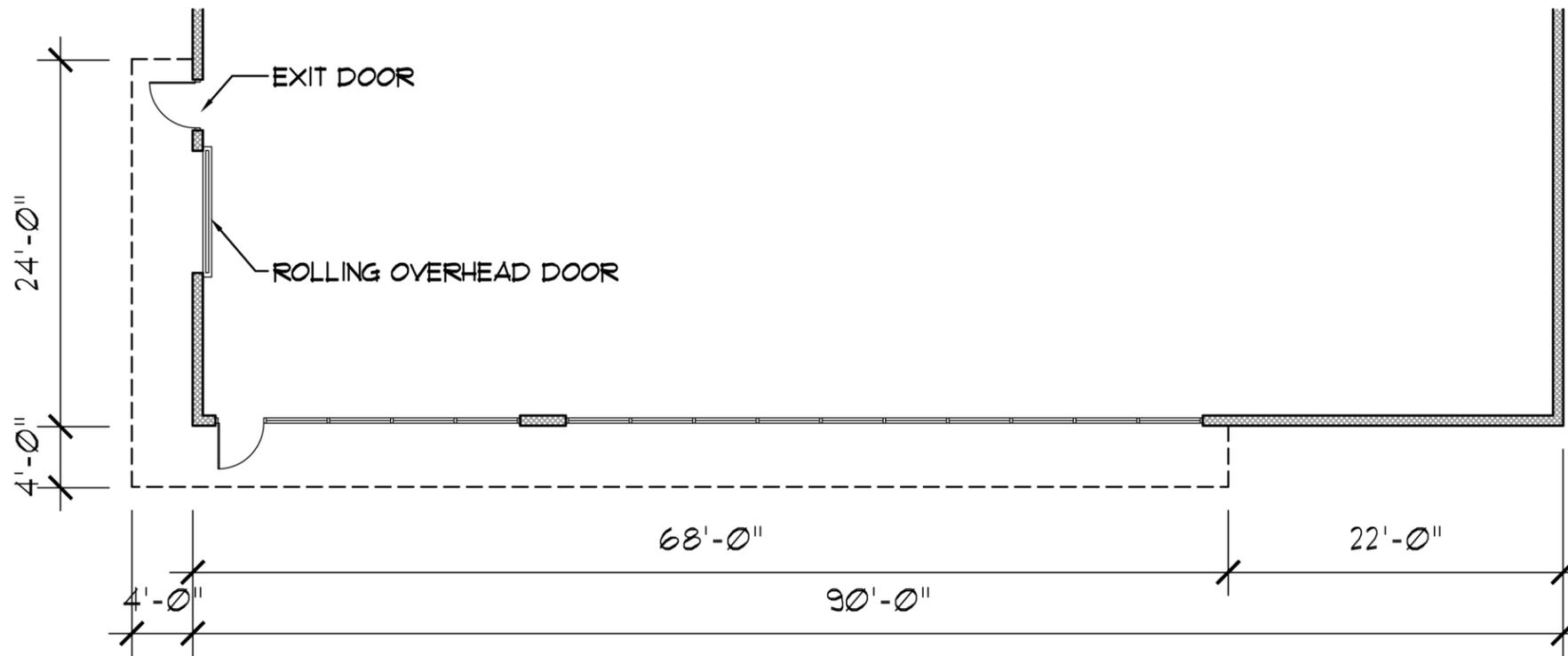
SUMMARY TABLE	
ITEM	QTY.
EXISTING ZONING	T5
PROPOSED ZONING	T5
LAND USE DESIGNATION	AUTO SUPPLY STORE
SITE ACREAGE	---
BUILDING SQUARE FOOTAGE	7,020 SF
NUMBER OF STORIES	1
MAXIMUM BUILDING HEIGHT	PER ORDINANCE
REQUIRED PARKING (9'x18' SP)/(7'x15')	
TOTAL SPACES PROVIDED	
SPACES PROVIDED: STANDARD	
SPACES PROVIDED: COMPACT	
SPACES PROVIDED: HC VAN ACCESS	

CLIENT INFORMATION
ADVANCED AUTO PARTS
 CONTACT: GREGORY MULKEY
 PHONE: (540) 561-8455

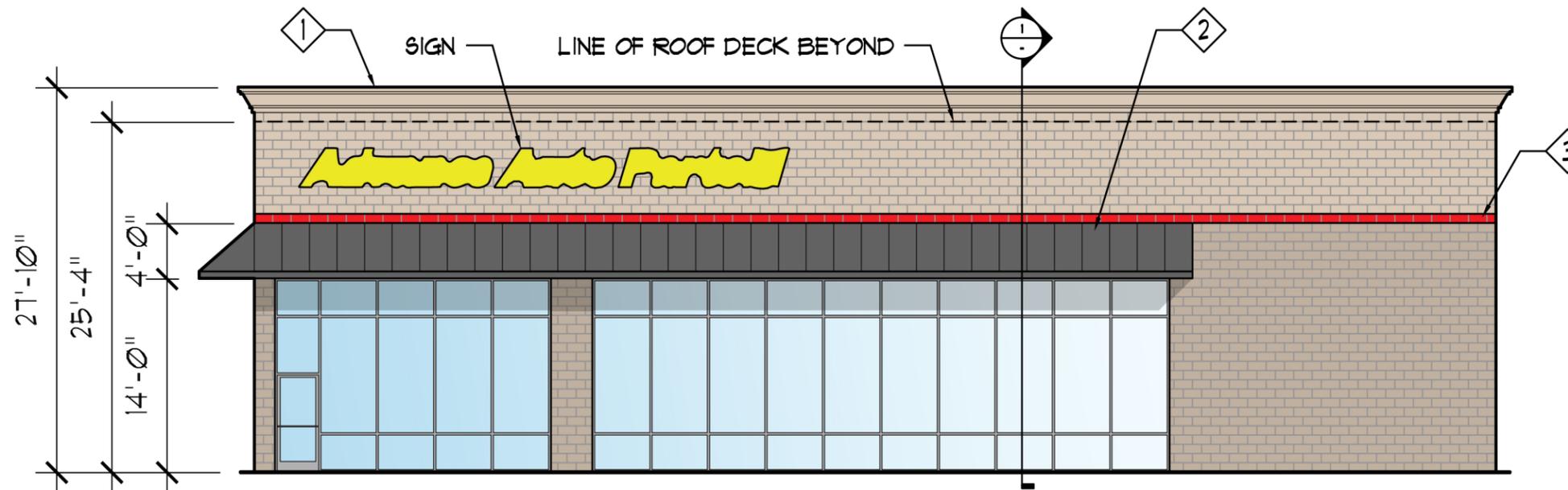
kbge
KIMBELL I BRUEHL I GARCIA I ESTES
 105 West Riverside Drive, Ste 110, Austin, Texas 78704
 T (512) 439-0400 www.kbge-eng.com
 TBPE No.F-12802

510 S GUADALUPE ST
 CITY OF SAN MARCOS, HAYS COUNTY, TX
ADVANCED AUTO PARTS
PRELIMINARY SITE LAYOUT

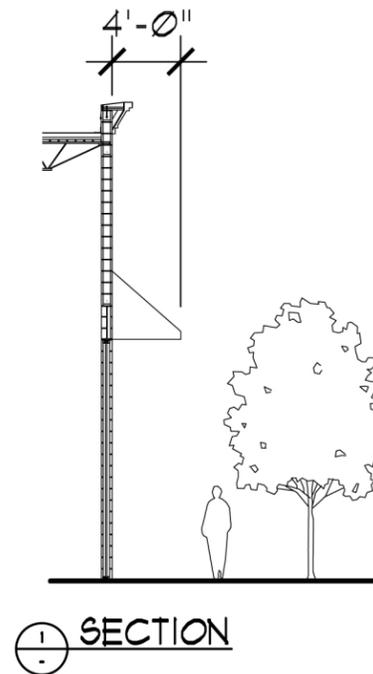
CHECKED BY: **JENNIFER GARCIA, PE**
 JOB NUMBER: **259-008** ISSUE DATE: **03/29/16**
 SHEET: **EXHIBIT 13**



PARTIAL PLAN VIEW AT SECONDARY ENTRANCE ON GUADALUPE STREET



FRONT ELEVATION



SECTION

EXPRESSION TOOLS LEGEND:

- ① EIFS CORNICE
- ② METAL CANOPY/AWNING
- ③ SECOND FLOOR EXPRESSION LINE (CHANGE OF MATERIAL TO SMOOTH FACE CMU)

* FRONT ELEVATION GLAZING CALCULATION (UP TO 14'): 866 SF / 1260 SF = 68.7%

ADVANCE AUTO PARTS - SAN MARCOS, TX

SCALE: 3/32" = 1'-0"
MARCH 15, 2016





Legislation Text

File #: ZC-16-02, **Version:** 1

AGENDA CAPTION:

ZC-16-02 (1331 Old Ranch Road 12) Hold a public hearing and consider a request by Billie Jo Allen, on behalf of Carol L. Snodgrass and Janet Honig, for a zoning change from "SF-6" Single Family Residential to "MU" Mixed Use for approximately 0.58 acres, out of the T.J. Chambers, Abstract No. 2, located at 1331 Old Ranch Road 12. (A. Brake)

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: N/A

Account Number: N/A

Funds Available: N/A

Account Name: N/A

CITY COUNCIL GOAL: Community Wellness / Strengthen the Middle Class

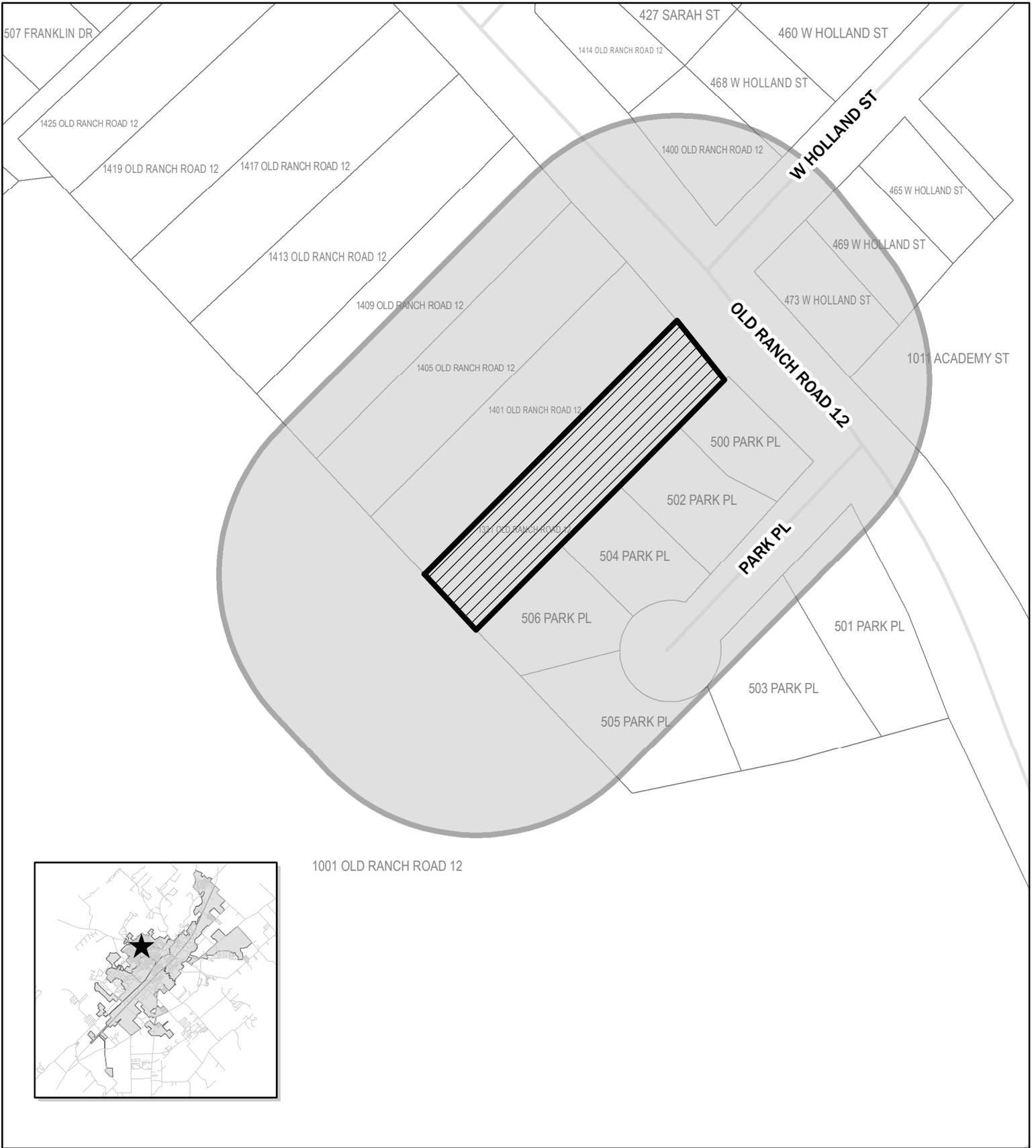
COMPREHENSIVE PLAN ELEMENT(s): Land Use, Goal 1

BACKGROUND:

The subject property is approximately 0.58 acres in size and is located along Old Ranch Road 12, south of the intersection with Holland Street and is zoned Single-Family Residential ("SF-6"). The property lies north of the San Marcos Cemetery and is within walking distance of Texas State University. The applicant is requesting a zoning change to Mixed Use ("MU") and has stated that the intent is to sell the property.

A Preferred Scenario Amendment was not required as the property lies within an Area of Stability and is classified as a Mixed Residential Area. Mixed Use is an eligible zoning category within this designation and most of the properties adjacent to the subject tract are rentals. Mixed Use zoning would allow the construction of duplexes, single-family attached, or single-family detached residential structures. It would also permit small scaled mixed use buildings that have residential units above retail or offices uses, especially on existing residential use properties.

Staff recommends **approval** of the zoning change request.

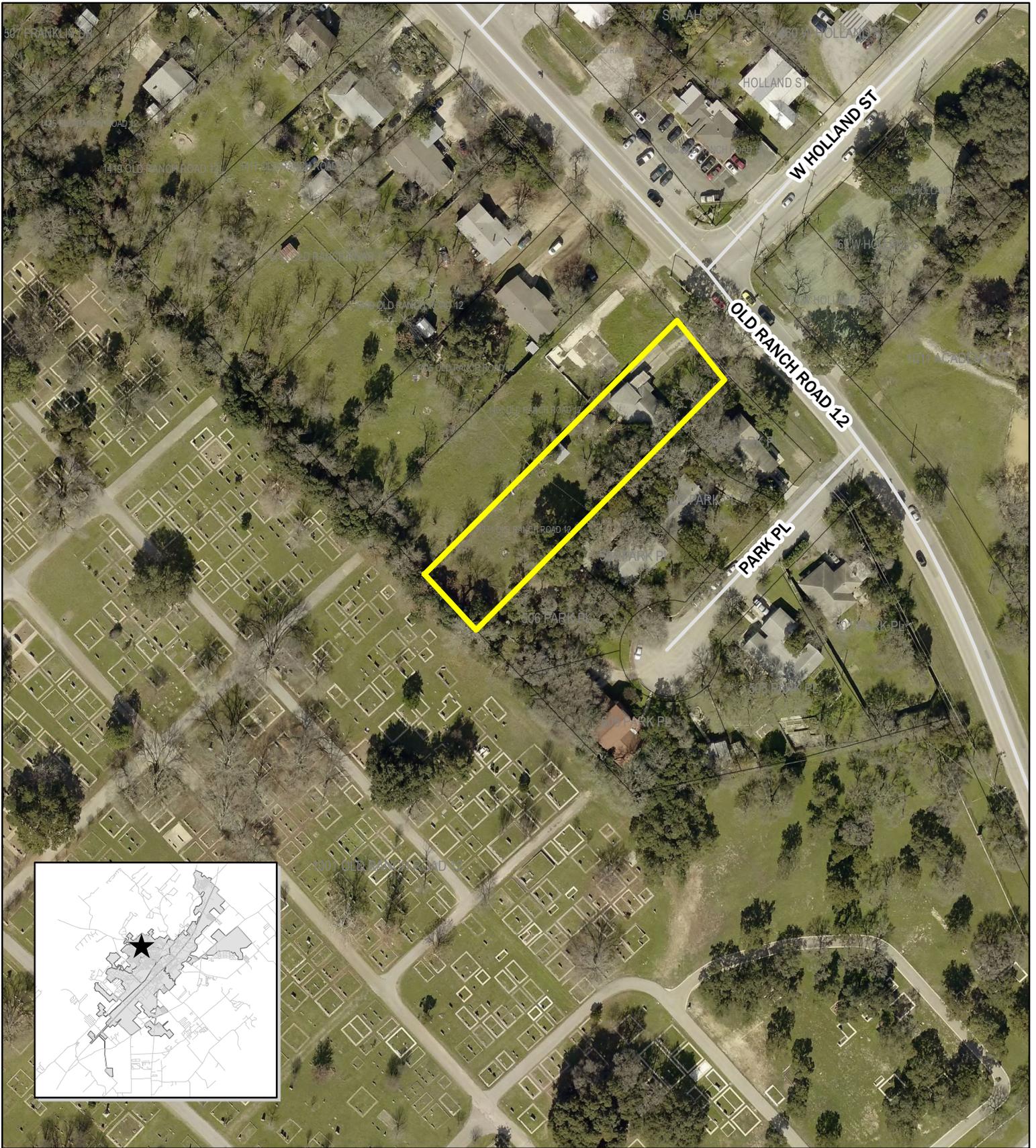


ZC-16-02
SF-6 to MU
1331 Old Ranch Road 12
Map Date: 3/25/2016

-  Site Location
-  Buffer



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ZC-16-02

SF-6 to MU

1331 Old Ranch Road 12

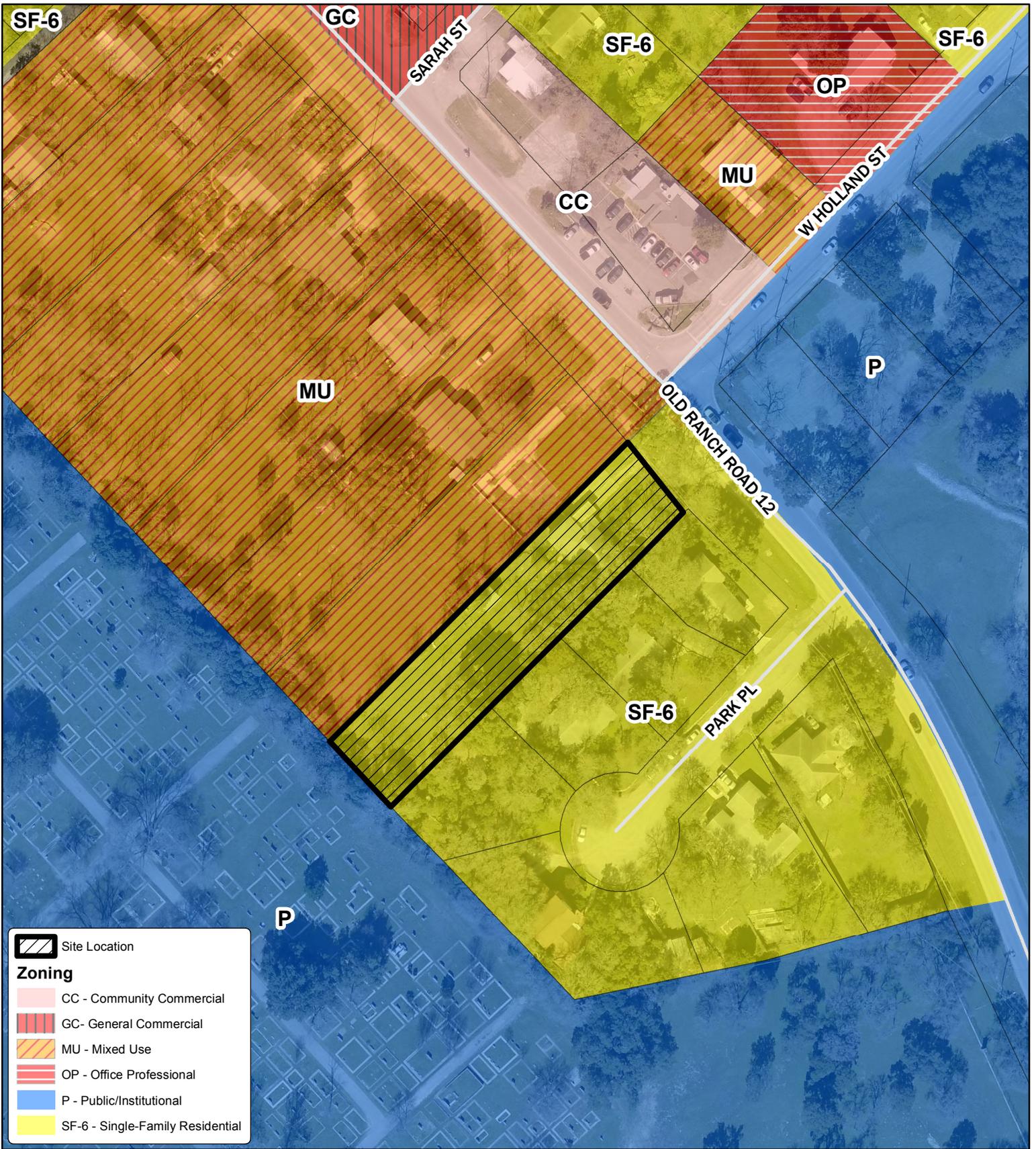
Map Date: 3/25/2016

 Site Location



0 100 200 400 Feet

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ZC-16-02
SF-6 to MU
1331 Old Ranch Road 12
Map Date: 3/29/2016



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Zoning Change ZC-16-02 1331 Old Ranch Road 12



Summary: The applicant is requesting a zoning change from “SF-6” Single-Family Residential to “MU” Mixed Use for approximately 0.58 acres out of the T.J. Chambers Survey, Abstract No. 2, located at 1331 Old Ranch Road 12.

Applicant: Billie Jo Allen
15922 Dulce Creek Drive
San Antonio, TX 78247

Property Owners: Carol L. Snodgrass, 121 N.W. Hills Road, San Marcos, TX 78666
Billie Jo Allen, 15922 Dulce Creek Drive, San Antonio, TX 78247
Janet Honig, 7619 Presidio Ledge, Boerne, TX 78015

Notification: Personal notifications of the public hearing were mailed on Friday, April 1, 2016 to all property owners within 200 feet of the subject property. Signs were posted on the subject property on Friday, April 1, 2016 for the public hearing on April 12, 2016.

Response: There have been no citizen comments as of the staff report date. The applicant sent out letters to the surrounding property owners and received feedback from them. Those letters have been included in the background information.

Property/Area Profile:

Legal Description: Approximately 0.58 acres out of the T.J. Chambers Survey, Abstract No. 2

Location: 1331 Old Ranch Road 12

Existing Use of Property: Single-family residence (vacant)

Proposed Use of Property: Commercial

Preferred Scenario Map: Area of Stability

Existing Zoning: SF-6 (Single-Family)

Proposed Zoning: MU (Mixed Use)

Utility Capacity: Adequate

Sector: 2

Area Zoning and Land Use Pattern:

	Zoning	Existing Land Use	Preferred Scenario
N of Property	CC and P	Restaurant & Texas State University	Area of Stability
S of Property	P	City Cemetery	Area of Stability
E of Property	SF-6	Single-family	Area of Stability
W of Property	MU	Single-family homes, mostly rentals	Area of Stability

Case Summary

The subject property is approximately 0.58 acres in size and is located along Old Ranch Road 12, south of the intersection with Holland Street and is zoned Single-Family Residential (“SF-6”). The property lies north of the San Marcos Cemetery and is within walking distance of Texas State University. The applicant is requesting a zoning change to Mixed Use (“MU”) and has stated that the intent is to sell the property. The properties west of the subject tract are zoned MU and are mostly rentals. The proposed Mixed Use zoning would allow the construction of duplexes, single-family attached, or single-family detached residential structures. It would also permit small scaled mixed use buildings that have residential units above retail or offices uses, especially on existing residential use properties. No plans have been submitted at this time; however, all requirements in the Land Development Code must be met. A use table has been attached outlining what uses could currently be considered in Mixed Use (MU) zoning.

Planning Department Analysis

The zoning change request has been reviewed using The Comprehensive Master Plan, *Vision San Marcos: A River Runs Through Us* as well as the guidance criteria in Section 1.5.1.5 of the Land Development Code. A review worksheet is attached to this report which details the analysis of the zoning change using Comprehensive Plan Elements. Staff finds this request is consistent with the Comprehensive Plan Elements as summarized below:

- The property lies within an Area of Stability and is classified as a Mixed Residential Area. Mixed Use is an eligible zoning category within this area; therefore, a Preferred Scenario Amendment is not required.
- The subject property is located in the Purgatory Creek Watershed. Additional impervious cover for the site is not accounted for within the Plan’s Water Quality Model. However, approximately 10% of additional impervious cover can be anticipated due to a change of maximum allowable percentage within the zoning district. SF-6 zoning allows for a maximum of 50% impervious cover, while MU zoning allows for a maximum of 60% impervious cover.
- The subject property is “moderately” constrained according to the Land Use Suitability Map – this is due mainly to the site’s location in the Purgatory Creek Watershed, erosive soils and minor elevation change in the middle of the property.

In addition, the consistency of this proposed change to the LDC criteria is detailed below:

Evaluation			Criteria (LDC 1.5.1.5)
Consistent	Inconsistent	Neutral	
X			<p>The proposed change implements the policies of the adopted Master Plan, including the land use classification on the Future Land Use Map and any incorporated sector plan maps</p> <p><i>The change is consistent with the Preferred Scenario Map and Comprehensive Plan Elements in Vision San Marcos. See the analysis above and the attached Comprehensive Plan Worksheet.</i></p>
		X	<p>Consistency with any development agreement in effect</p> <p><i>No development agreements are in effect for this property.</i></p>
X			<p>Whether the uses permitted by the proposed change and the standards applicable to such uses will be appropriate in the immediate area of the land to be reclassified</p> <p><i>The zoning designation of Mixed Use with the comprehensive plan goal to achieve diversified housing options and mixed-use development to serve citizens with varying needs and interests.</i></p>
X			<p>Whether the proposed change is in accord with any existing or proposed plans for providing public schools, streets, water supply, sanitary sewers, and other public services and utilities to the area</p> <p><i>The property is within the City’s water and wastewater service area. There</i></p>

Evaluation		Neutral	Criteria (LDC 1.5.1.5)
Consistent	Inconsistent		
			<i>is a Capital Improvement Plan project anticipated in the immediate area scheduled to begin construction in 2018. This project will widen Old Ranch Road 12 from Craddock Avenue to Holland Street and will include sidewalks on both sides of the street. While the project does not directly affect the subject property, it could alleviate some of the traffic issues in the area.</i>
X			<p>Other factors which substantially affect the public health, safety, morals, or general welfare</p> <p><i>None noted.</i></p>

Additionally, the Commission should consider:

- (1) Is the property suitable for use as presently zoned?

Staff evaluation: The property could be developed under the current zoning designation.

- (2) Has there been a substantial change of conditions in the neighborhood surrounding the subject property?

Staff evaluation: The neighborhood surrounding the subject property remains a mix of residential and commercial uses. The properties west of the subject tract are zoned Mixed Use and are mostly rentals.

- (3) Will the proposed rezoning address a substantial unmet public need?

Staff evaluation: This would not address a substantial unmet public need. However, a change to MU zoning would be consistent with the comprehensive plan goal to achieve diversified housing options and mixed-use development to serve citizens with varying needs and interests.

- (4) Will the proposed rezoning confer a special benefit on the landowner/developer and cause a substantial detriment to the surrounding lands?

Staff evaluation: No, there is no special benefit to the landowner.

- (5) Will the proposed rezoning serve a substantial public purpose?

Staff evaluation: The rezoning does not serve a substantial public purpose but one goal within the Comprehensive Plan is to achieve diversified housing options to serve citizens with varying needs and interests. The proposed rezoning could contribute to a diverse range of housing options as well as increase commercial goods and services in the immediate area.

Staff presents this request to the Commission and recommends approval.

The Commission's Responsibility:

The Commission is required by law to hold a public hearing and receive public comment regarding the proposed zoning. After considering the public input, the Commission is charged with making an advisory recommendation to the City Council regarding the request. The City Council will ultimately decide whether to approve or deny the zoning change request. The Commission's advisory recommendation to the Council is a discretionary decision.

Prepared by:

Alison Brake, CNU-A

Planner

March 29, 2016

Name

Title

Date

ZC-16-02 (1331 Old Ranch Road 12) Zoning Change Review (By Comp Plan Element)

LAND USE – Preferred Scenario Map / Land Use Intensity Matrix

	YES	NO (map amendment required)
Does the request meet the intent of the Preferred Scenario Map and the Land Use Intensity Matrix?	X – Staff determined the property lies within an existing Mixed Residential Area therefore a PSA is not required	

ECONOMIC DEVELOPMENT – Furthering the goal of the Core 4 through the three strategies

Not applicable to this Zoning Change Request

STRATEGY	SUMMARY	Supports	Contradicts	Neutral
Preparing the 21 st Century Workforce	Provides / Encourages educational opportunities			
Competitive Infrastructure & Entrepreneurial Regulation	Provides / Encourages land, utilities and infrastructure for business			
The Community of Choice	Provides / Encourages safe & stable neighborhoods, quality schools, fair wage jobs, community amenities, distinctive identity			

ENVIRONMENT & RESOURCE PROTECTION – Land Use Suitability & Development Constraints

	1 (least)	2	3 (moderate)	4	5 (most)
Level of Overall Constraint				X	
Constraint by Class					
Cultural	X				
Edwards Aquifer		X			
Endangered Species	X				
Floodplains	X				
Geological	X				
Slope	X		X		
Soils				X	
Vegetation	X				
Watersheds				X	
Water Quality Zone	X				

ENVIRONMENT & RESOURCE PROTECTION – Water Quality Model Results

Located in Subwatershed:	Purgatory Creek Watershed				
	0-25%	25-50%	50-75%	75-100%	100%+
Modeled Impervious Cover Increase Anticipated for watershed		X			
Notes: The change in impervious cover under the Preferred Scenario is attributed to portions of the Paso Robles development, the Government Center, and downtown development. Purgatory Creek is a direct tributary of the San Marcos River, home of several endangered species. The Plan emphasizes the need to identify potential pollution from redevelopment as construction runoff and debris can wash into the creek during storm events.					

NEIGHBORHOODS – Where is the property located

CONA Neighborhood(s):	Oak Heights
Neighborhood Commission Area(s):	2
Neighborhood Character Study Area(s):	Western

PARKS, PUBLIC SPACES AND FACILITIES –Availability of parks and infrastructure

	YES	NO
Will Parks and / or Open Space be Provided?		X
Will Trails and / or Green Space Connections be Provided?		X
Maintenance / Repair Density		
	Low (maintenance)	High (maintenance)
Wastewater Infrastructure	X	
Water Infrastructure	X	
Public Facility Availability		
	YES	NO
Parks / Open Space within ¼ mile (walking distance)? The entrance to Schulle Canyon is located approximately ½ mile from the subject property.		X
Wastewater service available?	X	
Water service available?	X	

TRANSPORTATION – Level of Service (LOS), Access to sidewalks, bicycle lanes and public transportation

	A	B	C	D	F
Existing Daily LOS Old Ranch Road 12			X		
Existing Peak LOS Old Ranch Road 12					X
Preferred Scenario Daily LOS Old Ranch Road 12				X	
Preferred Scenario Peak LOS Old Ranch Road 12					X

The Transportation Demand Model shows that Old Ranch Road 12 remain a LOS F in both the Existing Peak as well as in the Preferred Scenario Peak. A Capital Improvement Project for Old Ranch Road 12, scheduled for construction in 2018, could alleviate some traffic issues in the area.

	N/A	Good	Fair	Poor
Sidewalk Availability (Required to build.)	X			
There are no existing sidewalks. As the property redevelops, sidewalks will be required to be installed.				
	YES		NO	
Adjacent to existing bicycle lane?			X	
Adjacent to existing public transportation route?			X	
Notes: This site is not located adjacent to any public transit routes.				

USES ALLOWED IN MIXED USE (P = Allowed by Right; C = Conditional)

	MU
Farmers Market	P
Farms, General (Crops)	P
Accessory Building/Structure (No larger than 625 s.f. in size and 12' in height)	P
Accessory Building/Structure (Larger than 625 s.f. in size OR 12' in height)	C
Accessory Dwelling (One Accessory Dwelling Per Lot)	C
Bed and Breakfast Inn	P
Caretaker's/Guard's Residence	P
Community Home	P
Residential Hall or Boarding House	C
Duplex/Two-Family/Duplex Condominiums	C
Family Home Child Care	P
Four Family (Quadraplex) or Three Family (Tri-Plex)	C
Fraternity or Sorority Building	C
Loft Apartments	P
Multifamily (Apartments)	C
Single Family Detached House	P
Single Family Industrialized Home	P
Single Family Townhouse (Attached)	P
Single Family Zero Lot Line/Patio Homes	C
Armed Services Recruiting Center	P
Bank or Savings and Loan (w/o Drive-thru)	P
Bank or Savings and Loan (w Drive-thru)	C
Check Cashing Service	C
Offices (Health Services)	P
Offices (Medical Office)	P

	MU
Offices (Professional)	P
Appliance Repair	C
Artist or Artisans Studio	P
Automobile Driving School (including Defensive Driving)	P
Automatic Teller Machines (ATM's)	P
Barber/Beauty Shop, Haircutting (non-college)	P
Bed and Breakfast (No Permanent Residence)	P
Dance/Drama/Music Schools (Performing Arts)	P
Extended Stay Hotels/Motels (Residence hotels)	C
Exterminator Service (No outdoor sales or storage)	P
Hotel/Motel	P
Kiosk (Providing A Retail Service)	P
Laundry/Dry Cleaning (Drop Off/Pick Up)	P
Martial Arts School	P
Medical Supplies and Equipment	P
Mini-Warehouse/Self Storage Units	C
Photocopying/Duplicating/Copy Shop	P
Studio for Radio or Television (without tower)	P
Tool Rental (Indoor Storage only)	P
Antique Shop (with outside storage)	C
Auto Supply Store for New and Rebuilt Parts	C
Bike Sales and/or Repair	P
Convenience Store Without Gas Sales	P
Convenience Store With Gas Sales	C
Food or Grocery Store with Gasoline Sales	C

USES ALLOWED IN MIXED USE (P = Allowed by Right; C = Conditional)

	MU
Food or Grocery Store without Gasoline Sales	P
Home Improvement Center (10,000 s.f. or more)	C
Lawnmower Sales and/or Repair	C
Market (Public, Flea)	C
Pharmacy	P
Plant Nursery (Retail Sales/Outdoor Storage)	C
Recycling Kiosk	P
Restaurant/Prepared Food Sales	P
Restaurant/Prepared Food Sales with drive thru	C
Retail Store (Misc.) with Drive Thru Service	C
Retail Store (Misc.) without Drive Thru Service (Under 100,000 s.f. Bldg.)	C
Retail Store (over 10,000 s.f. or more Bldg.) outside sales	C
Retail Store (under 10,000 s.f. or more Bldg.) outside sales	C
Retail Store (under 10,000 s.f. or more Bldg.) no outside sales	P
Studio Tattoo or Body Piercing	C
Temporary Outdoor Retail Sales / Commercial Promotion (4 day time limit; Permit Required by Building Official)	P
Veterinarian (Indoor Kennels)	C
Woodworking Shop (Ornamental)	P
General Vehicular Sales and Service	C
Auto Repair (General)	C
Auto Repair as an Accessory Use to Retail Sales	C
Auto Tire Repair/Sales (Indoor)	C
Car Wash (Self Service; Automated)	C
Full Service Car Wash (Detail Shop)	C

	MU
Limousine/Taxi Service	C
Public Garage/Parking Structure	C
Amusement Services or Venues (Indoors)	C
Amusement Services or Venues (Outdoors)	C
Bar	C
Smoking Lounge	C
Civic/Conference Center	P
Country Club (Private)	C
Health Club (Physical Fitness; Indoors Only)	P
Motion Picture Theater (Indoors)	C
Museum (Indoors Only)	P
On-Premises Consumption of Alcohol	C
Park and/or Playground	P
Tennis Court (Lighted)	C
Theater (Non-Motion Picture; Live Drama)	C
Adult Day Care (No Overnight Stay)	P
Assisted Living Facility/Hospice	P
Child Day Care (Business)	P
Meeting Place/Nonreligious	P
Place of Religious Assembly/Church	P
Clinic (Medical)	P
Electrical Substation	C
Emergency Care Clinic	P
Fraternal Organization/Civic Club	P

USES ALLOWED IN MIXED USE (P = Allowed by Right; C = Conditional)

	MU
Governmental Building or Use (Municipal, State or Federal)	P
Heliport	C
Household Care Facility	C
Hospital (Acute Care/Chronic Care)	C
Nursing/Convalescent Home	P
Philanthropic organization	P
Post Office (Private)	P
Post Office (Governmental)	P
Rectory/Parsonage with Place of Worship	P
Retirement Home/Home for the Aged	P
School, K through 12 (Private)	C
School, K through 12 (Public)	P
School, Vocational (Business/Commercial Trade)	C
Caterer	P
Maintenance/Janitorial Service	C
Micro Brewery (onsite mfg. and sales)	C

Sept. 28,2015

Billie Jo Allen
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Please check one of the below :

..... No, I object to your property to be re-zone for mixed use

~~.....~~ I do not care if you get your property re-zoned for mixed use

Thank you for your quick response. Billie Jo Allen

any additional feedback is greatly appreciated.

Sept. 28,2015

Billie Jo Allen
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

Mr. & Mrs. Christopher Blood,

I am a home owner in San Marcos near your property -500 Park Place
I would like to get my property re-zoned for mixed use , but before
trying to do such I would like to know if you object? I would appreciate
your feelings on this subject.

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Hi Billie Jo -

Good luck on re-zoning and keep me
posted. We may try to re-zone sometime
in the future. Let me know how it goes.

Page 1

Chris

Sept. 28,2015

Billie Jo Allen
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Please check one of the below :

..... No, I object to your property to be re-zone for mixed use

.....  I do not care if you get your property re-zoned for mixed use

Thank you for your quick response. Billie Jo Allen

any additional feedback is greatly appreciated.

Sept. 28, 2015

Billie Jo Allen
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Please check one of the below :

.......... No, I object to your property to be re-zone for mixed use

.......... I do not care if you get your property re-zoned for mixed use

Thank you for your quick response. Billie Jo Allen

Any additional information is greatly appreciated.

Although you've not specified the anticipated "use," we believe

that an apartment complex or Page 1

the like would be unreasonably disruptive. Best wishes,
RT

Billie Jo Allen
Sept. 28,2015
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

Mr. Hueske,

I am a home owner in San Marcos near your property -501 Park Place
I would like to get my property re-zoned for mixed use , but before
trying to do such I would like to know if you object? I would appreciate
your feelings on this subject.

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Sept. 28,2015

Billie Jo Allen
15922 Dulce Creek DR
San Antonio, Texas 78666
210-973-9248

My Property:
1331 Old Ranch Rd 12
San Marcos, Tx 78666

Please check one of the below :

..... No, I object to your property to be re-zone for mixed use

..... I do not ^{object} care if you get your property re-zoned for mixed use

Thank you for your quick response. Billie Jo Allen

Any additional feedback is greatly appreciated.

The property at 504 Park place is a rental property.

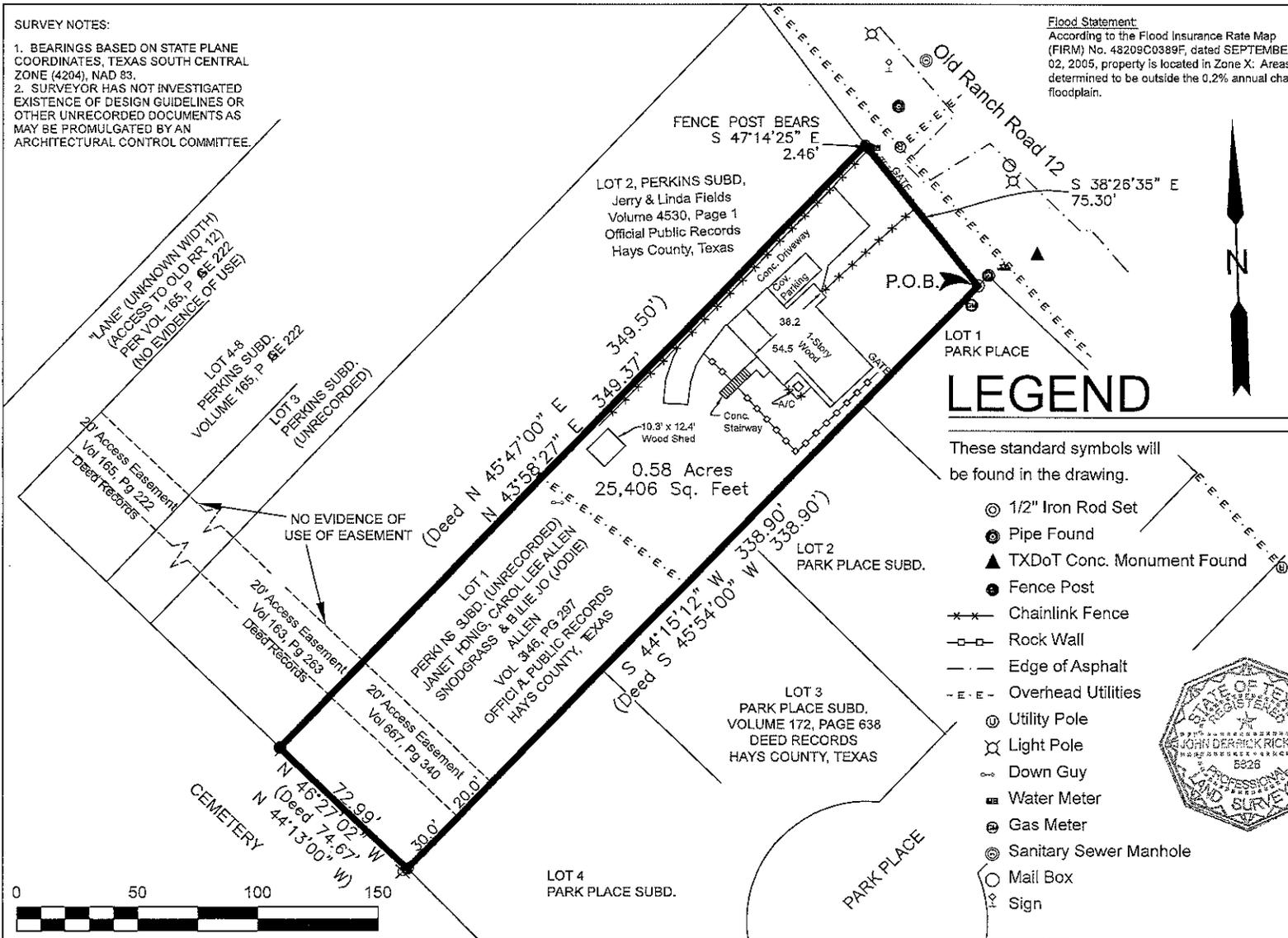
Page 1

I am curious to know your reasons for the zoning change. Do you want to expand your options or do you have something specific in mind? Please contact me at 512-699-1126.
Anthony

SURVEY NOTES:

1. BEARINGS BASED ON STATE PLANE COORDINATES, TEXAS SOUTH CENTRAL ZONE (4204), NAD 83.
2. SURVEYOR HAS NOT INVESTIGATED EXISTENCE OF DESIGN GUIDELINES OR OTHER UNRECORDED DOCUMENTS AS MAY BE PROMULGATED BY AN ARCHITECTURAL CONTROL COMMITTEE.

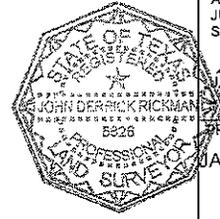
Flood Statement:
According to the Flood Insurance Rate Map (FIRM) No. 48209C0389F, dated SEPTEMBER 02, 2005, property is located in Zone X. Areas determined to be outside the 0.2% annual chance floodplain.



LEGEND

These standard symbols will be found in the drawing.

- ⊙ 1/2" Iron Rod Set
- ⊕ Pipe Found
- ▲ TXDoT Conc. Monument Found
- Fence Post
- *— Chainlink Fence
- Rock Wall
- Edge of Asphalt
- E - E - Overhead Utilities
- ⊕ Utility Pole
- ⊗ Light Pole
- ⊖ Down Guy
- Water Meter
- ⊕ Gas Meter
- ⊙ Sanitary Sewer Manhole
- ⊙ Mail Box
- ⊙ Sign



SURVEY OF A 0.58 OF AN ACRE (25,406 SQ. FT.) TRACT OF LAND SITUATED IN THE CITY OF SAN MARCOS, TEXAS, OUT OF THE T.J. CHAMBERS SURVEY, ABSTRACT NO. 2, BEING ALL OF LOT 1, PERKINS SUBDIVISION, AN UNRECORDED SUBDIVISION AS CONVEYED TO JANET HONIG, CAROL LEE ALLEN SNODGRASS AND BILLIE JO (JODIE) ALLEN OF RECORD IN VOLUME 3146, PAGE 297, OFFICIAL PUBLIC RECORDS OF HAYS COUNTY, TEXAS.

BAKER
SURVEYING, INC.
PH. (830) 833-2250
FAX. (830) 833-2257
2250 US 281 N.
BLANCO, TX. 78606

RICKMAN LAND SURVEYING

TBPLS FIRM NO. :01919-00
419 BIG BEND, CANYON LAKE, TEXAS 78133
PHONE (830) 935-2457
WWW.RICKMANLANDSURVEYING.COM

SIGNATURE/SEAL
CERTIFICATION:
THE UNDERSIGNED DOES HEREBY CERTIFY THAT THIS PLAT CORRECTLY REPRESENTS A SURVEY MADE ON THE GROUND ON JUNE 08, 2015, OF THE PROPERTY SHOWN HEREON

Derrick Rickman
Derrick Rickman, RPLS No. 5826

PROJECT DESCRIPTION
JANET HONIG, CAROL SNODGRASS & BILLIE ALLEN
1331 OLD RANCH ROAD 12
SAN MARCOS, TX

DATE	06/18/2015	JOB NO.
SCALE	1"=50'	DRG NO.
DRAWN BY	IS	
CHECKED BY		

City of San Marcos ZONING CHANGE APPLICATION

APPLICANT

PROPERTY OWNER

Name: Carol L. Snodgrass
 Mailing Address: 121 N.W. Hills Rd
San Marcos, TX 78666
 Telephone No.: 512-392-8794
 E-mail address: mcsnp2000@yahoo.com

SAME

*

PROPERTY DESCRIPTION:

Street Address: 1331 Old Ranch Rd-12 San Marcos, TX 78666
 Subdivision: Perkins Block: _____ Lot(s): A-2 Lot 1
 Other Description (if unplatted) 42-45 UNREC SD-T CHAMBERS
 * a metes and bounds description is required if property is a partial lot or is not platted
 Appraisal District Tax ID No.: R 38022 11-6450-0000-00100-3 Acres: 0.59

Lien Holder(s) - for notification purposes:

Name: _____

Mailing Address: _____

(If more than one lien holder, please provide information on a separate page)

A certificate of no tax delinquency must be attached to this application

ZONING CHANGE INFORMATION:

Zoning Designation: Current: SF-6 Requested: MU
 Master Plan Designation: AOS Land Use Map Amendment Required? _____
 Present Use of Property: Residential Home
 Desired Use of Property/Reason for Change: FOR SALE

I certify that the information in this application is complete and accurate.

I am the property owner of record; or 1 of 3 owners

I have attached authorization to represent the owner, organization, or business in this application.

Signature: Carol L. Snodgrass

Date: 10-02-15

Printed Name: Carol L. Snodgrass

*

City of San Marcos ZONING CHANGE APPLICATION

	<u>APPLICANT</u>		<u>PROPERTY OWNER</u>
Name:	<u>Billie Jo Allen</u>		
Mailing Address:	<u>15922 Dulce Creek Dr San Antonio, TX 78247</u>		
Telephone No.:	<u>210-973-9248</u>		
E-mail address:	<u>NURSEFN2000@yahoo.com</u>		<u>Sam P</u>

PROPERTY DESCRIPTION:

Street Address: 1331 Old Ranch Rd-12 San Marcos, TX 78666

Subdivision: Perkins Block: _____ Lot(s): A-2 Lot-1

Other Description (if unplatted) 42-45 UNREC sd-T chambers

* a metes and bounds description is required if property is a partial lot or is not platted

Appraisal District Tax ID No.: R 38022 11-6450000-001003 Acres: 0.59

Lien Holder(s) - for notification purposes:

Name: _____

Mailing Address: _____

(If more than one lien holder, please provide information on a separate page)

A certificate of no tax delinquency must be attached to this application

ZONING CHANGE INFORMATION:

Zoning Designation: Current: SF-6 Requested: MU

Master Plan Designation: AOS Land Use Map Amendment Required? _____

Present Use of Property: Residential Home

Desired Use of Property/Reason for Change: FOR SALE

I certify that the information in this application is complete and accurate.

I am the property owner of record; or 1 of 3 owners

I have attached authorization to represent the owner, organization, or business in this application.

Signature: Billie Jo Allen Date: Oct 5, 2015

Printed Name: Billie Jo Allen

City of San Marcos ZONING CHANGE APPLICATION

	<u>APPLICANT</u>	<u>PROPERTY OWNER</u>
Name:	<u>Janet Honig</u>	<u>Janet Honig</u>
Mailing Address:	<u>7619 Presidio Ledge Boerne, TX 78015</u>	<u>7619 Presidio Ledge Boerne, TX 78015</u>
Telephone No.:	<u>210-451-5684</u>	<u>210-451-5684</u>
E-mail address:	<u>honig-janet@yahoo.com</u>	<u>honig-janet@yahoo.com</u>

PROPERTY DESCRIPTION:

Street Address: 1331 Old Ranch Rd-12 San Marcos, TX 78666

Subdivision: Parkins Block: _____ Lot(s): A-2 Lot 1

Other Description (if unplatted) 42-45 UNREC S-D Chambers

* a metes and bounds description is required if property is a partial lot or is not platted

Appraisal District Tax ID No.: R 38022 11-6450-0200-00⁰⁰⁻³ Acres: 0.59

Lien Holder(s) - for notification purposes:

Name: _____

Mailing Address: _____

(If more than one lien holder, please provide information on a separate page)

A certificate of no tax delinquency must be attached to this application

ZONING CHANGE INFORMATION:

Zoning Designation: Current: SF-6 Requested: MU

Master Plan Designation: AOS Land Use Map Amendment Required? _____

Present Use of Property: Residential Home

Desired Use of Property/Reason for Change: FOR sale

I certify that the information in this application is complete and accurate.

I am the property owner of record; or 1 of 3 owners

I have attached authorization to represent the owner, organization, or business in this application.

Signature: Janet Honig Date: 10-10-15

Printed Name: Janet Honig

AGREEMENT TO THE PLACEMENT OF ZONING NOTIFICATION SIGNS

The City of San Marcos Land Development Code provides that notification signs shall be placed on any property that is the subject of a zoning change, zoning variance, or conditional use permit. The signs shall be placed on the property by the Development Services-Planning staff prior to the 10th day before the scheduled public hearing based on the following criteria:

- Signs shall be placed on each street for property having multiple street frontages
- Signs shall be placed in a visible, unobstructed location near the front property line

Signs shall remain in place until final action is taken on the application, unless the case is formally withdrawn by the applicant prior to a final decision. Staff will remove the signs.

It is the responsibility of the applicant to periodically check the sign locations to verify that signs remain in place and have not been vandalized or removed. It is the responsibility of the applicant to immediately notify the Development Services-Planning Department of any missing or defective signs.

It is unlawful for a person to alter any notification sign, or to remove it while the case is pending. However, any removal or alteration that is beyond the control of the applicant shall not constitute a failure to meet notification requirements.

I have read the above statement and agree to allow the placement of notification signs as required on the project covered by the attached request. The Development Services-Planning staff has my permission to place these signs on my property. I will notify City staff if the sign is damaged or removed.


Signature of Applicant

Oct 5, 2018
Date

FOR STAFF USE ONLY:	
Sign (s) were placed by staff on _____	by _____
Sign (s) were removed by staff on _____	by _____

AGREEMENT TO THE PLACEMENT OF ZONING NOTIFICATION SIGNS

The City of San Marcos Land Development Code provides that notification signs shall be placed on any property that is the subject of a zoning change, zoning variance, or conditional use permit. The signs shall be placed on the property by the Development Services-Planning staff prior to the 10th day before the scheduled public hearing based on the following criteria:

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I have read the above statement and agree to allow the placement of notification signs as required on the project covered by the attached request. The Development Services-Planning staff has my permission to place these signs on my property. I will notify City staff if the sign is damaged or removed.

*

Janet Horrig

Signature of Applicant

10-10-15

Date

FOR STAFF USE ONLY:

Sign (s) were placed by staff on _____ by _____.

Sign (s) were removed by staff on _____ by _____.

AGREEMENT TO THE PLACEMENT OF ZONING NOTIFICATION SIGNS

The City of San Marcos Land Development Code provides that notification signs shall be placed on any property that is the subject of a zoning change, zoning variance, or conditional use permit. The signs shall be placed on the property by the Development Services-Planning staff prior to the 10th day before the scheduled public hearing based on the following criteria:

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I have read the above statement and agree to allow the placement of notification signs as required on the project covered by the attached request. The Development Services-Planning staff has my permission to place these signs on my property. I will notify City staff if the sign is damaged or removed.

Carol Smodgrass
Signature of Applicant

10-02-15
Date

FOR STAFF USE ONLY:

Sign (s) were placed by staff on _____ by _____.

Sign (s) were removed by staff on _____ by _____.



Petition for Zoning Change, Zoning Overlay, or Historic District Checklist

- A pre-application conference with staff is recommended;
- A completed application form, including a statement verifying land ownership and, if applicable, authorization of the land owner's agent to file the petition and required fees;
- Legal description of the site (metes and bounds or lot and block if platted)
- Certificate of no tax delinquency
- Subdivision Concept Plats or Site Plans as required (see Chapter 1 of the LDC)
- Additional information as required to clarify the request

I hereby certify and attest that the application is complete and all information identified above is complete and hereby submitted for review.

Signed: Janet Horig

Date: 10-10-15

Print Name: Janet Horig

Engineer Surveyor Architect/Planner Owner Agent: _____



Petition for Zoning Change, Zoning Overlay, or Historic District Checklist

- A pre-application conference with staff is recommended;
- A completed application form, including a statement verifying land ownership and, if applicable, authorization of the land owner's agent to file the petition and required fees;
- Legal description of the site (metes and bounds or lot and block if platted)
- Certificate of no tax delinquency
- Subdivision Concept Plats or Site Plans as required (see Chapter 1 of the LDC)
- Additional information as required to clarify the request

I hereby certify and attest that the application is complete and all information identified above is complete and hereby submitted for review.

Signed: Billie Jo Allen

Date: Oct 5, 2015

Print Name: Billie Jo Allen

Engineer Surveyor Architect/Planner Owner Agent: _____



Petition for Zoning Change, Zoning Overlay, or Historic District Checklist

- A pre-application conference with staff is recommended;
- A completed application form, including a statement verifying land ownership and, if applicable, authorization of the land owner's agent to file the petition and required fees;
- Legal description of the site (metes and bounds or lot and block if platted)
- Certificate of no tax delinquency
- Subdivision Concept Plats or Site Plans as required (see Chapter 1 of the LDC)
- Additional information as required to clarify the request

I hereby certify and attest that the application is complete and all information identified above is complete and hereby submitted for review.

Signed: Carol L. Snodgrass Date: 10-02-15

Print Name: Carol L. Snodgrass

Engineer Surveyor Architect/Planner Owner Agent: _____

To Whom It May Concern,

10-6-15

I, Janet Honig, am $\frac{1}{3}$ owner of the
property on 1331 RR 12, San Marcos, Tx.

Janet Honig

February 27, 2016

To whom it may concern:

I Carol Lee Snodgrass am an owner of the property located at 1331 Old Ranch Road 12, San Marcos, Texas 78666. I am giving my sister Billie Jo Allen permission to represent me for the rezoning request and process.

Carol Lee Snodgrass

From: [Brake, Alison](#)
To: [Brake, Alison](#)
Subject: FW: Zoning Change 1331 Old Ranch Road 12 ZC-16-02
Date: Thursday, April 07, 2016 8:44:58 AM
Importance: High

From: Diann McCabe [<mailto:diann.mccabe@gmail.com>]
Sent: Wednesday, April 06, 2016 10:02 PM
To: Planning_Info <planning_info@sanmarcostx.gov>
Subject: Zoning Change 1331 Old Ranch Road 12 ZC-16-02

We don't know much about this request for a zoning change from "SF-6" Single family Residential to "MU" Mixed Use for .58 acres. The university residence hall will open in the fall of 2016, across RR 12 from this site (or nearby). With a mixed use facility on .58 acres, we see the trend on RR12 for dense urban living. This could be fine, or worrisome.

Please consider that the established single family neighborhoods down Holland Street and up Franklin in the opposite direction love their homes and the culture growing there.

If this zoning change is approved, make it something that fits into the scheme of the community in this part of San Marcos. Watch that parking, traffic, and design is compatible. Don't let it be so huge, so crowded, so poorly designed that families in the neighborhood choose to slip away.

Thank you,
Terry and Diann McCabe
1315 Alamo St.
512.353.8742



Legislation Text

File #: ZC-16-03, **Version:** 1

AGENDA CAPTION:

ZC-16-03 (202 Posey Road) Hold a public hearing and consider a request by Gunnerson Outdoor Advertising, on behalf of Bobbie Gilbert, for a zoning change from "AR" Agricultural Ranch to "GC" General Commercial for approximately 2.3 acres, more or less, out of the W. H. Van Horn Survey No. 107, Abstract No. 464, located west of IH-35 at 202 Posey Road. (A. Brake)

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: N/A

Account Number: N/A

Funds Available: N/A

Account Name: N/A

CITY COUNCIL GOAL: Community Wellness / Strengthen the Middle Class

COMPREHENSIVE PLAN ELEMENT(s): Land Use, Goal 1

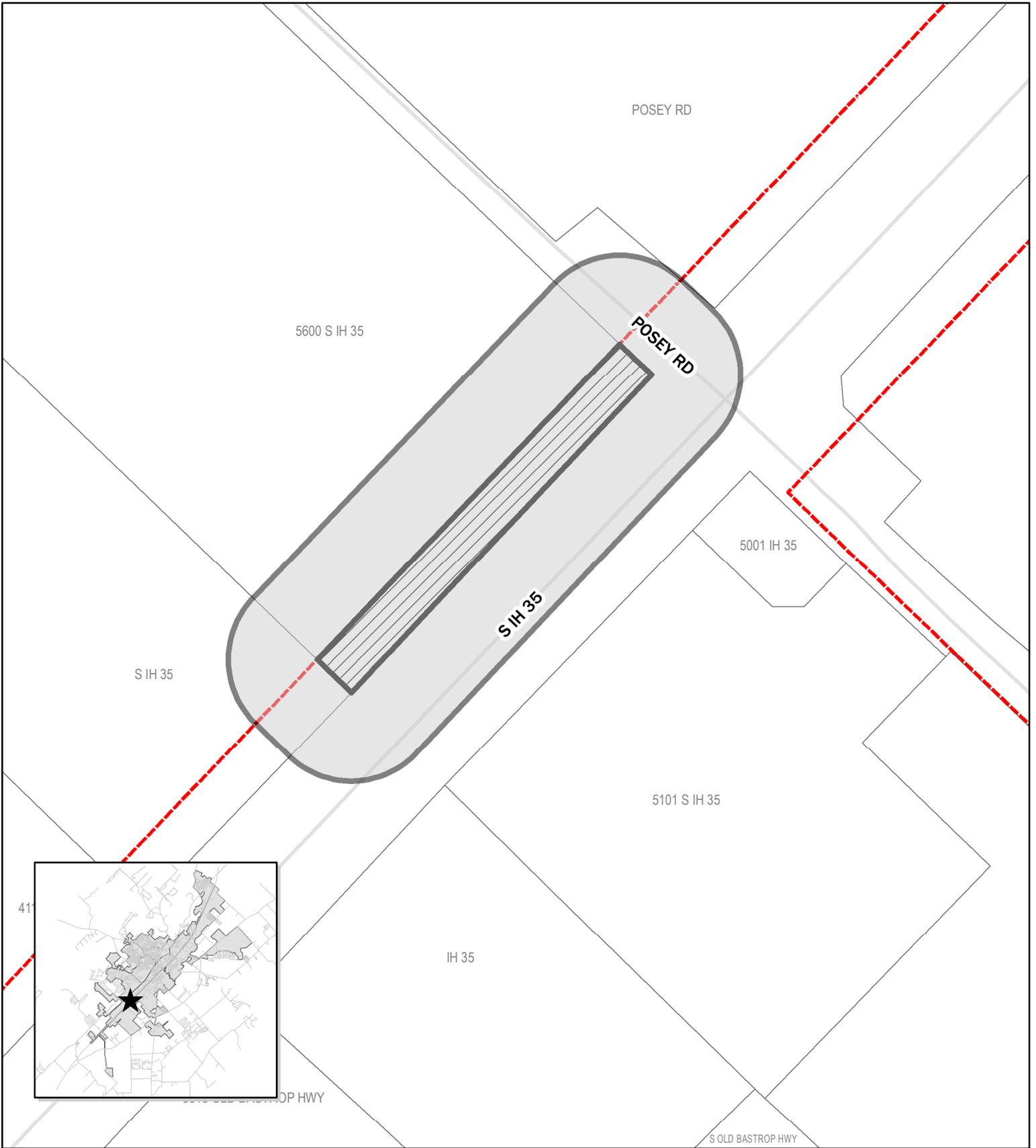
BACKGROUND:

The subject property consists of approximately 2.3 acres of vacant, unplatted land out of the W. H. Van Horn Survey. It fronts the southbound frontage road of IH-35 and also has frontage along Posey Road. The Texas State Transportation Center is located north of the property and the Toyota dealership is located to the east. While the majority of the property lies in the City's Extraterritorial Jurisdiction (ETJ) the subject property was annexed in 1986 and is zoned "AR" Agricultural Ranch. Prior to any site, building permit, or sign permit being issued, the property will need to be platted.

The applicant is requesting a zoning change to "GC" General Commercial in order to install an off-premise freestanding sign. Section 6.3.3.4(e) of the Land Development Code requires the applicant to first remove an existing off-premise sign that has a total area equal to or greater than the total area proposed for the new sign. An off-premise freestanding sign was removed on January 8, 2015 to make way for the construction of the Hilton Garden Inn, south of River Ridge Parkway.

The applicant applied for and received approval for a Preferred Scenario Amendment in Spring 2016. The proposed General Commercial zoning would allow the construction of limited commercial services.

Staff recommends **approval** of the zoning change request.



ZC-16-03
AG to GC
202 Posey Rd
Map Date: 3/30/2016

-  Site Location
-  200' Buffer
-  City Limit



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

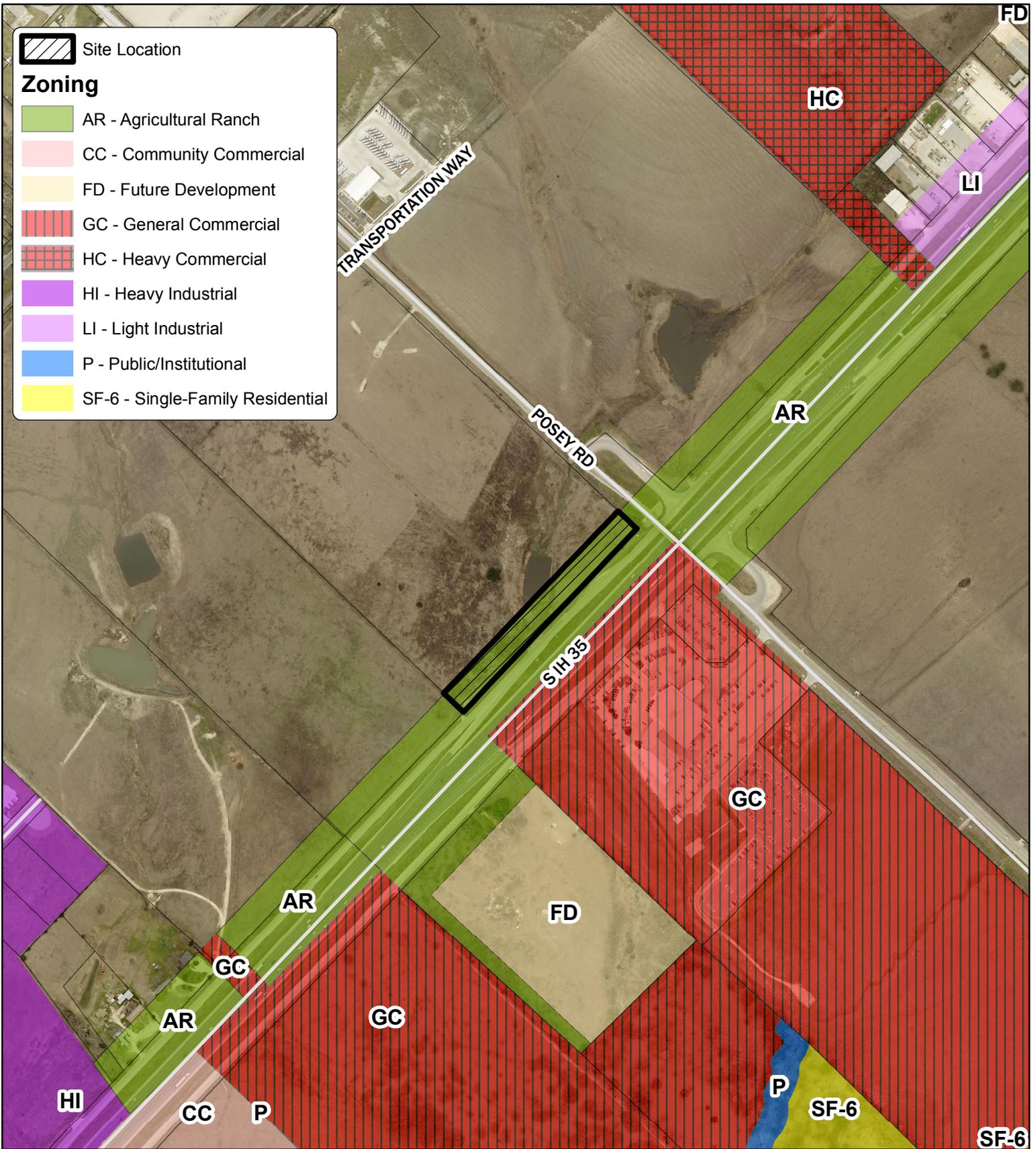


ZC-16-03
AG to GC
202 Posey Rd
Map Date: 3/30/2016

-  Site Location
-  City Limit



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.



ZC-16-03
AG to GC
202 Posey Rd
Map Date: 3/30/2016



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

Zoning Change ZC-16-03 202 Posey Road



Summary: The applicant is requesting a zoning change from “AR” Agricultural Ranch to “GC” General Commercial for 2.3 acres

Applicant: Gunnerson Outdoor Advertising
1801 IH-35 South
San Marcos, TX 78666

Property Owners: Bobbie Gilbert
15 Timbercrest
San Marcos, TX 78666

Notification: Personal notifications of the public hearing were mailed on Friday, April 1, 2016 to all property owners within 200 feet of the subject property. Signs were posted on the subject property on Friday, April 1 for the public hearing on April 12, 2016.

Response: There have been no citizen comments as of the staff report date.

Property/Area Profile:

Legal Description: Approximately 2.3 acres, more or less, out of the W. H. Van Horn Survey No. 107, Abstract No. 464

Location: Southwest corner of IH-35 South and Posey Road

Existing Use of Property: Vacant

Proposed Use of Property: Commercial

Preferred Scenario Map: Employment Center

Existing Zoning: Agricultural Ranch (“AR”)

Proposed Zoning: General Commercial (“GC”)

Utility Capacity: There is sufficient water utilities in the area but the extension of wastewater utilities will be needed for further development of the property

Sector: Sector Four (4)

Area Zoning and Land Use Pattern:

	Zoning	Existing Land Use	Preferred Scenario
N of Property	AR & outside City Limits	Vacant & Texas State Transportation Center	Area of Stability
S of Property	AR & outside City Limits	Vacant	Area of Stability
E of Property	GC	Toyota Dealership	Area of Stability
W of Property	Outside City Limits	PEC Electric Station	Area of Stability

Case Summary

The subject property is approximately 2.3 acres in size and is generally located along the southbound access road of Interstate 35 and its intersection with Posey Road, directly opposite from the Toyota dealership. The Texas State Transportation Center is located north of the property. Currently the site, the majority of which lies in the City's Extraterritorial Jurisdiction (ETJ), consists of vacant land. The subject property was annexed in 1986 and is zoned "AR" Agricultural Ranch; it has been zoned as such since at least the mid-1990s if not earlier.

The applicant is requesting a zoning change to "GC" General Commercial in order to install an off-premise freestanding sign. Before a sign permit for a new off-premise sign permit can be issued, Section 6.3.3.4(e) of the Land Development Code requires the applicant to first remove an existing off-premise sign that has a total area equal to or greater than the total area proposed for the new sign. The applicant stated that an existing sign was removed on January 8, 2015 to make way for the construction of the Hilton Garden Inn on the southbound frontage of IH-35, south of River Ridge Parkway. Staff verified that the existing sign was removed; an exhibit has been included in the background information illustrating this.

In Spring of 2016, the applicant applied for a Preferred Scenario Amendment (PSA) to change from an Area of Stability / New Development to an Employment Center. The Planning and Zoning Commission provided a recommendation of approval for the PSA, and following the Commission recommendation, received City Council approval of the Map Amendment to an Employment Center at the April 5 meeting.

The proposed General Commercial zoning would allow the construction of limited commercial services such as office, automotive supply stores, retail buildings, and other similar uses. If this request is not granted, the applicant would be able to develop the property under the current Agricultural Ranch zoning or apply for a variety of zoning districts noted on the Zoning Translation Table within an Employment Center such as Office Professional, Light and Heavy Industrial zoning, and Mixed Use. Examples of land uses which are currently permitted include farming, grain sales, single family homes with accessory buildings or dwellings, and government buildings. A use table has been attached outlining what uses could currently be considered in General Commercial (GC) zoning.

Planning Department Analysis

The subject property applied for a Preferred Scenario Amendment (PSA) to change from an Area of Stability / New Development to an Employment Center. This PSA was approved by City Council on April 5, 2016. *Vision San Marcos Comprehensive Plan* provides the following descriptions of an Employment Center:

- *New development such as large scale industrial, manufacturing, office park and intense commercial uses are appropriate in employment centers.*
- *Businesses that provide long-term sustainable employment opportunities should be encouraged in the employment centers. Typically these uses are located on large sites with access to road and rail transportation and have access to city services such as water, sewer, and electricity.*
- *Specialized uses such as industry, large office parks, retail malls and commercial recreation will be recommended for the potential employment centers in the preferred scenario.*
- *The Preferred Scenario is a recommendation from the public, adopted by City Council which supports development in the intensity zones and guides future economic development decisions supporting industrial, office park, and commercial uses in the employment centers.*

The zoning change request has been reviewed using The Comprehensive Master Plan, *Vision San Marcos: A River Runs Through Us* as well as the guidance criteria in Section 1.5.1.5 of the Land Development Code. A review worksheet is attached to this report which details the analysis of the zoning change using Comprehensive Plan Elements.

Staff finds this request is generally consistent with the Comprehensive Plan Elements as summarized below:

- The property is located along Interstate 35 and is located within a designated Employment Center on the Preferred Scenario Map (approved in April 2016);
- The property being considered for rezoning is located in a low to moderately constrained area according to the Land Use Suitability Map;
- The subject property is not located in a wastewater or water “hot spot”. Water service is available to the subject property; however, wastewater service is not. Further development of the site will require adequate wastewater service to be extended to this site.
- The transportation access to the site appears to be adequate. The Travel Demand Model (TDM) shows the segment of I-35 in which the property fronts on to be flowing at a level of service A. It also indicates that some improvements along Posey Road may be required. The Texas Department of Transportation (TxDOT) has plans to reconstruct the Posey Road overpass starting this year.

In addition, the consistency of this proposed change to the LDC criteria is detailed below:

Evaluation		Neutral	Criteria (LDC 1.5.1.5)
Consistent	Inconsistent		
X			<p>The proposed change implements the policies of the adopted Master Plan, including the land use classification on the Future Land Use Map and any incorporated sector plan maps</p> <p><i>The change is consistent with the Preferred Scenario Map and Comprehensive Plan Elements in Vision San Marcos. See the analysis above and the attached Comprehensive Plan Worksheet.</i></p>
		X	<p>Consistency with any development agreement in effect</p> <p><i>No development agreements are in effect for this property.</i></p>
X			<p>Whether the uses permitted by the proposed change and the standards applicable to such uses will be appropriate in the immediate area of the land to be reclassified</p> <p><i>The zoning designation of General Commercial is consistent with the designation of surrounding parcels to the east and along northbound I-35 access road. The installation of the off-premise freestanding sign itself will not promote economic development but rezoning the property has the potential to develop a site that is adjacent to the IH-35 corridor thus promoting economic development.</i></p>
X			<p>Whether the proposed change is in accord with any existing or proposed plans for providing public schools, streets, water supply, sanitary sewers, and other public services and utilities to the area</p> <p><i>The property is within the City's water and wastewater service area. Water utilities are sufficient in this area but further development of the site, other than an off-premise freestanding sign, will require the extension of wastewater utilities.</i></p>
X			<p>Other factors which substantially affect the public health, safety, morals, or general welfare</p> <p><i>None noted.</i></p>

Additionally, the Commission should consider:

- (1) Is the property suitable for use as presently zoned?

Staff evaluation: The property could be developed under the current zoning designation. It would be limited to uses that include agriculture/ranch uses, single family residential, government buildings, parks, religious assembly, plant nursery or stables.

- (2) Has there been a substantial change of conditions in the neighborhood surrounding the subject property?

Staff evaluation: The surrounding area along I-35 remains partially undeveloped and partially developed commercially. The Trace development is located to the southeast of the property which includes a mixture of zoning districts including General Commercial.

- (3) Will the proposed rezoning address a substantial unmet public need?

Staff evaluation: This would not address a substantial unmet public need.

- (4) Will the proposed rezoning confer a special benefit on the landowner/developer and cause a substantial detriment to the surrounding lands?

Staff evaluation: No, there is no special benefit to the landowner.

- (5) Will the proposed rezoning serve a substantial public purpose?

Staff evaluation: The rezoning does not serve a substantial public purpose but as infill areas along I-35 develop in a commercial manner this will allow for commercial development to occur allowing for providing improved economic opportunities for residents.

Staff recommends **approval** of the zoning change request.

The Commission's Responsibility:

The Commission is required by law to hold a public hearing and receive public comment regarding the proposed zoning. After considering the public input, the Commission is charged with making an advisory recommendation to the City Council regarding the request. The City Council will ultimately decide whether to approve or deny the zoning change request. The Commission's advisory recommendation to the Council is a discretionary decision.

Prepared by:

Alison Brake, CNU-A

Planner

March 30, 2016

Name

Title

Date

ZC-16-03 (202 Posey Road) Zoning Change Review (By Comp Plan Element)

LAND USE – Preferred Scenario Map / Land Use Intensity Matrix

	YES	NO (map amendment required)
Does the request meet the intent of the Preferred Scenario Map and the Land Use Intensity Matrix?		X

ECONOMIC DEVELOPMENT – Furthering the goal of the Core 4 through the three strategies

STRATEGY	SUMMARY	Supports	Contradicts	Neutral
Preparing the 21 st Century Workforce	Provides / Encourages educational opportunities			Applicant has not indicated that educational facilities will be included.
Competitive Infrastructure & Entrepreneurial Regulation	Provides / Encourages land, utilities and infrastructure for business			Applicant has not indicated that infrastructure will be extended.
The Community of Choice	Provides / Encourages safe & stable neighborhoods, quality schools, fair wage jobs, community amenities, distinctive identity			Applicant has not indicated that opportunities for jobs and services will be included.

ENVIRONMENT & RESOURCE PROTECTION – Land Use Suitability & Development Constraints

	1 (least)	2	3 (moderate)	4	5 (most)
Level of Overall Constraint	X	X			
Constraint by Class					
Cultural	X				
Edwards Aquifer	X				
Endangered Species	X				
Floodplains	X				
Geological	X				
Slope	X				
Soils	X				
Vegetation	X				
Watersheds	X				
Water Quality Zone	X			X	X

ENVIRONMENT & RESOURCE PROTECTION – Water Quality Model Results

Located in Subwatershed:	York Creek Watershed				
	0-25%	25-50%	50-75%	75-100%	100%+
Modeled Impervious Cover Increase Anticipated for	X				

watershed					
Notes: No additional impervious cover was anticipated, or modeled, in this subwatershed. There were no additional recommendations for this watershed in the Plan.					

NEIGHBORHOODS – Where is the property located

CONA Neighborhood(s):	N/A
Neighborhood Commission Area(s):	4
Neighborhood Character Study Area(s):	N/A

PARKS, PUBLIC SPACES AND FACILITIES –Availability of parks and infrastructure

		YES	NO
Will Parks and / or Open Space be Provided?			X
Will Trails and / or Green Space Connections be Provided?			X
Maintenance / Repair Density	Low (maintenance)	Medium	High (maintenance)
Wastewater Infrastructure	X		
Water Infrastructure	X		
Public Facility Availability			
		YES	NO
Parks / Open Space within ¼ mile (walking distance)?			X
Wastewater service available? Council approved an Out of City Utility Extension request for the 45 acre property located north of the subject property across Posey Road in December 2015. The extension of wastewater facilities, while not needed to install a billboard, is needed for future development of the subject property.			X
Water service available?		X	

TRANSPORTATION – Level of Service (LOS), Access to sidewalks, bicycle lanes and public transportation

		A	B	C	D	F
Existing Daily LOS	Posey Road	X				
	Southbound Frontage IH-35	X				
Existing Peak LOS	Posey Road	X				
	Southbound Frontage IH-35	X				
Preferred Scenario Daily LOS	Posey Road	X				
	Southbound Frontage IH-35	X				
Preferred Scenario Peak LOS	Posey Road Southbound Frontage IH-35	X		X		
The Transportation Demand Model shows that both Posey Road and the Southbound Frontage of IH-35 remain a LOS A in both Existing Daily and Peak as well as in the Preferred Scenario Daily. The TDM does show deterioration in LOS for Posey Road to a LOS C in the Preferred Scenario Peak.						
		N/A	Good	Fair	Poor	
Sidewalk Availability (Required to build.)		X				

There are no existing sidewalks. They will be required to be built along Posey Road and the Southbound Frontage Road of IH-35 at the time of development.

	YES	NO
Adjacent to existing bicycle lane?		X
Adjacent to existing public transportation route?	X	

Notes: The property is situated along a CARTS route however the closest stop is located at the Outlet Mall on the northbound side of IH-35.

USES ALLOWED IN GENERAL COMMERCIAL (P = Allowed by Right; C = Conditional)

	GC
Farmers Market	C
Accessory Building/Structure (No larger than 625 s.f. in size and 12' in height)	P
Accessory Building/Structure (Larger than 625 s.f. in size OR 12' in height)	P
Accessory Dwelling (One Accessory Dwelling Per Lot)	C
Bed and Breakfast Inn	P
Caretaker's/Guard's Residence	P
Loft Apartments	C
Armed Services Recruiting Center	P
Bank or Savings and Loan (w/o Drive-thru)	P
Bank or Savings and Loan (w Drive-thru)	P
Check Cashing Service	P
Offices (Health Services)	P
Offices (Medical Office)	P
Offices (Professional)	P
Call Service Center	P
Appliance Repair	P
Artist or Artisans Studio	P
Ambulance Service (Private)	P
Automobile Driving School (including Defensive Driving)	P
Automatic Teller Machines (ATM's)	P
Barber/Beauty College (barber or cosmetology school or college)	P
Barber/Beauty Shop, Haircutting (non-college)	P
Bed and Breakfast (No Permanent Residence)	P

	GC
Communication Equipment (Installation and/or Repair - No outdoor sales or storage)	P
Dance/Drama/Music Schools (Performing Arts)	P
Extended Stay Hotels/Motels (Residence hotels)	P
Exterminator Service (No outdoor sales or storage)	P
Funeral Home	P
Hotel/Motel	P
Kiosk (Providing A Retail Service)	P
Laundry/Dry Cleaning (Drop Off/Pick Up)	P
Martial Arts School	P
Medical Supplies and Equipment	P
Mini-Warehouse/Self Storage Units	C
Off-Premises Freestanding Sign	P
Photocopying/Duplicating/Copy Shop	P
Studio for Radio or Television (without tower)	P
Tool Rental (Indoor Storage only)	P
Tool Rental (with Outdoor Storage)	C
Washateria/Laundry (Self-Serve)	P
All-Terrain Vehicle (go-carts) Dealer/Sales	P
Antique Shop (with outside storage)	P
Auto Dealer (Primarily New/Used Auto Sales as accessory use only)	P
Auto Dealer, Used Auto Sales	P
Auto Supply Store for New and Rebuilt Parts	P

USES ALLOWED IN GENERAL COMMERCIAL (P = Allowed by Right; C = Conditional)

	GC
Bike Sales and/or Repair	P
Building Material Sales	P
Cabinet Shop (Manufacturing)	P
Convenience Store Without Gas Sales	P
Convenience Store With Gas Sales	P
Department Store	P
Food or Grocery Store with Gasoline Sales	P
Food or Grocery Store without Gasoline Sales	P
Gravestone/Tombstone Sales	C
Gun Smith	C
Home Improvement Center (10,000 s.f. or more)	P
Lawnmower Sales and/or Repair	P
Liquor Sales (retail)	P
Market (Public, Flea)	C
Pharmacy	P
Plant Nursery (Retail Sales/Outdoor Storage)	P
Recycling Kiosk	P
Restaurant/Prepared Food Sales	P
Restaurant/Prepared Food Sales with beer/wine off-premises consumption	C
Restaurant/Prepared Food Sales with drive thru	P
Retail Store (Misc.) with Drive Thru Service	P
Retail Store (Misc.) without Drive Thru Service (Under 100,000 s.f. Bldg.)	P
Retail Store (100,000 s.f. or more Bldg.)	P
Retail Store (over 10,000 s.f. or more Bldg.) outside sales	P

	GC
Retail Store (under 10,000 s.f. or more Bldg.) outside sales	P
Retail Store (under 10,000 s.f. or more Bldg.) no outside sales	P
Security Systems Installation Company	P
Shopping Center (Over 5 Acres)	P
Studio Tattoo or Body Piercing	P
Temporary Outdoor Retail Sales / Commercial Promotion (4 day time limit; Permit Required by Building Official)	P
Upholstery Shop (Non-Auto)	P
Veterinarian (Indoor Kennels)	P
Woodworking Shop (Ornamental)	P
General Vehicular Sales and Service	P
Auto Body Repair	C
Auto Muffler Shop	P
Auto Paint Shop	C
Auto Repair (General)	P
Auto Repair as an Accessory Use to Retail Sales	P
Auto Tire Repair/Sales (Indoor)	P
Car Wash (Self Service; Automated)	P
Full Service Car Wash (Detail Shop)	P
Limousine/Taxi Service	P
Public Garage/Parking Structure	C
Tire Sales (Outdoors/Storage)	P
Transit Terminal	C
Truck Terminal	C

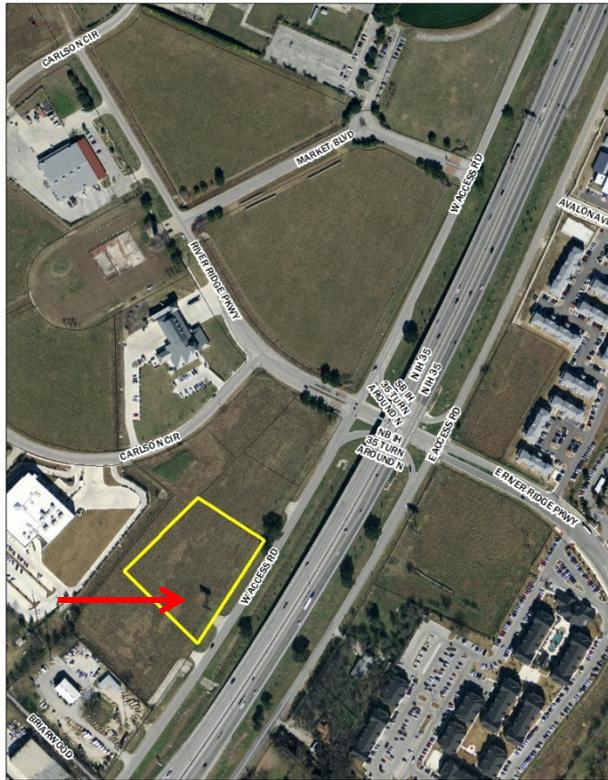
USES ALLOWED IN GENERAL COMMERCIAL (P = Allowed by Right; C = Conditional)

	GC
Amusement Services or Venues (Indoors)	P
Amusement Services or Venues (Outdoors)	P
Bar	C
Billiard/Pool Facility/Nightclub (Three or More Tables) No alcohol consumption	P
Smoking Lounge	P
Charitable Gaming Facility	C
Civic/Conference Center	P
Country Club (Private)	C
Dance Hall/Dancing Facility	P
Day Camp	P
Driving Range	P
Health Club (Physical Fitness; Indoors Only)	P
Motion Picture Theater (Indoors)	P
Motion Picture Studio, Commercial Film	P
Museum (Indoors Only)	P
On-Premises Consumption of Alcohol	C
Park and/or Playground	P
Travel Trailers/RVs (Short Term Stays)	P
RV/Travel Trailer Sales	P
Tennis Court (Lighted)	C
Theater (Non-Motion Picture; Live Drama)	P
Adult Day Care (No Overnight Stay)	P
Assisted Living Facility/Hospice	P
Child Day Care (Business)	P
Meeting Place/Nonreligious	P
Place of Religious Assembly/Church	P
Clinic (Medical)	P
Electrical Substation	C

	GC
Emergency Care Clinic	P
Fraternal Organization/Civic Club	P
Governmental Building or Use (Municipal, State or Federal)	P
Heliport	C
Hospital (Acute Care/Chronic Care)	P
Nursing/Convalescent Home	P
Philanthropic organization	P
Post Office (Private)	P
Post Office (Governmental)	P
Rectory/Parsonage with Place of Worship	P
Retirement Home/Home for the Aged	P
School, K through 12 (Private)	P
School, K through 12 (Public)	P
School, Vocational (Business/Commercial Trade)	P
Auction Sales (Non-Vehicle)	P
Caterer	C
Extermination Service	P
Feed and Grain Store	P
Maintenance/Janitorial Service	P
Metal Fabrication Shop	C
Moving Storage Company	C
Portable Building Sales	P
Taxidermist	P
Warehouse/Office and Storage	C
Electronic Assembly/High Tech Manufacturing	P
Micro Brewery (onsite mfg. and sales)	C
Outside Storage (as primary use)	C
Research Lab (Non-Hazardous)	C

Location of Removed Off-Premise Sign

2014



2015



ASH & ASSOCIATES

SURVEYING · ENGINEERING · ARCHITECTURE

"Serving the Community of Texas"

132 Jackson Lane
San Marcos, Texas 78666
Phone: 512-392-1719
ashandassociates.net

STATE OF TEXAS
COUNTY OF HAYS

2.30 ACRES
W.H. VANHORN SURVEY, A-464

BEING A 2.30 ACRE TRACT OF LAND SITUATED IN THE WILLIAM H. VANHORN SURVEY NO. 107, ABSTRACT NO. 464, HAYS COUNTY, TEXAS, SAME BEING A PORTION OF THAT CALLED 52.93 ACRE TRACT CONVEYED TO BOBBIE POLLARD GILBERT BY CORRECTION WARRANTY DEED RECORDED IN VOLUME 2872, APGE 852 OF THE OFFICIAL PUBLIC RECORDS OF HAYS COUNTY TEXAS, AND BEING MORE PARTICULARLY DESCRIBED BY METES AND BOUNDS AS FOLLOWS:

BEGINNING at a TXDOT concrete monument found at a corner fence post at the east corner of said Gilbert 52.93 acre tract, same being at the intersection of the northwest line of Interstate Highway No. 35, a 300 foot wide public right-of-way, with the southwest line of Posey Road – C.R. 235, a variable width public right-of-way, for the east corner of the tract of land herein described, and from which a 1" iron pipe found at the north corner of said Gilbert 52.93 acre tract, bears North 46°42'59" West (N 46°02' W Record), a distance of 2291.75 feet (2294.9' Record);

THENCE South 43°32'41" West (S 44°15' W Record), along the southeast line of said Gilbert 52.93 acre tract and the common northwest line of said Interstate Highway No. 35, a distance of **1001.75 feet** (1003.7' Record) to a 1/2" iron rod set at a 6" cedar post marking the south corner of said Gilbert 52.93 acre tract and the common east corner of that called 40 acre tract conveyed to Mary Ann Mutter by deed recorded in Volume 581, Page 491 of the Real Property Records of Hays County, Texas, for the south corner of the tract of land herein described, and from which a 4" pipe post marking the west corner of said Gilbert 52.93 acre tract bears North 46°16'20" West (N 45° W Record), a distance of 2342.18 feet (2346.0' Record);

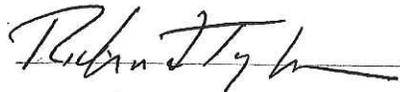
THENCE North 46°16'20" West (N 45° W Record), along the common line of said Gilbert 52.93 acre tract and said Mary Ann Mutter called 40 acre tract, a distance of **100.00 feet** to the intersection of said common line with the northwest line of the City Limits of San Marcos, Texas, for the west corner of the tract of land herein described;

THENCE through and severing said Gilbert 52.93 acre tract, **North 43°32'41" East**, along said City Limits, a distance of **1000.98 feet** to the northeast line of said Gilbert 52.93 acre tract and the common southwest line of aforesaid Posey Road – C.R. 235, variable width public right-of-way, for the north corner of the tract of land herein described;

THENCE South 46°42'59" East (S 46°02' E Record), along said common line, a distance of **100.00 feet** to the **POINT OF BEGINNING** and **CONTAINING 2.30 ACRES OF LAND.**

BEARING BASIS HEREIN DERIVED BY GPS MEASUREMENTS ADJUSTED BY HARN (HIGH ACCURACY REFERENCE NETWORK) & PROJECTED TO TEXAS STATE PLANE COORDINATES (TEXAS SOUTH CENTRAL ZONE) & NAD83.

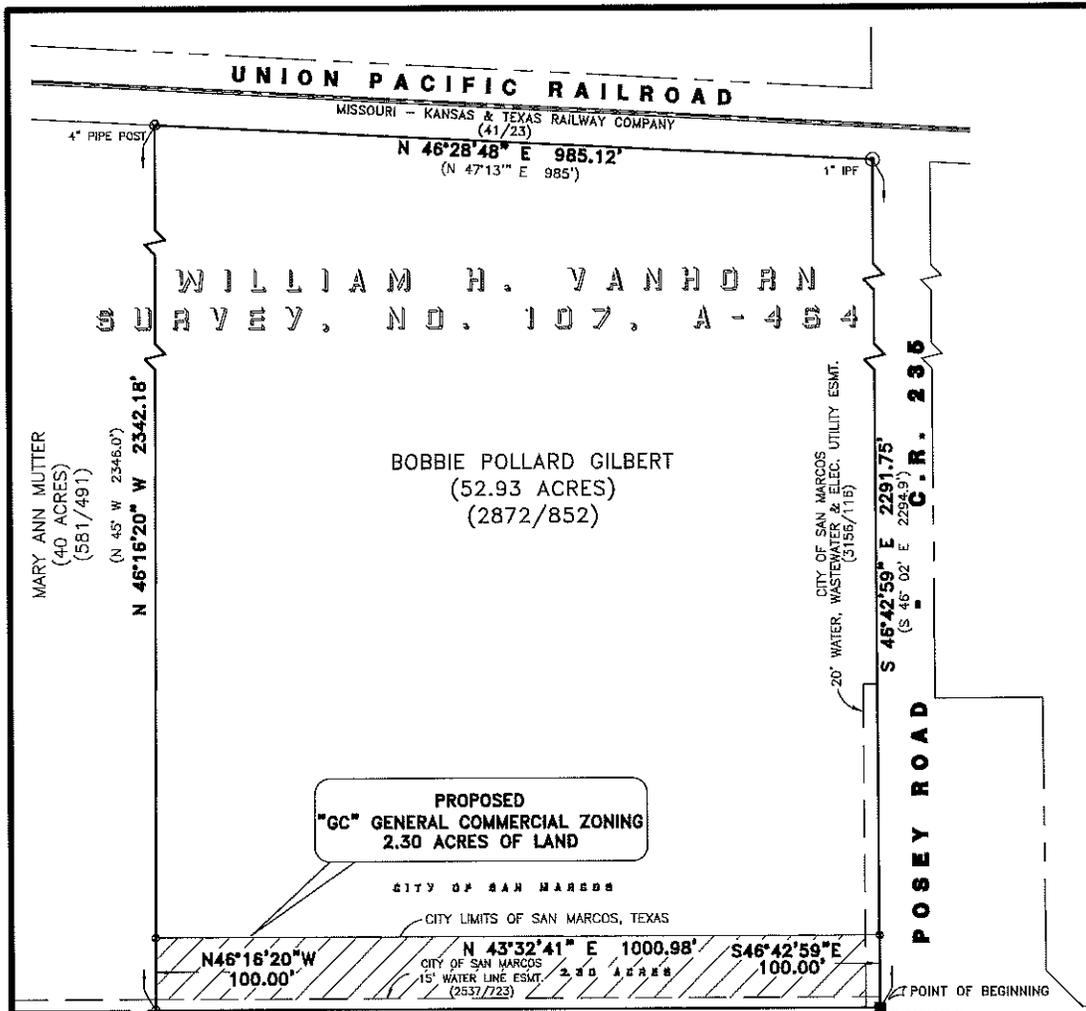
I, Richard H. Taylor, do certify that this description and associated exhibit were prepared this date from a survey performed under my direction during March 2015, and is true and correct to the best of my knowledge and belief.



Richard H. Taylor
Registered Professional Land Surveyor
No. 3986 State of Texas
Attachment: Drawing of 2.30 Acre Tract
Job: 15-5588

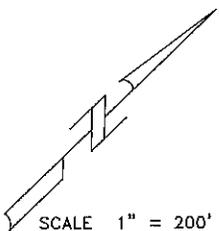
03/30/16
Date





LEGEND

- ⊙ IRON PIPE FOUND
- IRON ROD FOUND
- IRON ROD SET
- ▲ PK FOUND
- ⊕ CALCULATED CORNER
- ⊕ FIRE HYDRANT
- ⊕ WATER METER
- ⊕ WATER VALVE
- ⊕ POWER POLE
- //— WOOD FENCE
- x— CHAIN LINK FENCE
- x— WIRE FENCE
- E— OVERHEAD ELEC. LINE
- G— DOWN GUY
- P.U.E. PUBLIC UTILITY EASEMENT
- D.E. DRAINAGE ESMT.
- B.L. BUILDING LINE
- (BRG.-DIST.) RECORD CALL
- CONCRETE MONUMENT
- ⊕ FENCE CORNER POST
- ⊕ ELECTRIC METER
- ⊕ GAS METER
- ⊕ TELEPHONE PEDESTAL
- ⊕ LIGHT POLE
- ⊕ CLEANOUT
- P.O.B. POINT OF BEGINNING
- P.O.T. POINT OF TERMINATION



NOTES:

- 1) THE SURVEY OF THE PROPERTY SHOWN HEREON WAS MADE WITHOUT BENEFIT OF CURRENT COMMITMENT FOR TITLE AND THE PROPERTY MAYBE SUBJECT TO ADDITIONAL RESTRICTIONS, COVENANTS, EASEMENT AND RIGHT-OF-WAYS OF RECORD, NOT SHOWN HEREON.
- 2) ACCORDING TO THE NATIONAL FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO. 48209C0478F, DATED SEPTEMBER 2, 2005, THIS PROPERTY IS LOCATED IN ZONE "X", WHICH IS AN AREA OF MINIMAL FLOODING OUTSIDE THE 100-YEAR FLOODPLAIN.
- 3) BEARING BASIS DERIVED BY GPS MEASUREMENTS ADJUSTED BY HARN (HIGH ACCURACY REFERENCE NETWORK) & PROJECTED TO TEXAS STATE PLANE COORDINATES & NAD83.

MAP TO ACCOMPANY DESCRIPTION OF:

A 2.30 ACRE TRACT OF LAND SITUATED IN THE WILLIAM H. VANHORN SURVEY NO. 107, ABSTRACT NO. 464, HAYS COUNTY, TEXAS, SAME BEING A PORTION OF THAT CALLED 52.93 ACRE TRACT CONVEYED TO BOBBIE POLLARD GILBERT BY CORRECTION WARRANTY DEED RECORDED IN VOLUME 2872, APGE 852 OF THE OFFICIAL PUBLIC RECORDS OF HAYS COUNTY TEXAS, AND BEING MORE PARTICULARLY DESCRIBED BY METES AND BOUNDS ATTACHED:

I, RICHARD H. TAYLOR, DO HEREBY CERTIFY THAT THIS PLAT OF THE PROPERTY SHOWN HEREON, WAS PREPARED FROM A SURVEY PERFORMED UNDER MY DIRECTION AND SUPERVISION DURING MARCH 2016, AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF AND THAT SAID PROPERTY ADJOINS A PUBLIC RIGHT OF WAY.

Richard H. Taylor

03/30/16
DATE

RICHARD H. TAYLOR, RPLS
REGISTERED PROFESSIONAL LAND SURVEYOR
NUMBER 3986 STATE OF TEXAS
©:ASH & ASSOCIATES, L.L.C.



ASH & ASSOCIATES

SURVEYING - ENGINEERING - ARCHITECTURE
132 JACKSON LANE
SAN MARCOS, TEXAS 78666
(512) 392-1719
FAX (512) 392-1928
ashandassociates.net
Surveying: 100847-00
Engineering: F-13483
Architecture: TX20240
"SERVING THE COMMUNITY OF TEXAS"

ZONING CHANGE APPLICATION

City of San Marcos

	APPLICANT	OWNER
Name:	GUNNERSON OUTDOOR ADVERTISING	BOBBIE GILBERT
Mailing Address:	1801 IH 35 SOUTH SAN MARCOS, TX 78666	15 TIMBERCREST SAN MARCOS, TX 78666
Telephone No.:	512-396-5353	512-396-2313
E-mail address:	leasing@gunnersonadvertising.com	

PROPERTY DESCRIPTION:

Street Address: 202 POSEY ROAD

Subdivision: _____ Block: _____ Lot(s): _____

Other Description (if unplatted) 6.89 ACRES

* a metes and bounds description is required if property is a partial lot or is not platted

Appraisal District Tax ID No.: R 18638 Acres: 50.643

Lien Holder(s) - for notification purposes:

Name: _____

Mailing Address: _____

(If more than one lien holder, please provide information on a separate page)

A certificate of no tax delinquency must be attached to this application

ZONING CHANGE INFORMATION:

Zoning Designation: Current: AG Requested: GC

Master Plan Designation: _____ Land Use Map Amendment Required? _____

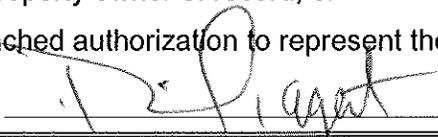
Present Use of Property: AGRICULTURAL

Desired Use of Property/Reason for Change: BILLBOARD SIGN

I certify that the information in this application is complete and accurate.

I am the property owner of record; or

I have attached authorization to represent the owner, organization, or business in this application.

Signature:  Date: 3-17-16

Printed Name: RICHARD MCDANIEL

APPLICATION FOR CITY OF SAN MARCOS ZONING CHANGE

APPLICATION FOR CITY OF SAN MARCOS ZONING CHANGE

FEE INFORMATION:

Fee Schedule:

Zoning Change to MF 12, 18, 14	\$1,015 plus \$51 acre (\$3,000 maximum) + \$11.00 Technology Fee
Zoning Change to all other categories	\$762 plus \$51 acre (\$2,000 maximum) + \$11.00 Technology Fee
Zoning variance/Special Exception	\$609 + \$11.00 Technology Fee
Renotification fee	\$77 + \$11.00 Technology Fee

APPLICATION PROCESS:

Please be advised that this is a 2-3 month process. The Planning and Zoning Commission will conduct a public hearing to consider your request. Prior to the hearing, the City will mail notices to all property owners within 200 feet of the subject tract, to the listed applicant and property owner, to any lien holders, and to the appropriate neighborhood representative. A sign advertising the change will also be placed on the property by the City.

At the public hearing the applicant, or a representative for the applicant, should be present to answer any questions the Commission may have. Failure to appear could result in your request being tabled or denied. Those in support of the request and those in opposition will be given an opportunity to speak. Following the close of the public hearing, the Planning and Zoning Commission will make a recommendation to either approve or deny the request.

This recommendation is then forwarded to City Council for their action. A notice is published in the newspaper 15 days prior to their hearing. City Council will conduct a public hearing and either adopt an Ordinance to approve the change or deny the request. You will be notified by mail of the date of the City Council public hearing. If an ordinance is adopted, at least one further meeting is required to give Council an opportunity to reconsider the request. If there is no reconsideration, the process is complete. If there is reconsideration, a third reading of the ordinance would be required for approval.

To be completed by Staff:

Property is located in: Edwards Aquifer Recharge Zone Historic District River Corridor
 Concurrent Land Use Amendment is Required: Yes No

Meeting Date: _____ Deadline: _____ Accepted By: _____ Date: _____

**AGREEMENT TO THE PLACEMENT
OF ZONING NOTIFICATION SIGNS**

The City of San Marcos Land Development Code provides that notification signs shall be placed on any property that is the subject of a zoning change, zoning variance, or conditional use permit. The signs shall be placed on the property by the Development Services-Planning staff prior to the 10th day before the scheduled public hearing based on the following criteria:

- Signs shall be placed on each street for property having multiple street frontages
- Signs shall be placed in a visible, unobstructed location near the front property line

Signs shall remain in place until final action is taken on the application, unless the case is formally withdrawn by the applicant prior to a final decision. Staff will remove the signs.

It is the responsibility of the applicant to periodically check the sign locations to verify that signs remain in place and have not been vandalized or removed. It is the responsibility of the applicant to immediately notify the Development Services-Planning Department of any missing or defective signs.

It is unlawful for a person to alter any notification sign, or to remove it while the case is pending. However, any removal or alteration that is beyond the control of the applicant shall not constitute a failure to meet notification requirements.

I have read the above statement and agree to allow the placement of notification signs as required on the project covered by the attached request. The Development Services-Planning staff has my permission to place these signs on my property. I will notify City staff if the sign is damaged or removed.

[Handwritten Signature]
Signature of Applicant

2/27/2015
Date

FOR STAFF USE ONLY:

Sign (s) were placed by staff on _____ by _____

Sign (s) were removed by staff on _____ by _____

October 8, 2015

I certify that I, Bobbie Gilbert, have given Ash and Associates authorization to represent me in the matter of Application for Preferred Scenario Map Amendment for my property located on the west side of IH-35, being on the south side of Posey Rd. (Property ID# R18639)

Property Owner

Signature: Bobbie P. Gilbert Date: 10-14-2015



Legislation Text

File #: PDD-15-02 (2), **Version:** 1

AGENDA CAPTION:

PDD-15-02 (Lindsey Hill Mixed Use) Hold a public hearing and consider a request by Guadalupe Re, LLC., for a zone change from “P” Public District to “PDD,” Planned Development District with a base zoning of “MU” Mixed-Use, on Block 4, Lindsey and Harvey Addition (4.83 +/- acres). The property is generally located at the northwest corner of W. Hutchison Street and Moore Street. (B. Melland)

Meeting date: April 12, 2016

Department: Planning and Development Services

Funds Required: N/A

Account Number: N/A

Funds Available: N/A

Account Name: N/A

CITY COUNCIL GOAL: Community Wellness/Strengthen the Middle Class

COMPREHENSIVE PLAN ELEMENT(s): *Economic Development; Land Use; Neighborhoods*

BACKGROUND:

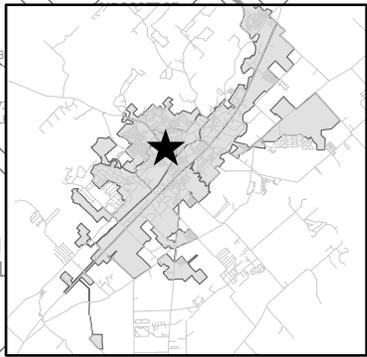
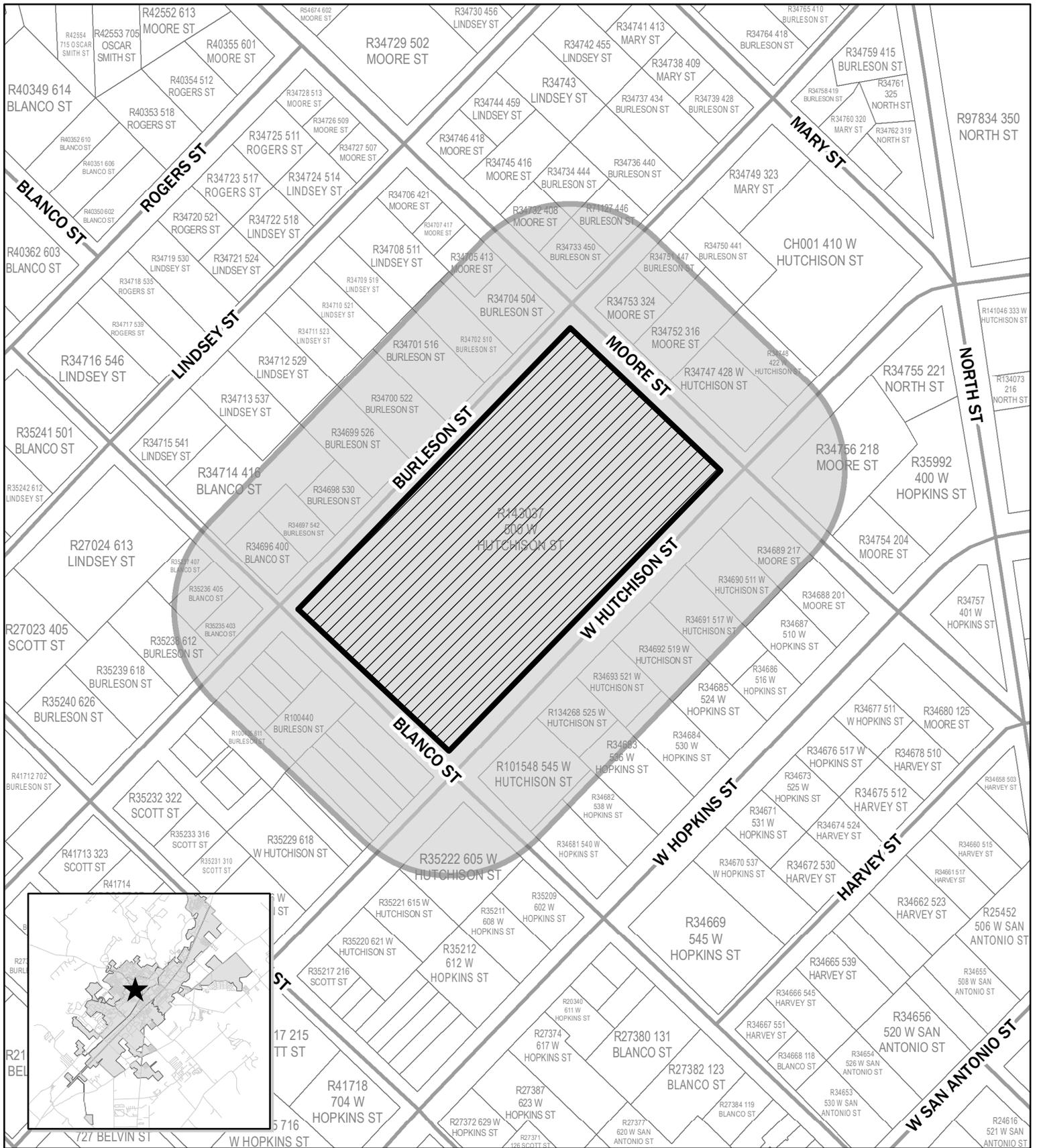
This is a request by Guadalupe Re., LCC for approval of a “PDD”, Planned Development District on the former site of the Lamar School at the northwest corner of West Hutchison Street and Moore Street. This PDD is proposing a base zoning classification of “MU” Mixed-Use District to facilitate the Mixed-Use Development referred to as “Lindsey Hill”. The proposed mix of uses for the project consists of a Hotel, Multi-Family Residential, Retail, a Restaurant, a parking garage, and the potential for professional offices. Several open spaces are also proposed in the Lindsey Hill PDD that employ the site’s natural features.

The Lindsey Hill PDD allows for flexibility in land use through modification from several standards, and also provides for enhancements and amenities above those which are typically required by the San Marcos Land Development Code (LDC). Noteworthy standards within the proposed PDD that are modified, are height, setback, residential density, impervious coverage, and parking. Noteworthy standards that provide for enhancements and amenities within the PDD, are new streetscapes surrounding the entire project site, the proposed public spaces, and a number of architectural guidelines.

This request was previously considered at the March 22, 2016 Planning and Zoning Commission meeting, at which time it was tabled until the April 12, 2016 meeting. Staff presents this request to the Planning and

File #: PDD-15-02 (2), **Version:** 1

Zoning Commission with a recommendation of **approval with conditions.**



PDD-15-02
Lindsey Hill Planned Development District

500 W Hutchison St
Map Date: 3/10/2016

-  Site Location
-  200 ft. Buffer



This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.



PDD-15-02
Lindsey Hill Planned Development District
500 W Hutchison St
Map Date: 2/19/2016

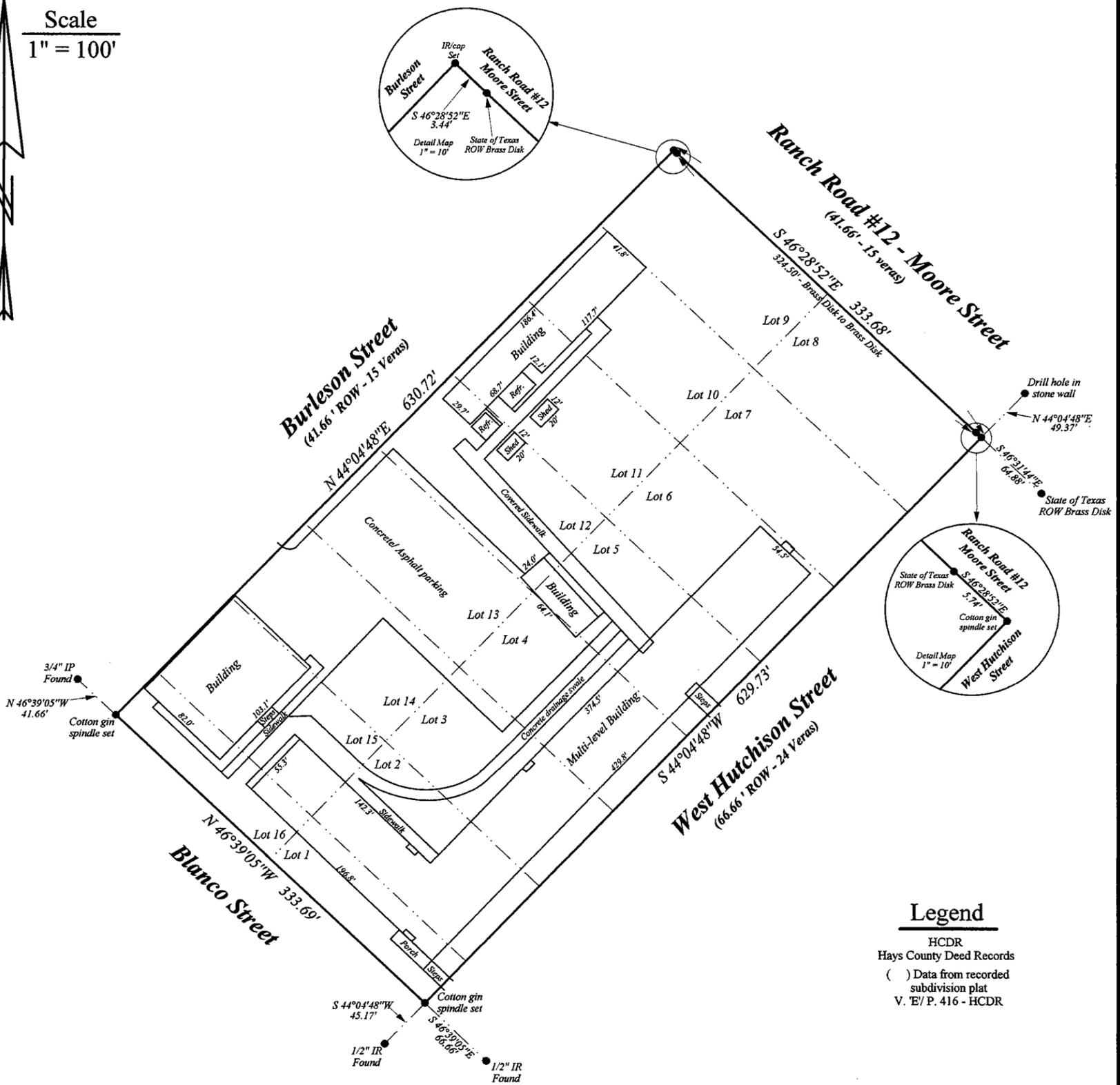
 Site Location



0 165 330 660 Feet

This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.

Scale
1" = 100'



Legend

- HCDR
- Hays County Deed Records
- () Data from recorded subdivision plat
- V. E/P. 416 - HCDR

Surveyor's Notes: All bearings and distances are based upon Texas State Plane Coordinate System, Texas South Central Zone, NAD 1983 datum. Block 4 contains 4.8275 acres, 210,284.07 sq. ft..

PLAT SHOWING SURVEY OF BLOCK 4, LINDSEY AND HARVEY ADDITION, A SUBDIVISION IN HAYS COUNTY, TEXAS, ACCORDING TO THE PLAT RECORDED IN VOLUME E, PAGE 416, HAYS COUNTY DEED RECORDS.

FOR: Guadalupe RE, LLC

GF: 14-00869-SM/ Corridor Title

DATE: September 18, 2014

ADDRESS: 500 West Hutchison Street, San Marcos, Texas.

The undersigned does hereby certify that this survey was made on the ground of the property legally described hereon and is essentially correct and that there are no visible discrepancies, encroachments, overlapping of improvements, visible utility easements or roadways, except as shown and that said property has access to a public road. Only those plats with a red surveyor's seal and red signature shall be deemed reliable and authentic.

[Signature]
Ronald D. Hayes, Registered Professional Land Surveyor, No. 5703

According to the scaling of Map Panel 0389F of the September 2, 2005 insurance rate map for the County of Hays, Texas, the property described hereon is in the Zone "X" of the flood hazard area and determined to be outside the 100 year flood zone.



**HAYES SURVEYING
 202 SUNFLOWER DRIVE
 KYLE, TEXAS 78640
 512-268-4813**

Zoning Change PDD-15-02 500 West Hutchison



Summary: Guadalupe Re, LLC is requesting a zoning change from “P” Public District to “PDD” Planned Development District with a base zoning classification of “MU”, Mixed-Use, on Block 4, Lindsey and Harvey Addition (4.83 +/- acres). (B. Melland)

**Applicant/
Property Owner:** Guadalupe Re, LLC.
1519 Spruce Street
Philadelphia, PA 19102

Notification: Personal notification mailed on April 1, 2016

Response: Thirty-six (36) written responses – two (2) within 200 foot notification area

Property/Area Profile:

Legal Description: A tract of land containing 4.8275 acres, being all of Block Four (4), of the Lindsey and Harvey Addition, a subdivision in Hays County, Texas according to the plat recorded in Volume “E”, page 416 of the Hays County Deed Records.

Location: Generally located at the northwest corner of West Hutchison Street and Moore Street.

Existing Use of Property: Partially developed; currently vacant.

Proposed Use of Property: Mixed Use: Hotel, Multi-Family Residential, Retail, Office, and Restaurant.

Intensity: The property is located within an Area of Stability.

Existing Zoning: “P”, Public District

Historic District: None

Proposed Zoning: “PDD” – Planned Development District; “MU” Mixed-Use base Zoning.

Utility Capacity: Limited utility capacity for proposed intensity of use.

Area Zoning & Land Use:

	Zoning	Existing Land Use	Adjacent Historic District
North	SF-6 & MU	Single-Family & Fraternity House	Lindsey-Rogers
South	TH, MF-24, MF12, MU NC	Townhome, Multi-Family, Fraternity House,	Hopkins Street
East	MF-18, MF-12, P	Multi-Family, Retail	N/A
West	TH, MF-12	Multi-Family, Townhome	Belvin Street Burleson

Case Summary

This is a request by Guadalupe Re., LCC for approval of a “PDD”, Planned Development District on the 4.83 +/- acre tract of land at the northwest corner of W. Hutchison Street and Moore Street. Most recently, the property is the former site of the Lamar School. Prior to, it was the historic site of the Coronal Institute. This PDD is proposing a base zoning classification of “MU” Mixed-Use District and is intended to facilitate the Mixed-Use Development referred to as “Lindsey Hill”.

The proposed mix of uses for the project consists of a Hotel, Multi-Family Residential, Retail, and a Restaurant, as well as an on-site Parking Garage to handle the project’s parking demand. This PDD also includes the option to develop a portion of the multi-family section for office use, depending on market demand. The project proposes to demolish most of the existing structures on the property with exception of the gym and auditorium buildings located along the Blanco Street frontage. They will be repurposed for the use of the hotel. New buildings will vary from 1 to 5 stories, depending on the surrounding neighborhood context. Setbacks will vary depending upon building height, topography and land use.

Additionally, the project proposes to preserve all existing specimen trees on the property with tree canopy over 30 feet; around which the site plan and building configurations have been designed. Several open spaces are also proposed in the Lindsey Hill PDD which utilize the sites natural features and engages the public frontage and private uses on the property. Among the open space proposed is “Poets Corner”, at the corner of West Hutchison Street and Moore Street, which may include an amphitheater.

The Lindsey Hill PDD includes modifications to several standards, while also providing for enhancements and amenities above that which is typically required by the San Marcos Land Development Code (LDC). Noteworthy standards within the proposed PDD that allow *flexibility* are those for height, setback, residential density, impervious coverage, and parking. Noteworthy standards that provide for *enhancements and amenities* within the PDD, are new streetscapes surrounding the entire project site, the proposed public spaces discussed above, and a number of architectural guidelines which respond to the diverse uses surrounding the project site. These standards regarding flexibility and enhancement are further summarized below. It should also be noted that to a large extent, staff has negotiated with the developers to limit variances to specific locations and/or circumstances, rather than through “blanket” application across the entire project site.

Planning Department Analysis

The proposed PDD was reviewed according to Section 4.2.6 “Planned Development Districts” within the City of San Marcos Land Development Code (LDC). More specifically, Section 4.2.6.1 states:

“The purpose of an overlay planned development zoning district (“PD District”) is to provide for the development of land as an integral unit for single or mixed use in accordance with a PD Concept Plan that may include uses, regulations and other requirements that vary from the provisions of other zoning districts. PD districts are intended to implement generally the goals and objectives of the City’s Comprehensive Plan. PD districts are also intended to encourage flexible and creative planning, to ensure the compatibility of land uses, to allow for the adjustment of changing demands to meet the current needs of the community, and to result in a higher quality development for the community than would result from the use of conventional zoning districts.”

LAND USE:

Consistent with the above stated purpose, the proposed PDD plans for a mixed-use development as illustrated in Figure 1-1 within the PDD Standards Book and as summarized below.

- **Hotel:** Located primarily along the Blanco frontage and a portion of the Hutchison frontage. The hotel component plans for a maximum of 120 rooms and a maximum of 17,000 sq.ft. of additional associated space to be used for meeting rooms and additional amenities. This includes the old Lamar School gymnasium and auditorium buildings, which are proposed to remain.
- **Retail:** Proposed to front the Hutchison and Moore Street frontages. There will be a maximum of 17,000 sq.ft. of retail space at these locations. This square footage includes the potential for an 8,500 sq.ft restaurant.

- Multi-Family: Proposed to front the Burleson and Hutchison Street frontage. The multi-family component will contain a maximum of 164 units which equates to 34 units/acre. The “MU” Mixed-Use District allows up to 5.5 units/acre. Total bedroom count be limited to a maximum of 226.
- Office: Proposed as an alternative scenario within the area planned for multi-family along the Hutchison Street frontage. Limited to a maximum of 27,000 sq.ft. of office space. In the event that the office space is developed, the multi-family unit maximum will be reduced to 144 units and 199 bedrooms, in order to ensure for appropriate parking relative to each use.

BUILDING HEIGHTS AND SETBACKS:

Building heights will vary from one to five stories depending on location. Buildings along Hutchison Street will be up to five stories. Setbacks along Hutchison will be 16 feet. The existing buildings to remain along Blanco Street are generally two stories and are setback 17 feet. Buildings along Burleson Street will graduate from two stories at the corner of Blanco and Burleson Streets to an eventual height of five stories at the corner of Burleson and Moore Streets. This height graduation is intended to compliment the first three properties from the intersection at Burleson and Blanco Streets which are single-family residential. Buildings along Moore Street will be five stories with a small portion being one story. Setbacks along Burleson and Moore Streets will generally be between eight and 10 feet.

PARKING:

A parking garage is also proposed on-site to handle the majority of the project’s parking demand. The garage will be no more than four stories above grade and will be entirely encapsulated by “liner units” to provide architectural sensitivity to adjacent properties and pedestrian ways. As illustrated in Figure 1-7 of the PDD Standards Book, the parking garage will take access from Burleson Street. The increased traffic demand anticipated to be generated by the parking garage on Burleson Street was analyzed. Staff recommends that the east-bound lane of Burleson be converted to a right-turn only lane at the intersection with Moore Street. Required parking in the Lindsey Hill PDD will be 405 spaces without the office component, and 450 spaces with the office component. Approximately 85% of the required parking must be located on-site, within the garage. The remaining 15% may be located off-site, which includes abutting on-street parking. In addition to the parking garage, the PDD proposes for additional vehicle loading and access at the hotel porte-cochere and the loading dock and waste management access point along the Blanco Street frontage, as well as the short-term vehicle loading zone on Burleson Street.

OPEN SPACE:

Open Space at Lindsey Hill will consist of three open spaces described below. Each will have varying levels of potential engagement by the general public. All open space in the Lindsey Hill PDD is proposed to remain privately owned, with no parkland being dedicated to the public or city. Therefore, the project will be required to pay a parkland fee-in-lieu amount proportional to final number of multi-family units that are constructed. This project was presented at the Parks Board at their March 17, 2016 meeting.

- Poet’s Corner will be located at the corner of Hutchison and Moore Streets and may include the construction of an amphitheater and other recreational amenities. It will be open to the public, but may be restricted or reserved by the property owner.
- The Grove will be surrounded by the hotel and multi-family uses. It will also be generally open to the public, but may restricted or reserved by the property owner.
- The Courtyard will be a private open space reserved for Lindsey Hill residents.

PUBLIC IMPROVEMENTS:

The Lindsey Hill project is also proposing to reconstruct all abutting public streetscapes. Proposed streetscape enhancements generally includes improvement and addition of on-street parking, curb extensions, the creation of a “street-tree” zone, 6 foot minimum sidewalk widths, and landscape buffers between the sidewalk and the building façade. These street improvements are above the street standards currently required by the City. They are intended to enhance the neighborhood and further integrate the Lindsey Hill project into the diverse surrounding area. Additionally, the electrical facilities necessary for the development will be placed underground and an upgraded waterline will be installed to provide necessary fire-flow for the development.

PHASING:

Also consistent with the purpose of the PDD, is the potential for phasing the Lindsey Hill project, though it is the stated intent of the developers to complete the project in one phase. If it is necessary to be phased, Phase 1 of development will include demolition of all existing structures except the existing gym and auditorium; and subsequent construction of the parking garage, the multi-family and retail components surrounding the garage, and Poet's Corner. All proposed streetscapes will also be constructed in Phase 1. Phase 2 will consist of the hotel and remaining multi-family. A possible Phase 3 would consist of the completion of the retail component along the Moore Street frontage.

MODIFICATIONS AND ENHANCEMENTS

Also consistent with the purpose of the PDD, there are modifications to several development standards, as well as enhancements and amenities that are intended to yield higher quality development. These modifications and enhancements are summarized as follows, with specific reference to the corresponding section of the attached PDD Master Plan Document. For a full detailed analysis of specific modifications and enhancements to the LDC, please see Section 6.2 of the attached PDD Standards Book.

NOTE: Items underlined below were not mutually agreed upon between the City and the applicants. These items are "redlined" within the attached PDD Document.

Modifications:

- Decrease in required parking per the LDC. (Section 3.4 "Parking")
- Increased overall impervious coverage for increased density. (Section 1.1 "Site Coverage")
- Increased hillside impervious coverage for increased density. (Section 1.1 "Site Coverage")
- Decreased setbacks to allow for more flexible site design. (Section 5.1 "Building Setbacks")
- Additional permitted land uses for a greater mix of uses. (Section 6.11 "Land Use Matrix")
- Increased Residential Density. (Section 1.13 "Multi-Family Housing")
- Increased building height to allow for increased open space. (Section 5.3 "Building Height")
- Decrease in required percentage of landscaping. (Section 3.2 "On-Site Landscaping")
- Pedestrian gates permitted at various locations. (Section 3.11 "Pedestrian Access")
- Modifications to several signage standards. (Section 3.6 "Signage")
- Encroachment and maneuvering allowed in ROW. (Section 3.5 "Vehicular Loading and Access")
- Porches permitted outside primary building footprint. (Section 5.8 "Balconies and Porches")
- Additional permitted architectural materials. (Section 5.10 "Materials")

Enhanced Development Standards:

- Prohibition of "rent-by-the-bedroom" leasing practices. (Section 1.13 "Multi-Family Housing")
- Additional limits to Multi-family leasing standards. (Section 1.13 "Multi-Family Housing")
- Preservation of existing trees with canopy over 30'. (Section 3.2 "Tree Preservation")
- Parking garage entirely screened. (Section 1.16 "Parking Garage")
- Reconstruction of all surrounding Streetscapes. (Section 4.0 "Street Design and Amenities")
- Addition of on-street parking. (Section 3.42 "On-Street Parking")
- Development of two privately-owned public spaces. (Section 2.2 "Description of Open-Spaces")
- Enhanced Signage Standards (Section 3.6 "Signage")
- Additional architectural standards regulating building frontages. (Section 5.2 "Frontage")
- Contextual Height Stepdown requirements. (Section 5.4 "Contextual Height Stepdown")
- Architectural expression requirements. (Section 5.5 "Expression Requirements")
- Additional transparency requirements. (Section 5.6 "Street Level Glazing and Transparency")
- Requirements for retaining walls at street level. (Section 5.9 "Retaining Walls at Street Level")

CONFORMANCE WITH COMPREHENSIVE PLAN ELEMENTS

Planning staff also reviewed the proposed PDD with the City’s Comprehensive Plan, *Vision San Marcos*. Staff finds that the draft submittal for the proposed PDD conforms to the Comprehensive Plan Elements and the following Comprehensive Plan Element “Goals.”

Economic Development:

- *Goal 3: Emerging markets and industry relationships that generate quality entrepreneurial and employment opportunities.*

Land Use:

- *Goal 1: Direct growth compatible with surrounding uses.*
- *Goal 2: High Density mixed-use development and infrastructure in the activity nodes and intensity zones, including the Downtown area supporting walkability and integrated transit corridors.*

Neighborhoods:

- *Goal 3: Diversified housing options to serve citizens with varying needs and interests.*

CONFORMANCE WITH PREFERRED SCENARIO MAP

Planning staff also reviewed the proposed PDD with the City’s Future Land Use Plan, better known as the “Preferred Scenario.” The proposed PDD project site is located within an *Area of Stability* on the Preferred Scenario Map. Additionally, the project site is classified by the Comprehensive Plan as “Redevelopment/Infill.” Redevelopment/Infill projects that are located within Areas of Stability generally allow for “single-family-uses with accessory buildings, Bed & Breakfast (5 rooms), Home Offices, Corner Neighborhood Retail and Restaurants – no drive through.”

The base zoning classification of “MU” Mixed Use District requested in this PDD, is a permitted zoning district within an Area of Stability. However, the Lindsey Hill PDD proposes uses and standards more intensive than those listed above. It is staff’s opinion, that given the proximity to downtown, the quality of the enhancements and amenities proposed, and the diverse existing surrounding zoning districts, land uses, and development patterns; this requested Planned Development District (**with staff redlines**), meets the intent of the Comprehensive Plan and the Preferred Scenario.

Evaluation		Neutral	Criteria (LDC 1.5.1.5)
Consistent	Inconsistent		
X			Change implements the policies of the adopted Master Plan, including the land use classification on the Future Land Use Map and any incorporated sector plan maps. <i>Yes, as previously discussed.</i>
		X	Consistency with any development agreement in effect <i>No development agreements are in effect for this property.</i>
X			Whether the uses permitted by the proposed change and the standards applicable to such uses will be appropriate in the immediate area of the land to be reclassified <i>The proposed PDD and underlying zoning would be meet the intent of the Comprehensive Plan and Preferred Scenario.</i>
X			Whether the proposed change is in accord with any existing or proposed plans for providing public schools, streets, water supply, sanitary sewers, and other public services and utilities to the area <i>The proposed PDD will provide electrical utility undergrounding and an upgraded water line.</i>
		X	Other factors which substantially affect the public health, safety, morals, or general welfare <i>None noted.</i>

Additionally, the Commission should consider:

(1) Is the property suitable for use as presently zoned?

Staff evaluation: The property is currently zoned "P" Public District. Some of the surrounding land uses are allowed in a "P" District, and some are not. "P" District allows for duplexes and townhomes, as well as fraternity houses. "P" District does not however allow for single family, multi-family, retail, or office.

(2) Has there been a substantial change of conditions in the neighborhood surrounding the subject property?

Staff evaluation: No.

(3) Will the proposed rezoning address a substantial unmet public need?

Staff evaluation: No

(4) Will the proposed rezoning confer a special benefit on the landowner/developer and cause a substantial detriment to the surrounding lands?

Staff evaluation: No

(5) Will the proposed rezoning serve a substantial public purpose?

Staff evaluation: A goal of the Comprehensive Plan is to support "High Density mixed-use development and infrastructure in the activity nodes and intensity zones, including the Downtown area supporting walkability and integrated transit corridors." This area is not within Downtown, but it is adjacent to it. This project provides for higher density mixed-use with walkability to Downtown, the University, and the CART's transit service (Monday-Friday, 7:00am - 6:00 pm.)

Staff provides this request to the Commission with a recommendation of approval subject to the following conditions:

1. That the attached PDD Master Plan Document is approved with staff redlined revisions.

Planning Department Recommendation:	
	Approve as submitted
X	Approve with conditions or revisions as noted
	Alternative
	Denial

The Commission's Responsibility:

To hold a discussion regarding the proposed zoning change. When the item is placed on the agenda for consideration, the Commission is required by law to hold a public hearing and receive public comment regarding the proposed zoning. After considering the public input, the Commission is charged with making a recommendation to the City Council regarding the request. The City Council will ultimately decide whether to approve or deny the zoning change request. The Commission's recommendation to the Council is a discretionary decision.

Prepared by:

Brandon Melland, AICP	Senior Planner	March 30, 2016
Name	Title	Date

LINDSEY HILL

San Marcos, Texas

Draft: 04/07/16

PDD Standards Book



Acknowledgements

Lindsey Hill Development Team: Guadalupe RE, LLC

David Lerman, Project Developer

Mark Berins, Project Developer and Project Manager

City of San Marcos, Texas

Shannon Mattingly, Director, Planning and Development Services

Kristy Stark, Assistant Director, Planning and Development Services

Brandon Melland, Senior Planner, Planning and Development Services

Kevin Burke, Economic and Development Projects Coordinator

Professional Consultants

Overland Partners – Architecture and Project Design

Samantha Whitney Schwarze, AIA

Michael Rey, AIA, Principal

James Andrews, RIBA, Principal

Architectural Engineers Collaborative – Structural Engineer

G. Charles Naeve, PE, Principal

Karina Tribble, PE, Senior Associate

D.A. Mawyer Land Surveying, Inc. – Topographic Survey

Dakota Mawyer, Project Manager

R-K Traffic Engineering, LLC – Transportation Consultant

James Schwerdtfeger, Principal

The Parking Advisory Group – Parking Consultant

Jerry Marcus, Principal

PKF Consulting USA – Hotel Development Consultant

Randy McCaslin, Senior Vice President

Zoning

This zoning applies to the property located at 500 W. Hutchison Street in San Marcos, Hays County, Texas, more particularly described by metes and bounds in Section 6.4 (the “Property” or the “Project Site”). The Property has been rezoned from “P” Public and Institutional District, to “PDD” Planned Development District, subject to a base zoning classification of “MU” Mixed Use District, as modified by the standards set forth within this document.

This PDD Standards Book (the “PDD Standards Book” or the “Book”) applies to all use of the Property and describes a mixed-use project that is sometimes referred to in this document as “Lindsey Hill” or the “Project.”

Except as varied by this PDD Standards Book, the Property and the Project shall be subject to all use and development standards applicable to the “MU” Mixed Use District (the “Base Regulations”) of the City of San Marcos Land Development Code (the “LDC”) and to the procedural requirements outlined in the LDC and in other San Marcos Codes. In the event of any conflict between the standards, allowances, or any other item outlined in this PDD Standards Book and any requirements outlined within the LDC, this PDD Standards Book shall govern and control.

Additionally, because complete site plan and building design has not yet occurred, the images, drawings, figures, and standards in this Book are not depictions of actual buildings, final site plan, or other final designs, but instead shall be considered the “Concept Plan”, as described by Section 4.2.6.4 of the LDC. In addition to other applicable health and safety ordinance requirements, each phase of the Project will be required to meet Emergency Access requirements (as defined by the International Fire Code) at all stages of development.

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2.2	Description of Open Spaces	
3.0	SITE DESIGN	Site Design
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1.0	PLAN & DESIGN		Plan & Design
1.1	Land Use		
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1.1 Land Use

This section provides an overview of the Lindsey Hill concept plan. Additional details and requirements are more specifically described elsewhere in this PDD Standards Book.¹

Site Layout

Figure 1-1 depicts the site layout of Lindsey Hill. Construction of Lindsey Hill shall conform substantially to *Figure 1-1*, except that in the event of any conflict between *Figure 1-1* and other more detailed depictions or standards within this Book, such other more detailed depictions and standards shall govern and control. Any deviations from what is described in this Book shall be subject to the provisions of Section 4.2.6.5 of the LDC regarding “Subsequent Development Applications.”

Site Coverage

Lindsey Hill is an urban, mixed-use complex. The Project will include buildings, a parking structure, sidewalks and other pathways, and various other elements. The base zoning of “MU” Mixed Use District allows a maximum site-wide impervious coverage of sixty percent (60%). Instead, pursuant to this PDD Standards Book, the maximum allowable impervious coverage at the Property shall not exceed eighty-five percent (~~85~~80%) of the Project Site’s total land area.

The PDD permits additional hillside impervious coverage in the locations specifically illustrated in Section 6.5 “Hillside Impervious Cover” of this Book. The maximum impervious coverages allowed within Section 6.5 were determined with respect to the proposed building footprints and streetscape improvements. Under the Base Regulations slope impervious cover is limited as follows: maximum thirty-five percent (35%) coverage for slopes with a gradient between fifteen and twenty-five percent (15-25%), and twenty percent (20%) coverage for slopes with a gradient greater than twenty-five percent (25%). Adjacent to the Project Site, in the downtown core, one hundred percent (100%) lot coverage is permitted, without regard to slopes. Additional hillside/slope impervious coverage may be considered or warranted based on final site design and materials used. Pursuant to existing City of San Marcos codes, the Project shall use appropriate erosion control measures during construction to minimize runoff on steep slopes.

¹ For visual ease and convenience, a separate larger format site layout has been submitted with this Book. It is for reference purposes only.

Density and Square Footage Summary

Lindsey Hill will contain a mix of multifamily residential, hotel, and retail. Alternatively, a portion of the multifamily section of the Project adjacent to Hutchison Street may be designed as “dual-use” space—so that if, during development, an office user expresses interest, that space can be completed as office. Alternatively, if no office user is secured, that space will be absorbed into the multifamily portion of Lindsey Hill. The uses permitted at the Project Site shall not exceed the following gross square footages and/or unit counts.

Multifamily Residential:

- Without office component: 164 units/226 bedrooms
- With Office 144 units/199 bedrooms

Hotel

- Guest rooms: 120 rooms
- Other Space: 17,000 sq.ft.

Office 27,000 sq.ft.

Retail Space 17,000 sq.ft

** Hotel Other space includes parking garage rooftop amenities (if shared with multi-family)*

***Note: Up to 8,500 SF of the Retail Space may be used as restaurant space.*

Figure 1-1: Site layout of Lindsey Hill.



Note: This drawing (or derivations of this drawing) appear at various places throughout this document. The purpose of this drawing is to depict the uses that will appear at the Project and the approximate building footprint locations where these uses will appear. These drawings do not reflect final committed design.

1.11 Open Spaces

The Project's areas for casual social engagement, relaxation, shared events, and experiences (spontaneous and programmed) include—in addition to the retail spaces described in this Book:

- Poet's Corner, a privately-owned public space generally available and open to public use at the Project owner's discretion (as more particularly described in this Book);
- The Courtyard, a private open space available exclusively to residents of Lindsey Hill's multifamily community;
- The Grove, a privately-owned open space available to the public at the owner's discretion, will function as the hotel's entertainment, gathering, special events, and outdoor enjoyment center; and
- Public street frontage adjacent to the Project Site that will foster interaction and social engagement among Lindsey Hill residents, tenants, neighbors, and community members.
- Pursuant to the LDC, a minimum of twenty percent (20%) of the Project Sites gross land area shall be devoted to open space.

See *Figure 1-2* for a general depiction of the locations of open spaces at the Project Site.



Figure 1-2: The approximate locations of Lindsey Hill's open (outdoor) spaces are highlighted in green. **Note:** This diagram is intended only to show the general location of open spaces at Lindsey Hill. Landscaping and other project requirements shall be regulated as otherwise described in this PDD Standards Book.

1.12 Retail Spaces

Figure 1-3 depicts the locations of ground floor retail spaces at the Project Site, which collectively will be a maximum of 17,000 square feet of leasable space for restaurant, food service, and retail occupancy. Surrounding the retail spaces will be patios, decks, and outdoor dining spaces that soften the retail edges and take advantage of tree canopies for a portion of shade needs.

While the developer desires to locate restaurant and food service options adjacent to Poet's Corner, dining options may be anywhere within the retail areas of Lindsey Hill.

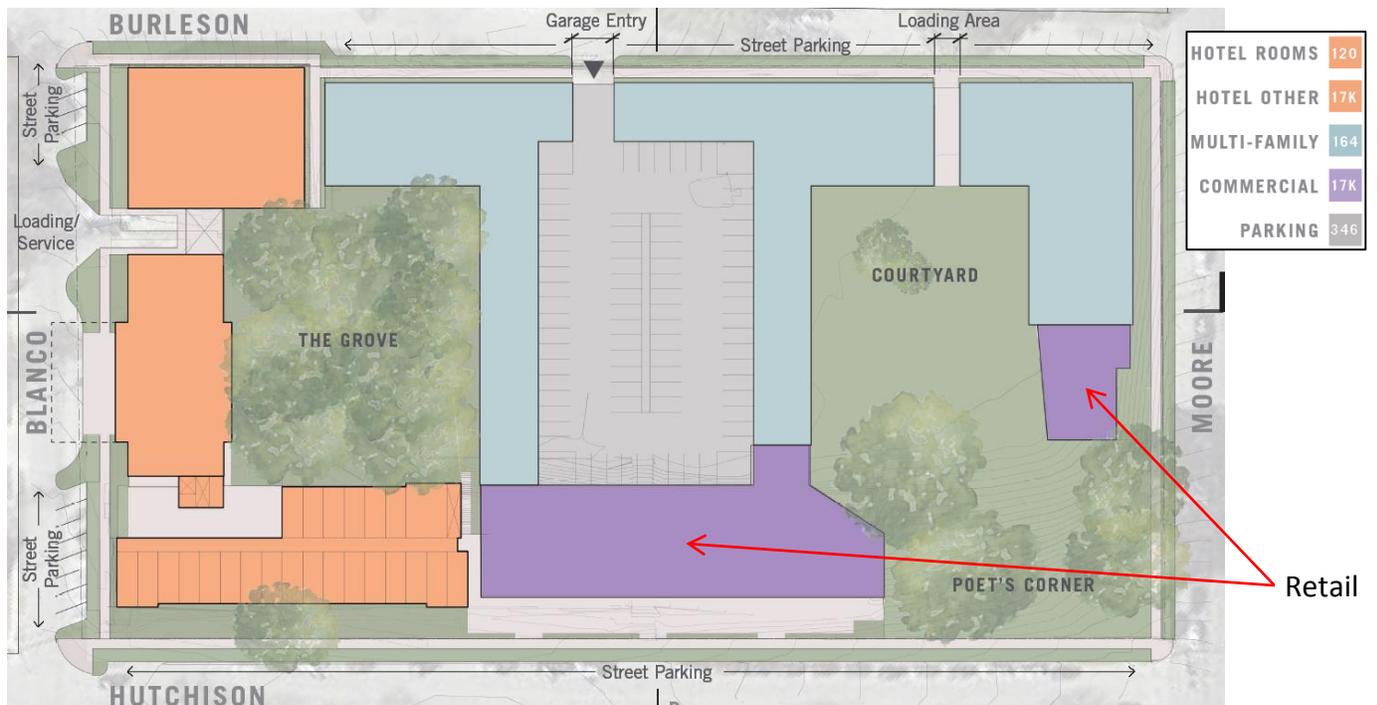


Figure 1-3: Ground floor retail spaces are depicted in purple.

1.13 Multifamily Housing

A combination of one, two, and three bedroom apartments will be developed within the multifamily portion of the Project in the area marked below in *Figure 1-4*. Without the office component the PDD permits a maximum of 164 multifamily units (34 units/acre) and 226 bedrooms. With the office component, the PDD permits 144 multi-family units (30 units/acre) and 199 bedrooms. Lindsey Hill’s multifamily apartments shall also be subject to the following restrictions and requirements:

1. Lindsey Hill’s multifamily landlord shall not be permitted to lease apartments on a “rent by the bedroom” basis.
2. Subject to the requirements of applicable state and federal law, as the same may change from time to time, Lindsey Hill’s multifamily landlord shall not be permitted to lease any single one-bedroom or two-bedroom apartment to more than two (2) unrelated individuals between the ages of eighteen (18) and twenty-two (22), and for three (3) bedroom apartments, Lindsey Hill’s multifamily landlord shall not be permitted to lease any single apartment to more than three (3) unrelated individuals between the ages of eighteen (18) and twenty-two (22). As used in this section, “unrelated” means individuals not related by blood, legal adoption, marriage, or conservatorship.
3. Lindsey Hill’s multifamily landlord shall not permit subleasing.
4. All leases shall require:
 - a. all occupants to be named within the tenant lease; and
 - b. at least one occupant to have a minimum five (5) year credit, property rental, or property ownership history.
 - c. Tenant leases shall include a statement informing tenants that Landlord shall not be responsible for tenant violations of any City of San Marcos codes, including for tenant violations of the requirements of this section. Tenant leases shall further state that in the event of any such violations, the City of San Marcos may issue directly to tenant notices of violations, which may include monetary penalties that tenant alone shall be responsible to pay.
5. Upon reasonable notice, the City of San Marcos shall be permitted to review tenant leases (limited to those sections of the leases pertaining to these lease requirements) at any time during normal business hours at Lindsey Hill’s leasing office.

In the event that there is a possible breach of the requirements of this section, the City of San Marcos shall notify: (i) the tenant; and (ii) Lindsey Hill’s multifamily landlord. So long as the tenant lease provides for the requirements of this section, no action shall be taken against Lindsey Hill’s multifamily landlord while the landlord is using its best efforts to cure any such

breach pursuant to legal and other remedies that may be available to the landlord.

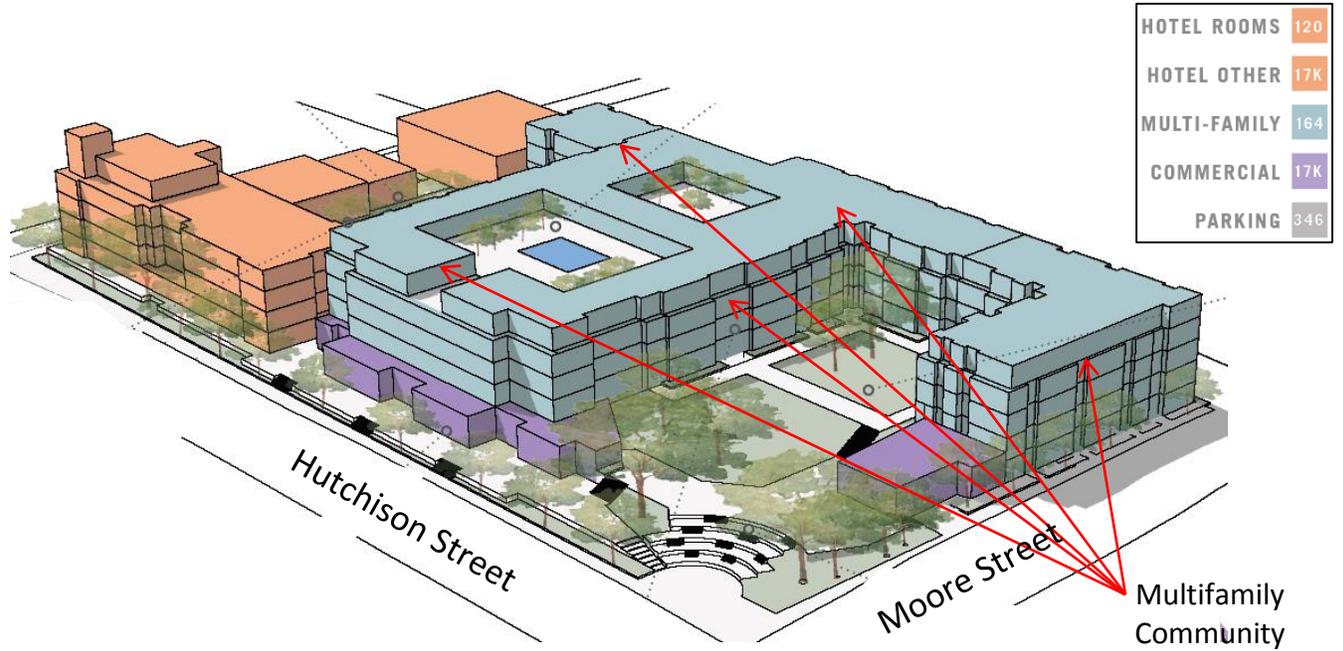


Figure 1-4: Multifamily housing is depicted in light blue.

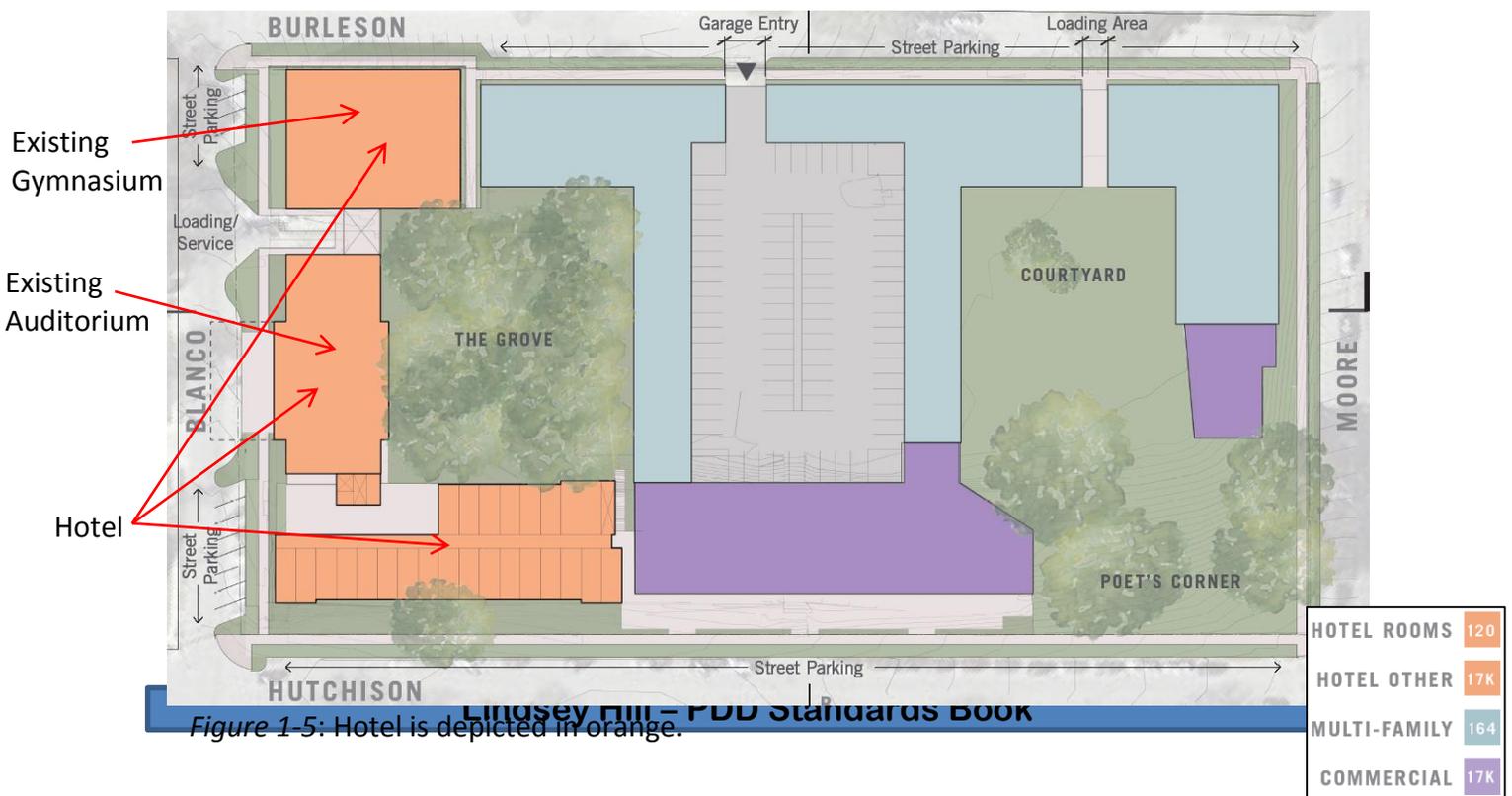
1.14 Hotel

Lindsey Hill will include a hotel containing 120 guest rooms (a mix of traditional rooms to accommodate short-term guests, together with larger suite-style and apartment-style rooms for executive and extended stay patrons). Hotel guest rooms shall not be rented to an individual, business, or entity, for periods exceeding 30 consecutive days within a 6 month period. In an effort to preserve some of the Project Site's history, the development team hopes to repurpose and reuse the existing gymnasium and auditorium buildings as part of the hotel facility. The locations of the hotel and the auditorium and gymnasium buildings are depicted below in *Figure 1-5*.

The hotel at Lindsey Hill shall be an establishment providing accommodations and meals, and other hotel-related services for travelers, tourists, and other guests. The hotel at Lindsey Hill shall remain a hotel and may not be converted for use as a student dormitory.

On the roof of the hotel may be amenities shared by the hotel and multifamily community, which may consist of a pool, pool deck, community space, terrace, green roof, gardens, solar panels, etc.

The square footage devoted to the guest rooms and associated corridors will be determined at the time of final design. All other hotel square footage, including the gymnasium, auditorium, hotel rooftop amenities, parking garage rooftop amenities (if shared with multifamily uses) and other establishments located within the hotel shall be limited to 17,000 square feet.



1.15 Office Space

As previously stated, a portion of the multifamily section of the Project adjacent to Hutchison Street may be designed as “dual-use” space—so that if, during development, an office user expresses interest, that space can be completed as office. Alternatively, if no office user is secured, that space will be absorbed into the multifamily portion of Lindsey Hill. If a portion of the multifamily component is developed as office space, the multifamily component shall not exceed 144 units and 199 bedrooms.

This “dual-use” space may contain a maximum of 27,000 square feet of office space and is depicted below in *Figure 1-6*. The office space will be strategically positioned above the Project’s main retail component in order to provide additional daytime activity to Lindsey Hill’s retail merchants, as well as additional activity to the more urban frontage and the adjacent public spaces.

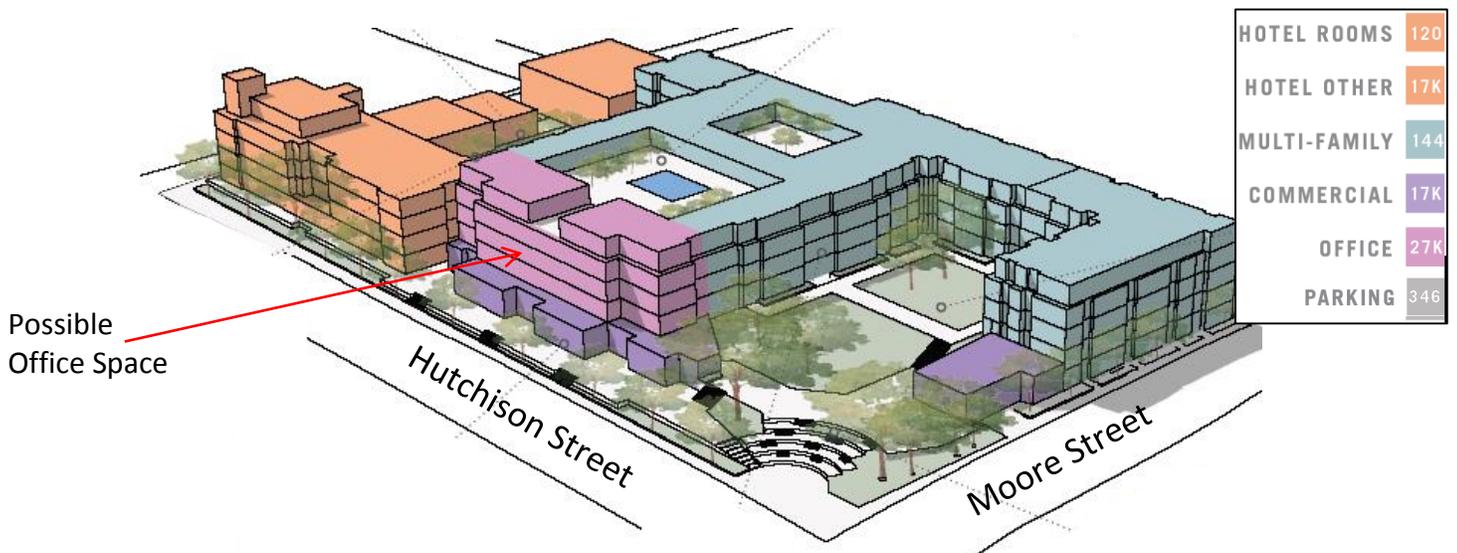


Figure 1-6: Possible office space is shown in pink. Note location is above retail activity and adjacent to public spaces.

1.16 Parking Garage

The parking garage structure will handle the majority of the parking needs of Lindsey Hill. The structure may include one or more below-grade parking levels and four or more above-grade parking levels (as depicted in *Figures 1-7, 1-8, and 1-9* below). On the roof of the parking garage may be amenities shared by the hotel and multifamily community, which may consist of a pool, pool deck, community space, terrace, green roof, gardens, solar panels, etc.

All parking levels, including the rooftop, shall be wrapped by apartment “liner units” (and/or office space, if applicable) surrounding its entirety, so that no portion of the parking structure is visible from street-level view. However, as illustrated in *Figure 1-9*, an opening is permitted at the Hutchison frontage.

Note: Parking garage vehicular access point may be positioned anywhere between these green lines and may be up to 36’ wide (plus adjacent pedestrian exit).

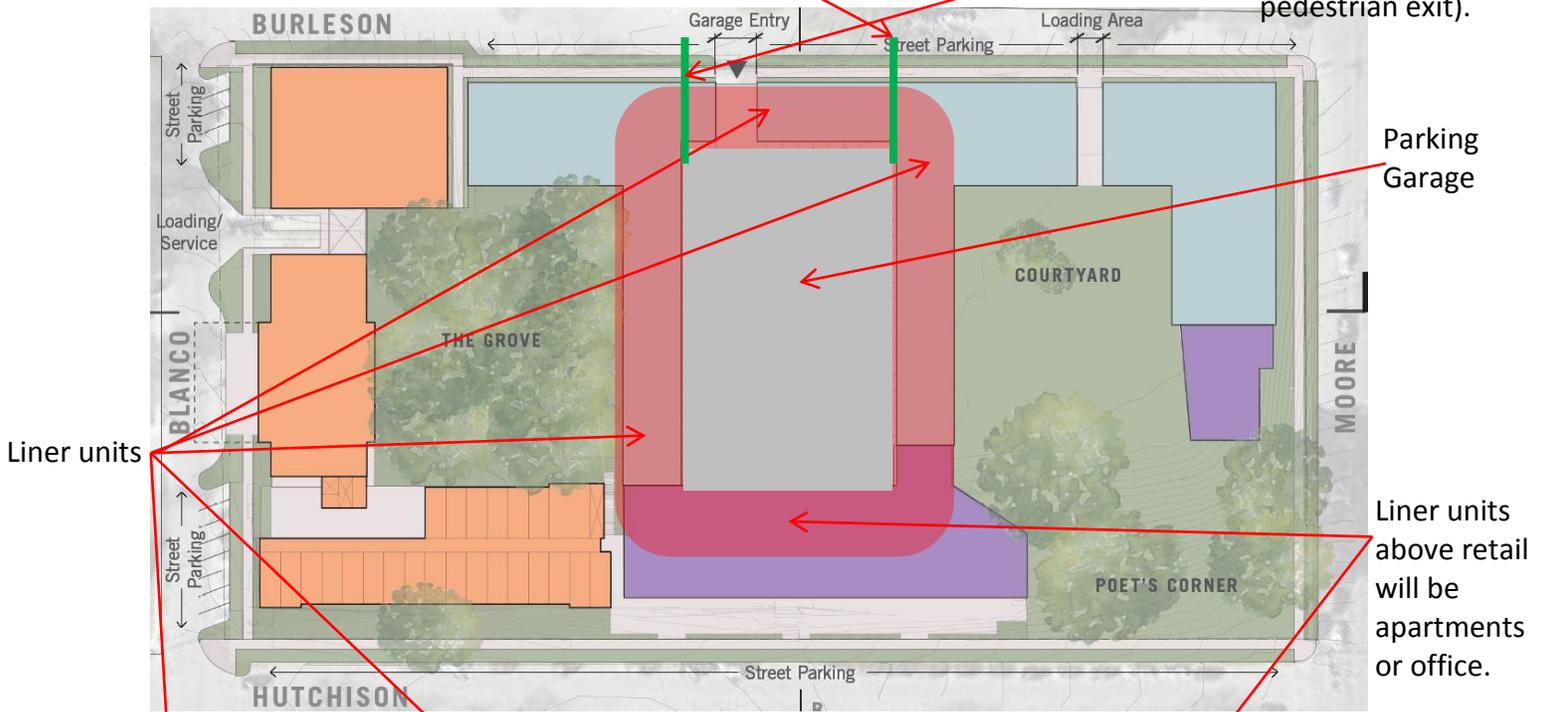


Figure 1-7: Location of parking garage is depicted in gray. Liner units surrounding garage are depicted in the red ringed area and include units above the retail space.

HOTEL ROOMS	120
HOTEL OTHER	17K
MULTI-FAMILY	164
COMMERCIAL	17K
PARKING	346

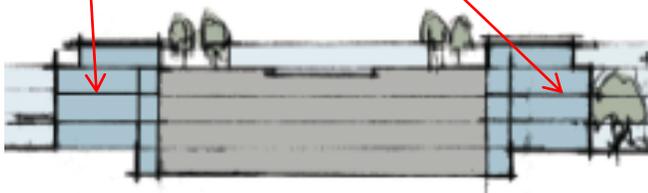


Figure 1-8: Section view of garage depicting one level below grade and four levels above grade. Note liner units in light blue surrounding garage.

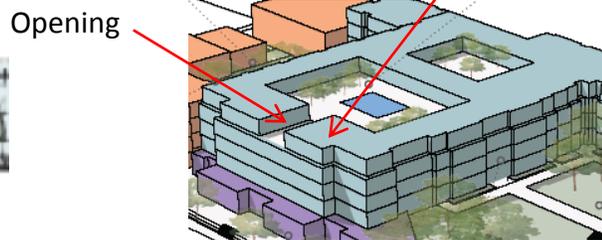


Figure 1-9: Garage will be surrounded by liner units hiding it from view. Garage entrance will be visible only from Burleson Street.

1.2 Phasing

The Project is intended to be constructed in a single phase and in a manner that is consistent with the Concept Plan (but with allowances for deviation pursuant to LDC Section 4.2.6.5(b)). However, economic conditions and operational considerations may dictate otherwise. In the event that Lindsey Hill is completed in phases, the following phasing plan shall apply. All existing LDC development and permitting regulations shall apply to all Project phases.

1.21 Platting, Dedications, and Public Improvements

Prior to submission of building plans for review and permit, Final Plat approval shall be required for the Project Site. Rights-of-way and easements may be required to be dedicated at that time, subject to and in accordance with existing provisions of the LDC. Also subject to and in accordance with existing provisions of the LDC, prior to Final Plat approval, Public Improvements Construction Plans (“PICPs”) shall be required to be submitted and approved for both on-site and off-site improvements, and such improvements shall be required to be constructed at the owners expense, or fiscal surety provided by the developer. Required improvements shall include underground utilities and the proposed streetscapes for all street frontages as illustrated in *Figures 4.1-4.7*, to the extent required under existing LDC provisions and this PDD Standards Book. If necessary, streetscape improvements may be phased as the development of the site occurs, if fiscal surety has been provided.

1.22 Phase 1

At a minimum, the first phase will include demolition of all buildings other than the existing auditorium and gymnasium. As soon thereafter as practical, the developer will commence and complete construction of the parking garage and the components wrapping the garage. Additionally, Phase 1 will include completion of Poet's Corner described in this Book, and completion of the on-street parking, street-tree zone, and sidewalk zone improvements along all rights-of-way adjacent to the entire Project Site, as illustrated below in Figures 4-1 through 4-7. Figure 1-10 depicts Phase 1 improvements.

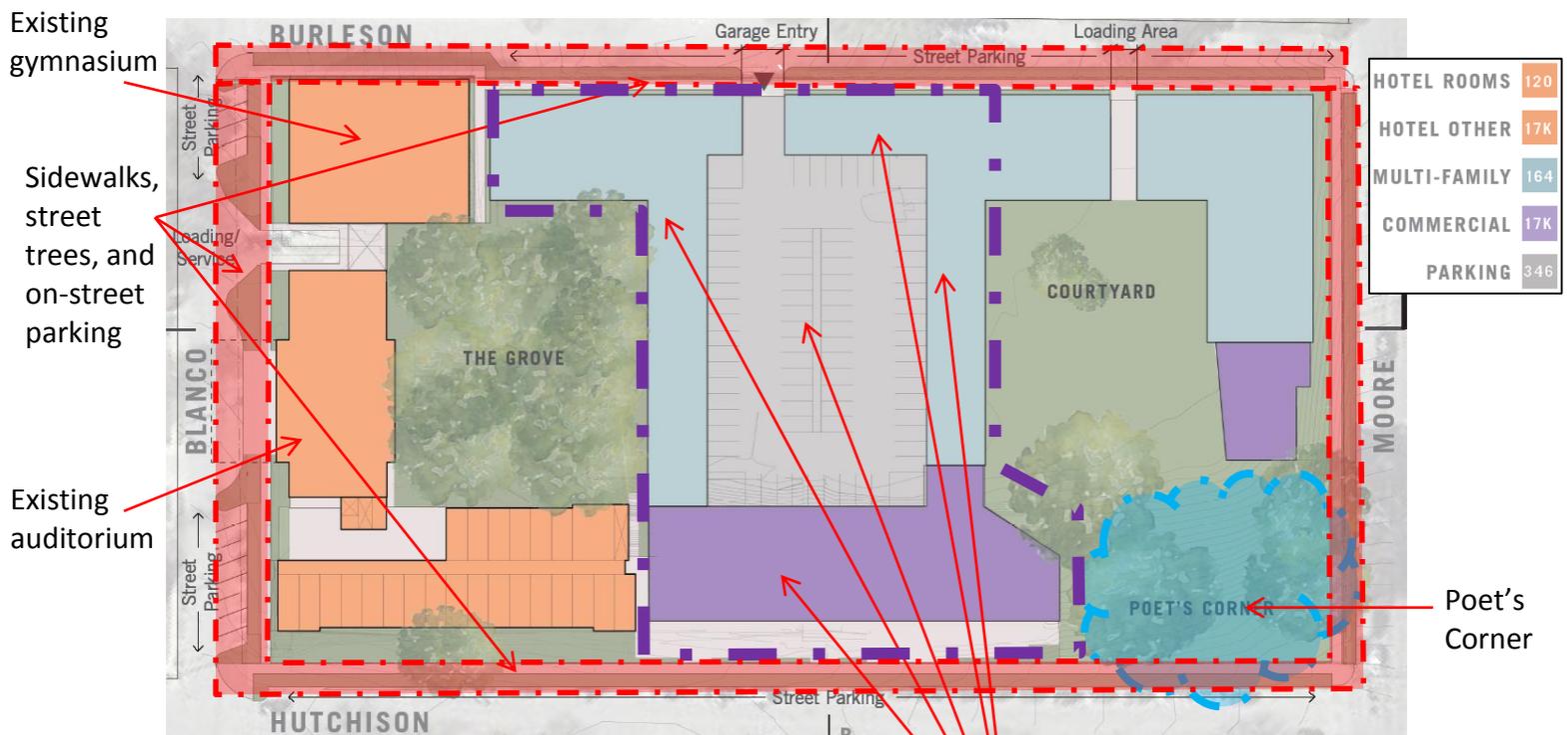


Figure 1-10: Portions of the Project improvements to be completed during Phase 1 are outlined as dashed, shaded areas.

Garage, Hutchison retail, and liner buildings

1.23 Phase 2

The second phase will include either completion of the hotel and adjacent outdoor spaces or the remaining portion of the apartment complex located in the area near Burleson and Moore Streets and the adjacent outdoor spaces (which also may occur as part of Phase 1), or both the hotel and remaining portion of the apartment complex along with their adjacent outdoor spaces. A depiction of Phase 2 components is included below in *Figure 1-11*.



Figure 1-11: Portions of the Project to be completed during Phases 2 or 3 are outlined as dashed, shaded areas.

1.24 Phase 3

In the event that the Project is not completed during Phases 1 and 2, Phase 3 will include completion of the balance of the Project other than the Moore Street retail space depicted in *Figure 1-11*, which shall not be a required project element.

- 2.0 OPEN SPACES
- 2.1 General Parkland Dedication Requirements
- 2.2 Description of Open Spaces

Open Spaces

2.1 General Parkland Dedication Requirements

The LDC requires the public dedication of one (1) acre of parkland per ninety-four (94) multifamily dwelling units. As this document specifies a maximum of one hundred sixty-four (164) multifamily dwelling units, approximately 1.75 acres would be required to be publicly dedicated to parkland.

The LDC also requires approximately twenty percent (20%) of the entire gross land area within a PDD to be devoted to open space that may be publicly-owned or a combination of publicly- and privately-owned. The Project Site is approximately five (5) acres and is therefore required to have a minimum of one (1) acre of open space.

The Project will meet the open space requirements for a PDD. However, none of the Project's open spaces will be publicly dedicated, as required generally under the LDC and discussed above. To meet the LDC's general parkland dedication requirement, the Project instead shall be required to provide a maximum payment of \$43,750 (subject to reduction if fewer multifamily units are constructed) as a fee-in-lieu of the public dedication of parkland for the Project. Payment shall be received by the city prior to final plat approval. ~~Additionally, because the fee in-lieu payment is an alternative to parkland dedication, the Project shall receive credit toward the fee in lieu payment for Project provided Poet's Corner improvements that "benefit the character and quality" of the Project pursuant to LDC Section 7.6.1.2(i).~~



2.2 Description of Open Spaces

2.2.1 Poet's Corner

Poet's Corner, located at the corner of Hutchison and Moore Streets, will be a privately-owned open space that generally will be open and available for public use and enjoyment in the Project owner's discretion. It will be managed, maintained, and controlled by the Project owner. Because this space is on the corner of Lindsey Hill that is closest to Downtown San Marcos, Poet's Corner will be designed to invite and encourage the public to enter, explore, and enjoy Lindsey Hill.

Poet's Corner may include an amphitheater-type element that can be utilized for, among other things, artistic performances, special events, lounging, reading, and interacting with neighbors. Poet's Corner shall be available for the use and enjoyment of residents and non-residents in accordance with rules

Examples of amphitheater-type elements and spaces that may be incorporated into Poet's Corner



and regulations of the Project owner, including but not limited to restrictions as to manner of use and hours of use.

Poet’s Corner Requirements and Checklist:

1. Preservation and maintenance of specimen trees.
2. The Project shall be responsible for ongoing maintenance of Poet’s Corner.
3. Terracing to define various recreational areas.
4. Lighting.
5. Landscaping, bike rack, seating (may be built-in, moveable, benches, chairs, etc.), public drinking water fountain, trash receptacles, and a sculpted slope.
6. Optional Improvements: Way finding signage, public art, bollards along road frontages, playscapes, water features, shade structures, exercise equipment, playing field, spray ground, game table(s), fenced off-leash dog area, and event space.

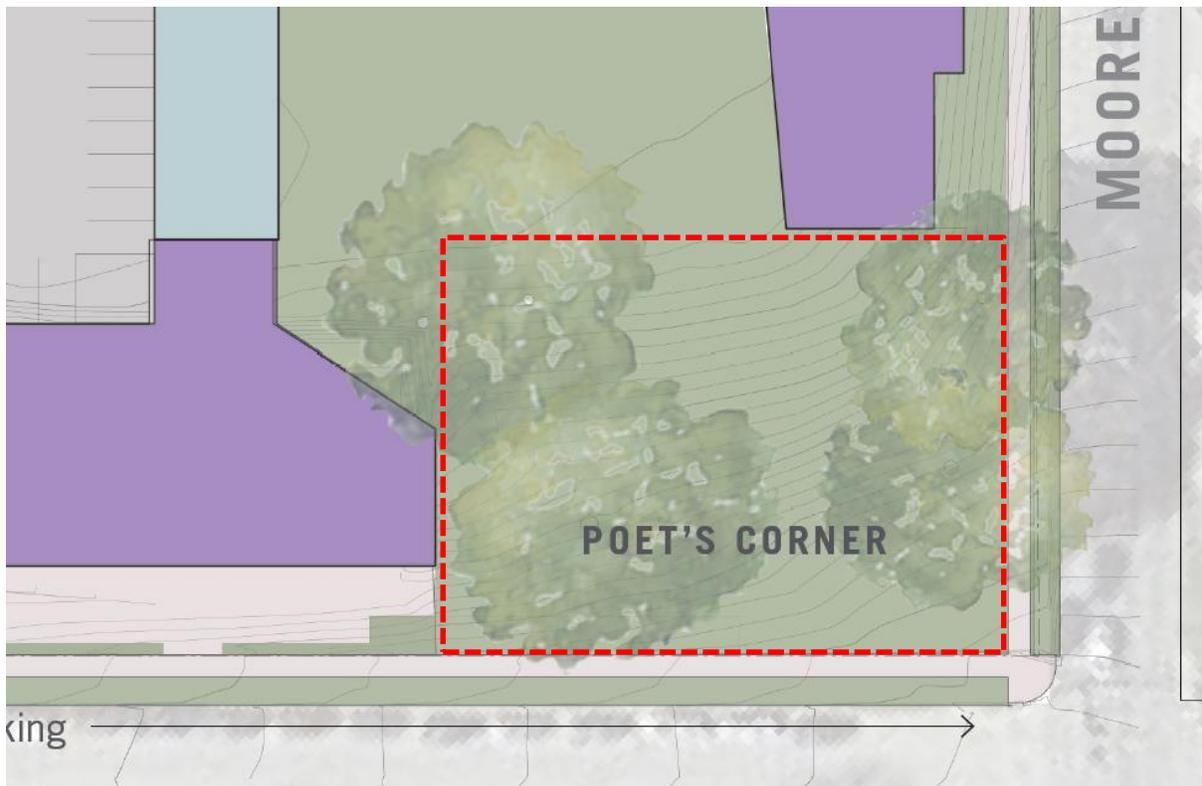


Figure 2-1: Approximate area of the Project Site to be known as Poet’s Corner is depicted in red.

2.22 The Grove

The Grove will be a privately-owned open space that generally will be open and available for public use and enjoyment, but sometimes will be reserved for special events related to the hotel and/or other parts of the Project. The development team currently envisions renovation of the auditorium building to include glass-lined front and rear walls that will become a jewel box of sorts, allowing pedestrians walking along Blanco Street to see into and through the lobby building and to enjoy the magnificent oak trees and green space beyond. That view will invite the public to enjoy a space that will include furnished patios and decking.



2.23 The Courtyard

The Courtyard will be a private open space reserved for residents of Lindsey Hill's multifamily community. The Courtyard will be separated from Poet's Corner with a fence or wall.



- 3.0 SITE DESIGN
- 3.1 Pedestrian and Bicycle
- 3.2 On-Site Landscaping
- 3.3 Exterior Lighting
- 3.4 Parking
- 3.5 Vehicular Loading and Access
- 3.6 Signage
- 3.7 Water Quality & Permanent Best Management Practices
- 3.8 Sale of Alcoholic Beverages

Site Design

3.1 Pedestrian and Bicycle

3.1.1 Pedestrian Access

The Lindsey Hill Project Site will be open to the public on a controlled basis, but also may close due to special events and security purposes. *Figure 3-1* depicts entrances that will be available to the pedestrian, and includes detail about whether such entrance will be gated or secured in some other manner.

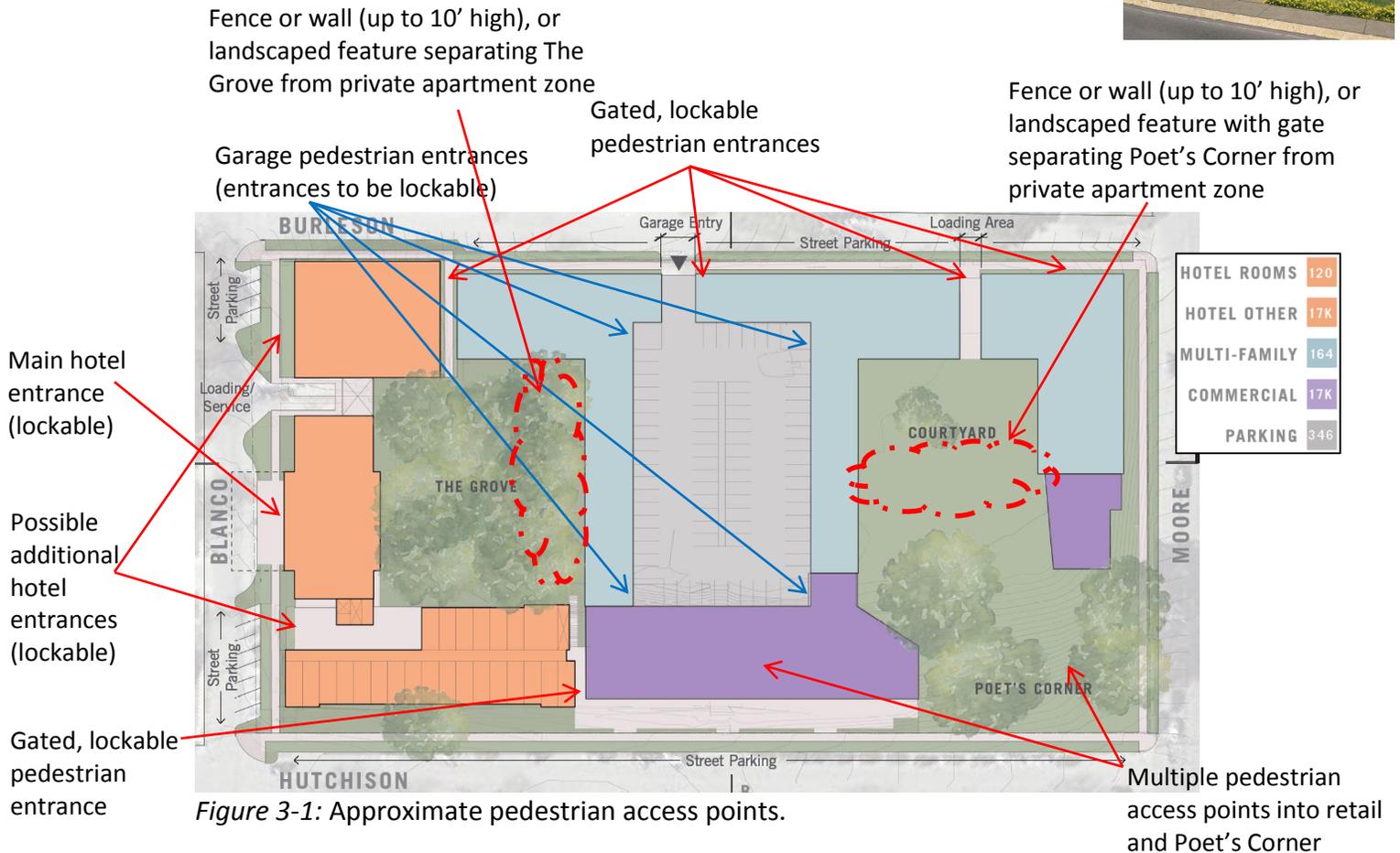


Figure 3-1: Approximate pedestrian access points.

Pedestrian Gates & Fencing

1. Gates along streetscapes shall be setback a minimum of four feet (4') from the building face (other than Moore Street gates described in Street Design & Amenities).
2. *Figure 3-2* provides examples of acceptable and unacceptable gates.
3. Gates may be a maximum of ten feet (10') high. Any gate above eight feet (8') high shall be at least seventy-five percent (75%) transparent and use metal as its predominant structural material. Any gate above six feet (6') high shall be at least fifty percent (50%) transparent. All other gates six feet (6') high or less shall be at least twenty-five (25%) percent transparent.

Notwithstanding the foregoing, there shall be no transparency requirement for the loading dock gate or for forecourt gates along the Moore Street frontage.

- Fencing shall be allowed only as illustrated above in Figure 3-1 and shall not exceed ten feet (10') in height. Fencing shall also be subject to the transparency requirements outlined above.

Figure 3-2: Examples of acceptable and unacceptable gate and fencing types within the PDD.



Simple wooden picket gate or fence - Not acceptable



Chain link gate or fence - Not acceptable



Decorative wooden gate and fencing - Acceptable



Metal gate and fencing - Acceptable



Greater than 8' tall, but at least 75% transparent and crafted of metal - Acceptable



Greater than 8' tall, but at least 75% transparent and crafted of metal - Acceptable



Less than 6' tall, but at least 25% transparent and with metal as structural material - Acceptable

3.12 Bicycle Parking

Bicycling will be encouraged by creating convenient bike storage locations, some of which will include covered bicycle parking for guests, and secured bicycle storage for residents.

Bicycle Parking Requirements and Checklist:

1. A minimum of sixteen (16) bicycle parking spaces shall be located within the street tree zone along the Hutchison frontage, not more than seventy-five feet (75') from the Project's retail component.
2. A minimum of sixteen (16) bicycle parking spaces shall be located within the parking garage.
3. In the event of a city-wide bike sharing program, space shall be provided for a bike-share rack.
4. There shall be no multifamily distance-to-door requirement for bicycle parking.



3.2 On-Site Landscaping

Except for the modifications and clarifications outlined below and in the attached Variances & Enhancements Matrix, landscaping shall be governed by the Base Regulations.

Because the base zoning of the PDD is MU, Mixed Use District, the Base Regulations require that a minimum of twenty percent (20%) of the entire site to be landscaped. Additionally, the Base Regulations require that a minimum of seventy-five percent (75%) of this required landscaping be installed between the property line and the building façade. However, because the Project may observe smaller setbacks than are permitted by right under the Base Regulations, satisfying the standard landscape requirements may not be possible. Therefore, landscaping may be achieved within the green space illustrated within *Figure 1-2* and as provided below.



Examples of drought-tolerant, flowering plants.

Landscape Requirements and Checklist:

1. Notwithstanding the Project's mixture of uses, the entire Project Site shall have a ~~fifteen~~ twenty percent (~~15~~20%) minimum required landscape area.
2. One hundred percent (100%) of Lindsey Hill's new plantings shall be chosen from San Marcos Preferred Plant List.
3. At least sixty-five percent (65%) of new plantings (other than grass and trees) shall be drought-tolerant, flowering plants.
4. The landscape plan submitted to the City of San Marcos for review and permitting shall exhibit a visible priority for areas with greatest visibility from the street in satisfying required tree, shrub, and groundcover landscaping requirements.



Tree Preservation

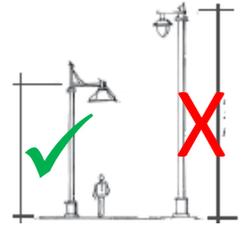
As depicted in *Figure 3-3*, the Project Site currently contains ten specimen trees with canopy sizes of at least 30'. Each of these ten trees will be preserved and protected as part of the Lindsey Hill project.



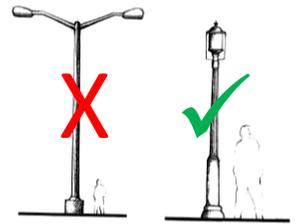
Figure 3-3: The green circles depict existing trees with canopy sizes of 30' or greater.

3.3 Exterior Lighting

The Project shall provide pedestrian scaled lighting along Lindsey Hill's pedestrian pathways. Light fixtures may be pole or building mounted, and lighting may be integrated into the trees and landscaped areas. Pole mounted fixtures may not exceed 20 feet in height.



Pedestrian scaled?



ACCEPTABLE



Lighting integrated into trees

ACCEPTABLE



Pedestrian-scaled building-mounted exterior lighting

ACCEPTABLE



Tree and landscape lighting

3.4 Parking

~~An Urban Land Institute (“ULI”) based parking study was completed for this Project (the “Parking Study”) and submitted to the City. The Parking Study examines the mixture of uses at the Project Site and the Project’s varied parking demand throughout the day. The parking study concluded that Lindsey Hill’s peak parking need, which occurs only on Friday and Saturday evenings during the 10:00 hour, totals 346 parking spaces.~~

~~The Parking Study’s ULI based model considers three variables: shared parking potential, modal split (arrival by means other than a car that will be parked in the Project’s parking garage), and captive market factor (factor considered when patrons visit more than one use within a single development).~~

~~Due to the Project’s proximity to downtown San Marcos and the Project’s proposed development intensity, which is consistent with development under San Marcos’ SmartCode, the Parking Study also compares the results of the ULI model to the Downtown SmartCode’s Shared Parking Model. The Parking Study’s ULI approach concluded that the Project would have a peak parking demand of 317 spaces with the office component, and 346 spaces without the office component. By way of comparison, application of the SmartCode’s Shared Parking Model and available transit reduction results in a peak parking demand of 275 spaces with the office component, and 286 spaces without.~~

Parking Comparison Table

	<u>Without Office</u>	<u>With Office</u>
Peak Demand (per Parking Study)	346 spaces	317 spaces
SmartCode Requirement	286 spaces	275 spaces
Surplus of Peak Demand Versus SmartCode Requirement	60 additional spaces	42 additional spaces

~~Across the nation, cities of all sizes are reducing and/or eliminating parking requirements, particularly for urban core development. Based on the rapid growth and measurable experiences of Uber, Lyft and other similar companies, transportation is evolving quickly whereby people’s dependence on and desire to own individual cars is rapidly decreasing. That clear fact is coupled with the Project developer’s goal to incorporate car sharing through Car2Go and other on-site rental opportunities, the availability of bus service one block away from the Project Site, and the Project’s urban location and focus on walkability and bikeability. Those alternative transit opportunities support the SmartCode’s conclusion about how much parking actually should be required for this type of Project at this location.~~

~~At Lindsey Hill, approximately 315 Required parking for the Lindsey Hill development spaces shall be provided on-site in the Project's proposed parking garage structure shall be dependent on the development of the office component. If the office component is not included in the final development of the project, the total parking required shall be approximately 405 spaces. If the office component is included, the total parking required shall be approximately 450 spaces. The following chart provides a detailed analysis of the required on-site parking requirements based on the land use square footage and unit maximum described in Section 1.1 – Land Use. Based on the Parking Study, all of the Project's daytime parking demand and 94.6% of the Project's weekly peak parking demand will be accommodated within the parking garage. During peak demand periods, valet parking can be utilized to "stack" vehicles thereby increasing garage capacity. Additionally, street parking will be available to relieve peak period needs.~~

LINDSEY HILL PARKING REQUIREMENTS - WITHOUT OFFICE

LAND USE	LINDSEY HILL - NO OFFICE					TOTAL
	Multi-Family (Bedrooms)	Hotel (rooms)	Hotel (Other)	Retail	Restaurant	
Unit (bedrooms/rooms/sqft.)	226	120	17000	8500	8500	n/a
PARKING RATIOS (spaces/unit)	0.85	0.85	0.003	0.003	0.004	n/a
REQUIRED PARKING SUPPLY						
On-Site	192	102	51	26	34	405

LINDSEY HILL PARKING REQUIREMENTS - WITH OFFICE

LAND USE	LINDSEY HILL - WITH OFFICE						TOTAL
	Multi-Family (Bedrooms)	Hotel (rooms)	Hotel (Other)	Office	Retail	Restaurant	
Unit (bedrooms/rooms/sqft.)	199	120	17000	27000	8500	8500	n/a
PARKING RATIOS (spaces/unit)	0.85	0.85	0.003	0.0025	0.003	0.004	n/a
REQUIRED PARKING SUPPLY							
On-Site	169	102	51	68	26	34	450

3.41 Off-Site Parking

~~To provide on-site flexibility in meeting the overall parking requirements discussed above, the Lindsey Hill project may locate up to 15% of required parking supply off-site, which is equal to approximately 61 spaces without office or 68 spaces with office. For the purposes of this document, off-site parking includes leased or owned private properties and the on-street parking located along Blanco Street and Burlson Street directly abutting the project site. Legal documentation, including a long-term agreement, shall be provided prior to final issuance of a Certificate of Occupancy that confirms the dedication of off-site parking for the Lindsey Hill project.~~

Additionally, if the project elects to locate a portion of its required parking off-site, and if after a period of at least one year following full build-out and full occupancy, it can be demonstrated that a portion or all of the off-site parking is not necessary for the project, the parking requirements of the Lindsey project listed above in Section 3.4 may be reduced up to 15%. This reduction may occur administratively.

3.4142 On-Street Parking

~~While 94.6% of Lindsey Hill's weekly peak parking demand will be handled within Lindsey Hill's parking garage, some patrons of the retail spaces and visitors will prefer to park their vehicles along the street.~~ As illustrated in Figures 4-1 through 4-7, on-street parking shall be provided along Hutchison, Blanco, and Burleson Streets. Those parking opportunities represent a relatively small amount of spaces, but are intended to provide additional spaces for the Project and existing adjacent developments.

On the Blanco Street frontage, the existing school use was supported by parking that was fully within the Property's boundaries. As part of the planned Lindsey Hill redevelopment, the Project's Blanco frontage expands the pedestrian right-of-way by including wide sidewalks with landscape/tree well zones on each side of the sidewalk causing the existing Blanco parking spaces to turn into a hybrid where a portion of each space is within the Project Site and a portion extends into the public right-of-way. ~~Since the developer is creating an improved condition by providing an enhanced pedestrian frontage as well as these now publicly available parking spaces, Blanco spaces will be credited to the Project's peak parking requirement.~~

On the Hutchison frontage, parking will continue to be available on both sides of the street. It is anticipated that approximately five (5) additional street spaces will be added to the Project's side. ~~Because sufficient street parking exists to support all property owners on this double block, Hutchison Street parking on the Project's side of the street will be counted toward Lindsey Hill's peak parking requirement.~~

The Burleson Street block currently does not have improved on-street parking. Overall, Burleson Street is improved to the level of an alley (e.g., poor, intermittent sidewalks, signage, and curbing), thus it is unclear whether parking is meant to be permitted on the opposite side of the street even though a small number of vehicles currently park there anyway. Under the PDD, at the Project's expense the Burleson frontage of the Property will be substantially upgraded with a high-quality pedestrian right-of-way including wide sidewalks, street trees and landscaping, and pedestrian-focused building design. Additionally, clearly marked street parking will be provided for a likely

total of eighteen (18) parking spaces on this frontage. ~~Because of this improved streetscape condition and the addition of clearly marked parking spaces provided by the Project, Burlison Street spaces will be counted toward Lindsey Hill's peak parking requirement.~~

3.43 Parking Garage

Lindsey Hill's total parking garage capacity takes into account the need to support a minimum of one parking space per multifamily unit and 0.05 parking spaces per unit for multifamily visitor parking.

The parking garage may have a single access point with up to three lanes (thereby reducing points of conflict between pedestrians and motor vehicles) and an adjacent pedestrian access point. The service and loading dock driveway is the only other location where vehicles will cross sidewalks surrounding the Project Site.

The garage's rooftop level may be used for parking and/or for Project amenity opportunities. In order to potentially make that space available and still provide sufficient parking for Lindsey Hill, a portion of the parking garage will be underground.

Parking Garage Attributes and Allowances:

1. Parking garage may include below-grade parking.
2. Parking garage will be hidden from public view through the use of liner buildings.
3. Hotel and multifamily amenities and related event spaces may be on the parking structure's rooftop level.
4. Parking garage entrance/exit gates, doors, etc. shall be recessed from the main building façade a minimum of four feet (4').
5. Curb stops will be provided as necessary throughout the parking garage.
6. Parking spaces within the PDD shall be a minimum of 8'6" wide.
7. No parking spaces shall be required to be marked as "visitor" parking spaces.
8. Spaces or sections within the garage can be blocked off with signage, gates, etc. as necessary for valet use, resident use, and/or other purposes.
9. A minimum of 1 space per multifamily unit provided for residential use.
10. A minimum of 0.05 spaces per unit provided for visitor parking for multifamily residential.



3.5 Vehicular Loading and Access

In addition to the parking garage, vehicle loading and access shall be restricted to the hotel porte-cochere, the short-term vehicle loading zone on Burleson Street, and the loading dock and waste management access point along the Blanco Street frontage.

3.51 Porte-Cochere

As illustrated in *Figure 3-4*, the Project plans to locate a porte-cochere along a portion of the Blanco Street frontage. The porte-cochere will be a drop-off and passenger loading point for hotel guests and may be used for valet parking for the hotel and retail spaces.

Because the porte-cochere likely will extend from an existing structure that the developer hopes to adapt for the Project, a space constraint exists which creates the need for the porte-cochere to extend into the Blanco Street right-of-way. Thus, the developer will be required to obtain approval of a right-of-way use agreement by the City of San Marcos, outside of the approval of this document.

The City is under no obligation to enter into such agreement and final approval of a right-of-way use agreement shall be contingent upon the submittal and review of construction documents for the porte-cochere.

The proposed right-of-way use agreement and final design of the porte-cochere, to be submitted by the developer to the City for review, shall adhere to the following requirements:

- a. All porte-cochere structural elements shall be located within the boundaries of the Property.
- b. Any portion of the porte-cochere overhanging the public right-of-way shall be at least fourteen feet (14') above the street grade below.
- c. The porte-cochere must be setback a minimum of two feet (2') from any adjacent travel lane along Blanco Street.



Porte-cochere examples



Porte-cochere examples



3.52 Short Term Vehicle Loading Zone on Burleson

A loading zone may be provided on the Burleson Street frontage adjacent to the pedestrian entrance on Burleson near Moore Street. The approximate location of that loading zone is depicted above in *Figure 1-1*.

Burleson Street Loading Zone Requirements and Checklist:

1. Burleson Street loading zone may be located within the parking lane only.
2. Appropriate signage shall be installed at the Project's expense.
3. Loading zone to have a maximum length of forty feet (40').
4. Loading zone shall be in front of the Burleson Street building entrance closer to Moore Street.



3.53 Loading Dock and Waste Management Access

Commercial loading/unloading and trash pickup for the PDD are centralized in order to reduce pedestrian realm disruptions. Depicted below in *Figure 3-4* is a concept diagram showing the loading dock and waste management areas that will service the PDD, and a depiction of truck movements related to those areas.



Example of paving to safeguard pedestrians at vehicular crossings

Loading Dock and Waste Management Access Requirements and Checklist:

1. To utilize the loading dock, truck movement may occur within the public right-of-way (as further depicted in *Figure 3-4*).
2. Trucks will be permitted to back across the sidewalk in the loading dock area.
3. Trucks may cross the pedestrian plane in forward or in reverse.
4. In the area where trucks cross the pedestrian plane, additional measures shall be taken in order to enhance pedestrian safety. Such measures may include different pavement, special signage, special lighting, sound-making devices, gates, and/or other barriers.
5. Loading dock doors, gates, etc. may be up to ten feet (10') tall, but shall not have a transparency requirement. The loading dock gates alone shall satisfy loading dock screening requirements under LDC Section 6.1.2.4(a).
6. Loading dock gates, doors, etc. shall be setback a minimum of four feet (4') from adjacent building frontages.
7. Loading dock throat (depicted in *Figure 3-4*) width of up to twenty-five feet (25').
8. Trash receptacles will be entirely screened from public view and will be located behind the loading dock gates.



Example of signage to safeguard pedestrians at vehicular crossings

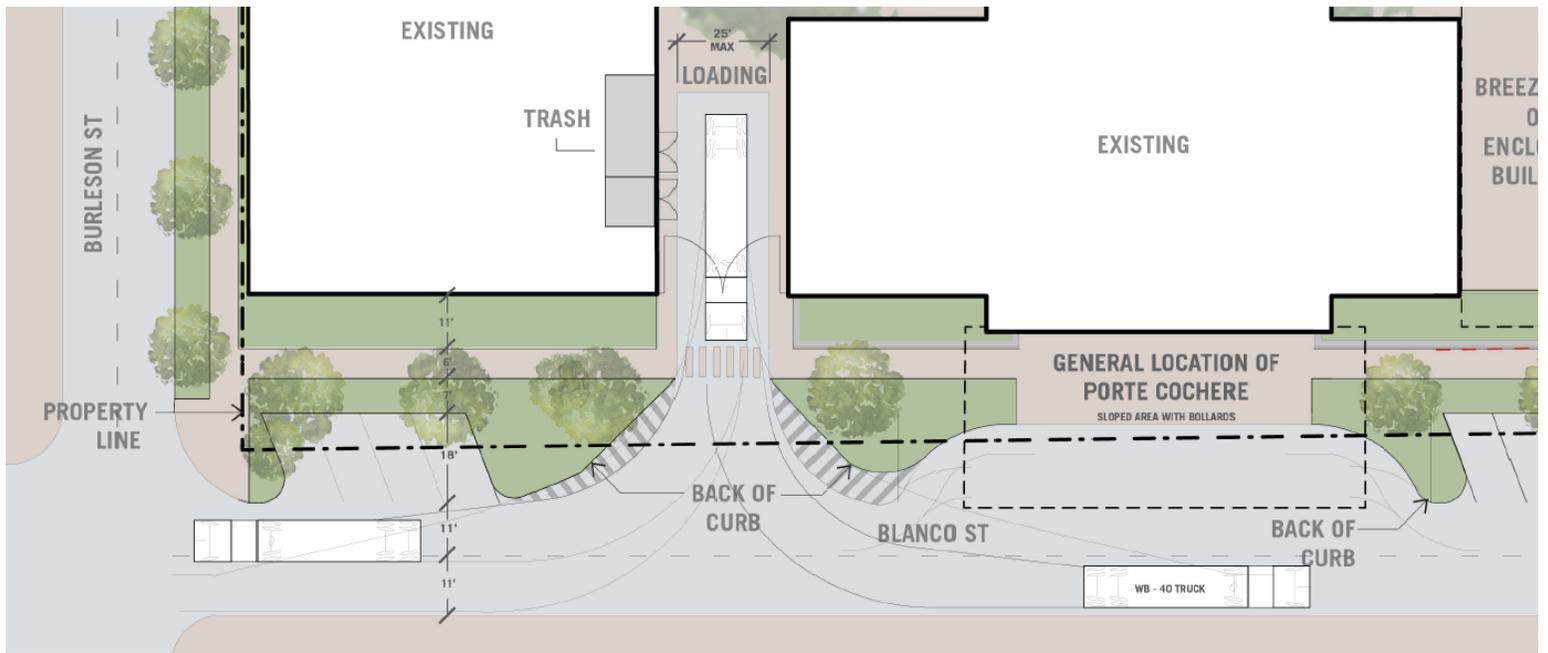


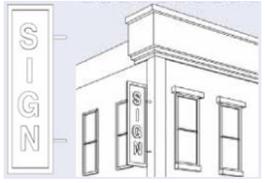
Figure 3-4: Loading dock and waste pickup area concept drawing.

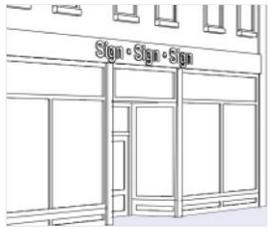
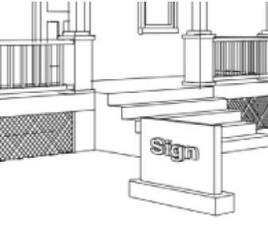
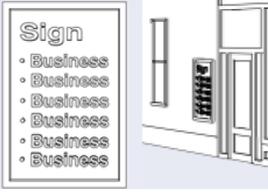
3.6 Signage

Signage within the Project Site shall be subject to the signage standards applicable under the Base Regulations and the standards set forth below. In the event that a conflict may arise between the two, the standards contained within this PDD Standards Book shall prevail.

Figure 3-5 depicts the sign types permitted within the PDD and their permitted frontages.

Figure 3-5: Permitted sign types and frontages

<u>Sign Type Defined</u>	<u>Depiction</u>	<u>Permitted Frontage(s)</u>
Awning or Canopy Sign: A sign painted on or attached flat or flush against the surface of an awning or canopy.		Hutchison Blanco Moore (on retail only)
Projecting Sign: A sign that is attached directly to the building wall and which extends out from the face of the wall.		Hutchison Blanco Moore Burleson (limited to wayfinding signage for parking garage only)
Hanging Sign: A sign that is hanging or suspended (such as by chains or hooks) from a canopy, awning, or building overhang.		Hutchison Blanco Moore (on retail only)
Sandwich Board: A portable sign designed in an A-frame or other fashion, and having back-to-back sign faces.		Hutchison Blanco Moore (near retail only)
Retaining Wall Sign: A sign that is engraved, painted on, or attached directly to a retaining wall.		Hutchison Moore (adjacent to Poet's Corner)

<p>Wall Sign: A sign that is engraved, painted on, or attached directly to the building wall within a Sign Band on the building facade.</p>		<p>Hutchison Blanco Moore</p>
<p>Monument Sign: A sign that is erected on a solid base placed directly on the ground, and that is itself constructed of a solid material.</p>		<p>Hutchison Blanco Moore (on retail only) Burlison (limited to wayfinding sign for parking garage only)</p>
<p>Directory Sign: A sign that displays the tenant name and location for a building containing multiple tenants.</p>		<p>Hutchison Burlison Moore</p>
<p>Window Sign: Signs affixed, painted, or etched to windows or hanging inside of windows that are visible to the exterior of the building.</p>		<p>Hutchison Blanco Moore (on retail only)</p>

PDD Signage Standards

1. General

- a. Freestanding pole signage shall not be permitted at the Project Site.
- b. The sign area is calculated as the largest area of the sign visible at any one time from any one point and enclosed by a rectangle, including any framing or trim, but not including any structural parts lying outside the limits of the sign and which do not form an integral part of the display. If the copy is enclosed by a box, outline or frame, area is the total area of the enclosure. If the sign consists of individual letters, numbers or symbols, on a surface or having no frame, area shall be the sum of the areas of the rectangles which can encompass each portion of the copy. The area of four-side signs is considered the same as two double-faced signs.
- c. Shopfront window signage may be up to 30% of the window area and may be neon or LED lit.
- d. Basic sign types permitted include awning or canopy signs, projecting signs, hanging signs, sandwich boards, and wall signs.
- e. Special sign types permitted include shopfront window signs, directory signs, monument signs, and retaining wall signs.



Pole-mounted signs not allowed

2. Illumination
 - a. Signage within the PDD shall be externally illuminated, except as follows:
 - i. Signage within the storefront glazing may be neon or LED lit.
 - ii. The parking garage monument sign, signage located along Moore Street, and signage at the corner of Moore and Hutchison Streets may be neon (or LED equivalent), halo, or internally illuminated.
 - iii. Wall signs serving the hotel on Blanco and retail or office tenants on the Hutchison frontage may be neon , LED lit, or diffused internal illumination (as defined below)
 3. ***Diffused Internal Illumination:*** A source of illumination entirely within the sign which makes the sign content visible at night by means of the light being transmitted through a translucent material, but wherein the source of the illumination is diffused by a solid front panel. Directory Signs
 - a. One directory sign is permitted at each street-level entrance to upper-floor businesses and residences, and on facades facing entrances to alleys, rear lanes and parking lots.
 - b. The area of a directory sign shall not exceed six square feet (6 SF).
 - c. Directory signs shall be no taller than three feet (3’).
 4. Awning or Canopy Signs
 - a. One awning or canopy sign is permitted per business.
 - b. The sign may be placed on either the vertical valance flap, the sloped portion, or on a side panel of the awning or canopy.
 - c. The sign shall not extend below or above the awning or canopy to which it is attached.
 - d. When properly installed on canopies, canopy signs may extend into Setback Zones.
 - e. An awning or canopy sign shall not exceed three feet (3’) in height.
 - f. An awning or canopy sign shall not exceed twelve feet (12’) in length.
 5. Projecting Signs
 - a. One projecting sign is permitted per building façade.
 - b. Sign area shall not exceed nine square feet (9 SF) for each projecting sign. Notwithstanding the foregoing, one projecting sign on the Moore Street frontage may be up to eighteen square feet (18 SF).
 - c. A projecting sign must maintain a minimum eight foot (8’) clearance above the sidewalk or finished ground surface below the sign.
 - d. A projecting sign may be attached to the building façade.
 - e. For single story buildings up to sixteen feet (16’) in height, a projecting sign may extend no more than fifty percent (50%) of the sign height up to a maximum of two feet (2’) above the parapet or roof of the structure to which it is attached. For all other building heights, a projecting sign may not extend above the parapet or roof of the structure to which it is attached.

- f. Projecting signs may penetrate the Setback Zone by up to thirty-six inches (36") from the building face.
6. Hanging Signs
 - a. One hanging sign is permitted per business.
 - b. A hanging sign must maintain a minimum eight foot (8') clearance above the sidewalk or finished ground surface below the sign.
 - c. Sign area shall not exceed six square feet (6 SF) for each hanging sign.
 - d. Hanging Signs are permitted within Setback Zones adjacent to retail spaces.
 7. Sandwich Board Signs
 - a. One sandwich board sign is permitted per business.
 - b. The area of each face of sandwich board shall not exceed twelve square feet (12 SF).
 - c. The overall sign shall be no taller than four feet (4').
 - d. A sandwich board within the public right-of-way must be placed such that at least a six foot (6') unobstructed sidewalk width remains.
 - e. Sandwich board signs may be "A-frames" or spring base.
 - f. A sandwich board must have a stable base.
 - g. Sandwich boards shall be removed at the close of business each day.
 8. Wall Signage
 - a. One wall sign is permitted per business.
 - b. Wall signs shall be located within a single external sign band located on the first story façade. Additionally, in the event that office space is incorporated into the Project, signage for office tenants may be located within a second sign band near the top of the building on the Hutchison Street frontage.
 - c. Wall signs should be attached flat to the wall. Three-dimensional signage is permitted, but shall not extend more than sixteen inches (16") beyond the face of the wall.
 - d. The sign band shall not exceed three feet (3') in height.
 9. Monument Signage
 - a. Monument signs shall incorporate a supporting base that is at least seventy-five percent (75%) of the width of the sign face at its widest point. The supporting base shall be constructed of brick, stone, masonry, or scored concrete.
 - b. Sign area shall not exceed eighteen square feet (18 SF).
 - c. Sign height shall not exceed four feet (4').
 - d. Monument signs may be installed within Setback Zones, but shall be at least two feet (2') behind sidewalks.
 - e. One monument sign may be installed on Burlison Street for parking garage wayfinding. The following special rules apply to that sign:
 - i. Sign may be located in the street tree zone adjacent to the parking garage entrance.



Sandwich board signs allowed (with restrictions)

- ii. Sign area shall not exceed fifteen square feet (15 SF). Sign height may be up to five feet (5’).
- iii. Sign may be constructed of metal and shall not be subject to the supporting base materials requirement of Subsection 9.a. above.

10. Retaining Wall Signage

- a. Two retaining wall signs are permitted near the corner of Hutchison and Moore Streets adjacent to Poet’s Corner.
- b. Signs shall be attached to the retaining wall. Three-dimensional signage is permitted, but shall not extend more than twelve inches (12”) beyond the face of the wall.
- c. Signs shall not exceed four feet (4’) in height and sixteen feet (16’) in length.
- d. Signs shall not extend beyond or above retaining wall edges.
- e. Signs shall be located behind sidewalks.

11. Temporary Signage

- a. A maximum of two (2) temporary signs shall be permitted on each of the Hutchison, Moore, and Blanco Street frontages.

12. Hotel Tower Element

- a. In addition to other allowable signage, the hotel’s tower element (described in this Book) may include signage.
 - i. This sign may be wall mounted (with or without a sign band), projecting, or may wrap around the tower element.
 - ii. Total square footage shall not exceed one hundred square feet (100 SF) per face, with up to three faces (or a total of 300 SF in the aggregate).
 - iii. May not be internally illuminated, but may be neon or LED lit.

3.7 Water Quality and Permanent Best Management Practices

The growth of impervious surfaces within urbanizing watersheds has the potential to reduce water quality. Pollutants that collect on pavement and other impervious surfaces may be washed into local creeks and rivers. The San Marcos community goals include protecting water quality by limiting impervious cover and requiring Permanent Best Management Practices (“PBMPs”) when necessary.

The base regulations limit impervious cover to sixty percent (60%). Increasing impervious cover may have negative impacts on water quality. In an attempt to reduce potential water quality impact, the following standards shall apply to the Project Site:

- A. That portion of impervious cover that is greater than sixty percent (60%) of the Project Site shall be designed and operated to achieve storm-water pollutant removal through the use of PBMPs approved by the City.

- B. Removal of a minimum of seventy percent (70%) of the Project’s increase of Total Suspended Solids (“TSS”) shall be achieved for that portion of impervious cover greater than sixty percent (60%) of the Project Site.

3.8 Erosion Control

Cuts between 4 feet and 8 feet are allowed with approval of the Director of Engineering and Capital Improvements. Cuts greater than 8 feet require approval of variance by City Council. It is anticipated that construction of Poet’s Corner will require significant grading which may require such a variance to implement the amphitheater concept in the PDD. Proper consideration of erosion control, tree preservation and slope stability shall be included in future variance requests.

3.9 Sale of Alcoholic Beverages

The sale of alcoholic beverages is intended to be a use permitted at the Property, subject to the more specific requirements of LDC Section 4.3.4.2 – Conditional Use Permits for On-Site Alcoholic Beverage Consumption and LDC Chapter 1, Article 5, Division 7, which governs conditional use permits generally. During design, development, and lease-up, requests will be submitted to the City of San Marcos for Conditional Use Permits (“CUP”) for the service and sale of alcoholic beverages. *Figure 3-6* depicts the various locations where alcohol is planned to be sold and/or served.

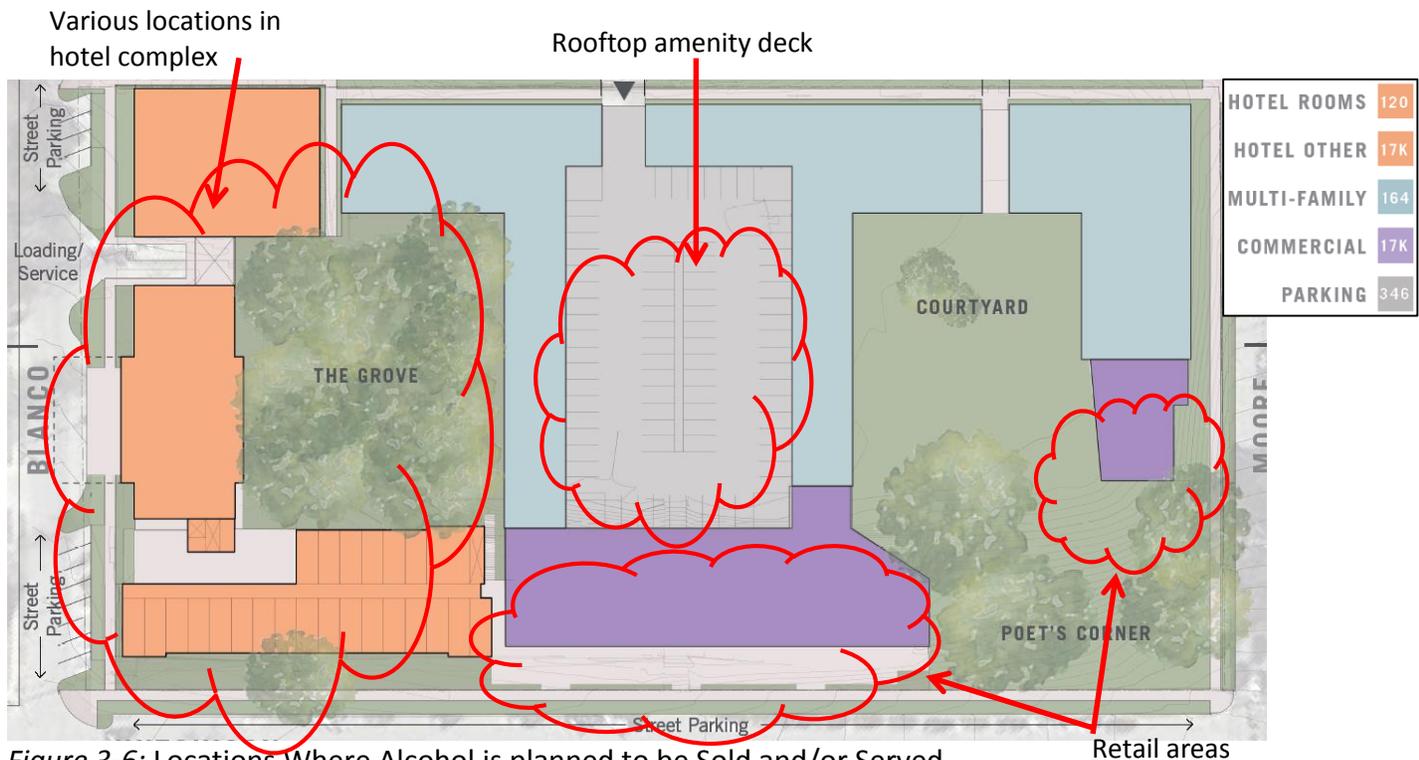


Figure 3-6: Locations Where Alcohol is planned to be Sold and/or Served.

- 4.0 STREET DESIGN & AMENITIES
- 4.1 Hutchison Street Streetscape
- 4.2 Moore Street Streetscape
- 4.3 Burleson Street Streetscape
- 4.4 Blanco Street Streetscape
- 4.5 Streetscape Standards Matrix

Streetscapes

4.0 Street Design & Amenities

Special attention will be paid to streetscape design in order to create an interesting and inviting pedestrian environment that softens the transition from public to private, blends Lindsey Hill's buildings into the existing historic district, and sets the tone for future neighboring redevelopment. In addition, street design will take into consideration the added demand that the Lindsey Hill development places on the existing neighborhood and streets.

A Traffic Impact Analysis (the "TIA") was completed by the developer and submitted to the City prior to completion of this PDD Standards Book. The TIA concluded that the Project will cause minimal impact to the surrounding roadway network. However, prior to the issuance of any Certificates of Occupancy for the project the east bound lane of Burleson at the intersection with Moore, shall be converted to a right-turn-only lane. This improvement shall be installed in cooperation with the City of San Marcos Engineering and Transportation Departments and at the expense of the developer. ~~a level of service ("LOS") decrease during the PM peak period was observed for the eastbound movement of Burleson Street at Moore Street, resulting in a diminished traffic flow during that time.~~

~~To mitigate that LOS decrease, the Project's traffic engineer recommended the installation of a traffic signal at the Burleson/Moore intersection and the City's engineering department agrees with that approach. Pursuant to the TIA, the Project's fair share of the cost of that traffic signal is \$26,750. Upon commencement of construction at the Project Site, the Project shall pay that sum to the City of San Marcos and prior to completion of Phase 1 of the Project, the City shall work with the Texas Department of Transportation to complete the installation of a traffic signal at that intersection.~~

The following street sections and diagrams, and the matrixes that follow those drawings incorporate the above goals and provide additional specificity about streetscape design on each street adjacent to Lindsey Hill. Setbacks and yard requirements within the Project shall be consistent with the measurements depicted in this section and summarized in the matrixes.

The street sections below are provided to illustrate conceptual design and to outline required elements that will be part of Lindsey Hill's streetscape. These illustrations are also intended to establish minimum dimensions of required streetscape elements, streetscape-related setbacks, and right-of-way components. These illustrations are not intended however, to dictate building design.

4.1 Hutchison Street Streetscape

Figures 4-1 and 4-2 depict typical design elements and dimensions that will be incorporated into the design of the Hutchison Street frontage. *Figure 4-1* depicts the design concept of the streetscape area along the retail frontage and *Figure 4-2* depicts the design concept of the area adjacent to the hotel building.

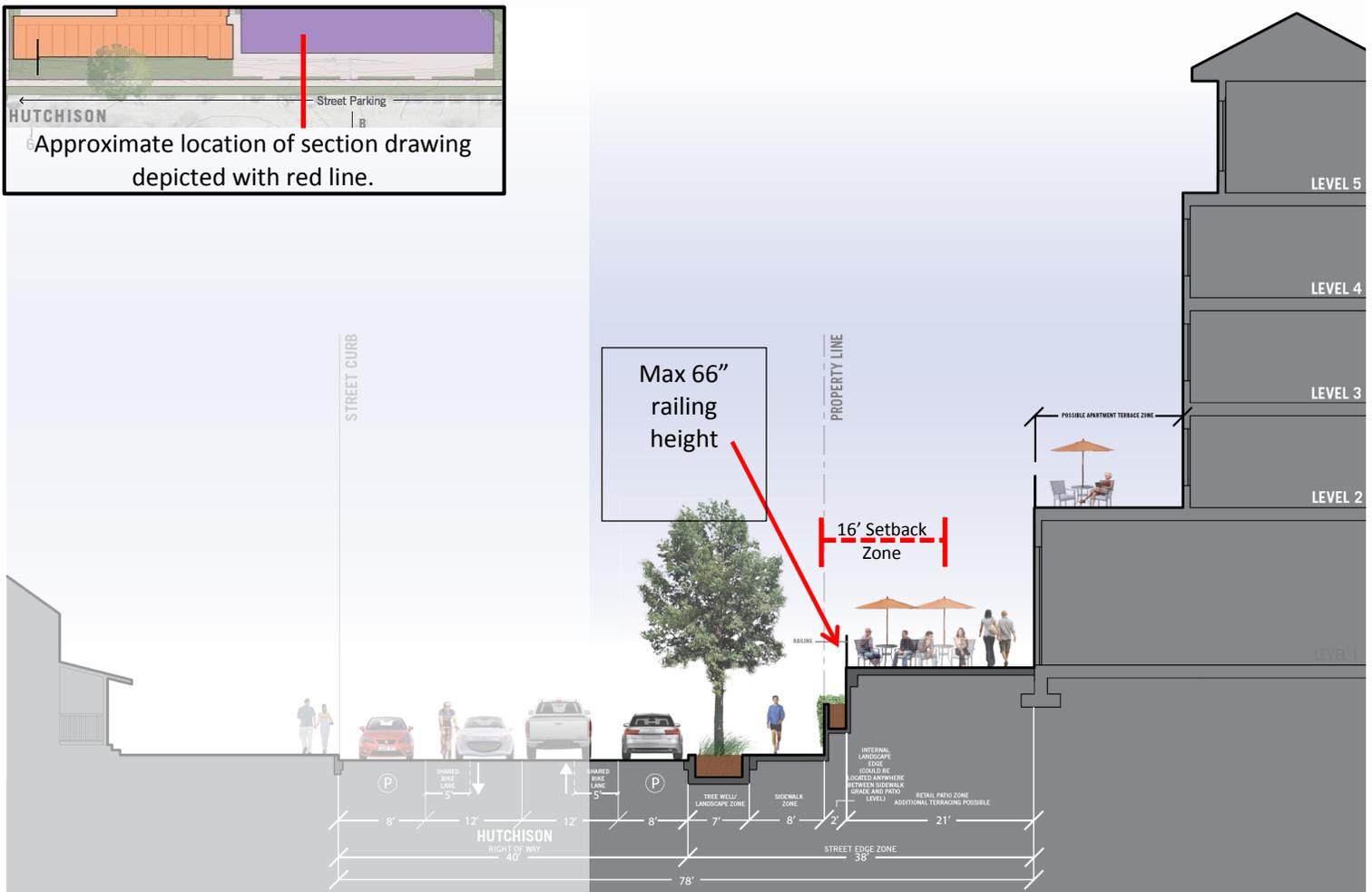


Figure 4-1: Hutchison Street streetscape design concept adjacent to retail space.

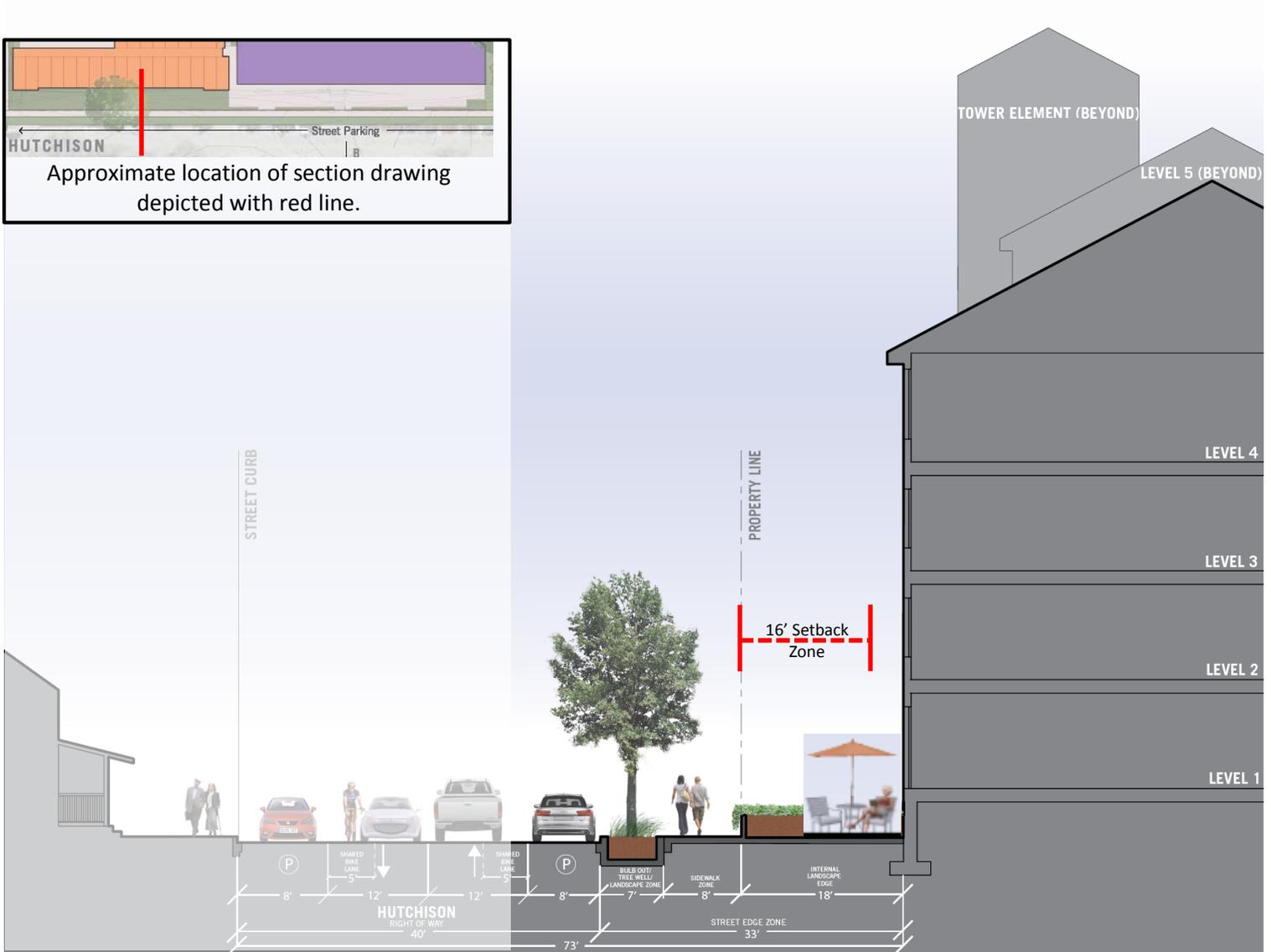
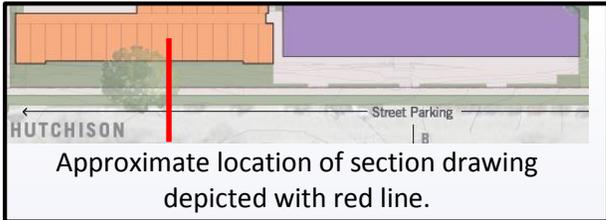


Figure 4-2: Hutchison Street streetscape design concept adjacent to hotel.

4.2 Moore Street Streetscape

Figure 4-3 depicts the typical streetscape design concept along the Moore Street frontage.

To provide comfort and enjoyment of ground floor apartment residents and for additional screening along the Moore Street frontage, a low wall and landscaping will be constructed to shield the forecourts and apartment entry doors beyond. The forecourt wall may be made of stone, brick, concrete, masonry, wood, metal or other innovative materials and may be topped with semi-transparent fencing for a total height of 66" above grade. Inclusion of that wall/landscaping shall fully satisfy the screening requirement under LDC Section 6.1.2.1(b)(1) related to multifamily use on the Moore Street frontage.

To provide further privacy for Moore Street forecourts, each forecourt entryway within the wall also may have an entry gate incorporated into (and "in-line" with) the wall. These entry gates have no transparency requirement and may be up to 66" tall. There shall be no easement or land dedication to the City related to the wall/landscaping for maintenance or for any other purposes.

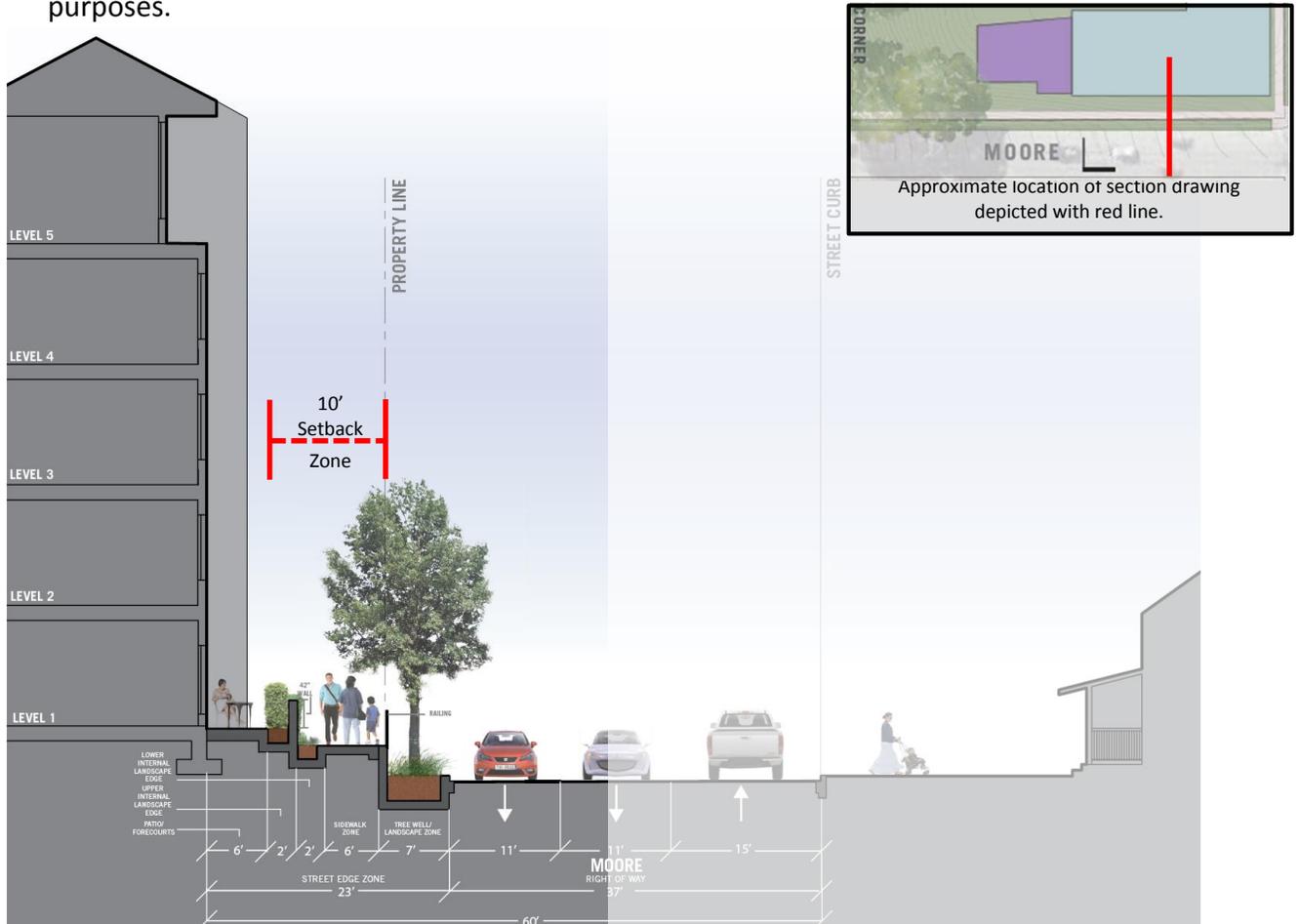


Figure 4-3: Moore Street streetscape design concept adjacent to multifamily building.

4.3 Burleson Street Streetscape

A substantial portion of Lindsey Hill’s Burleson Street frontage is across from properties that are zoned single-family. The entirety of those properties are within the Lindsey-Rogers Historic District.

Because a portion of that frontage is more sensitive than the other street frontages, the Burleson streetscape’s street tree zone shall be softened by plantings and landscaping.

Figures 4.4, 4.5, and 4.6 depict the minimum design criteria and related setback requirements for the Burleson Street streetscape.

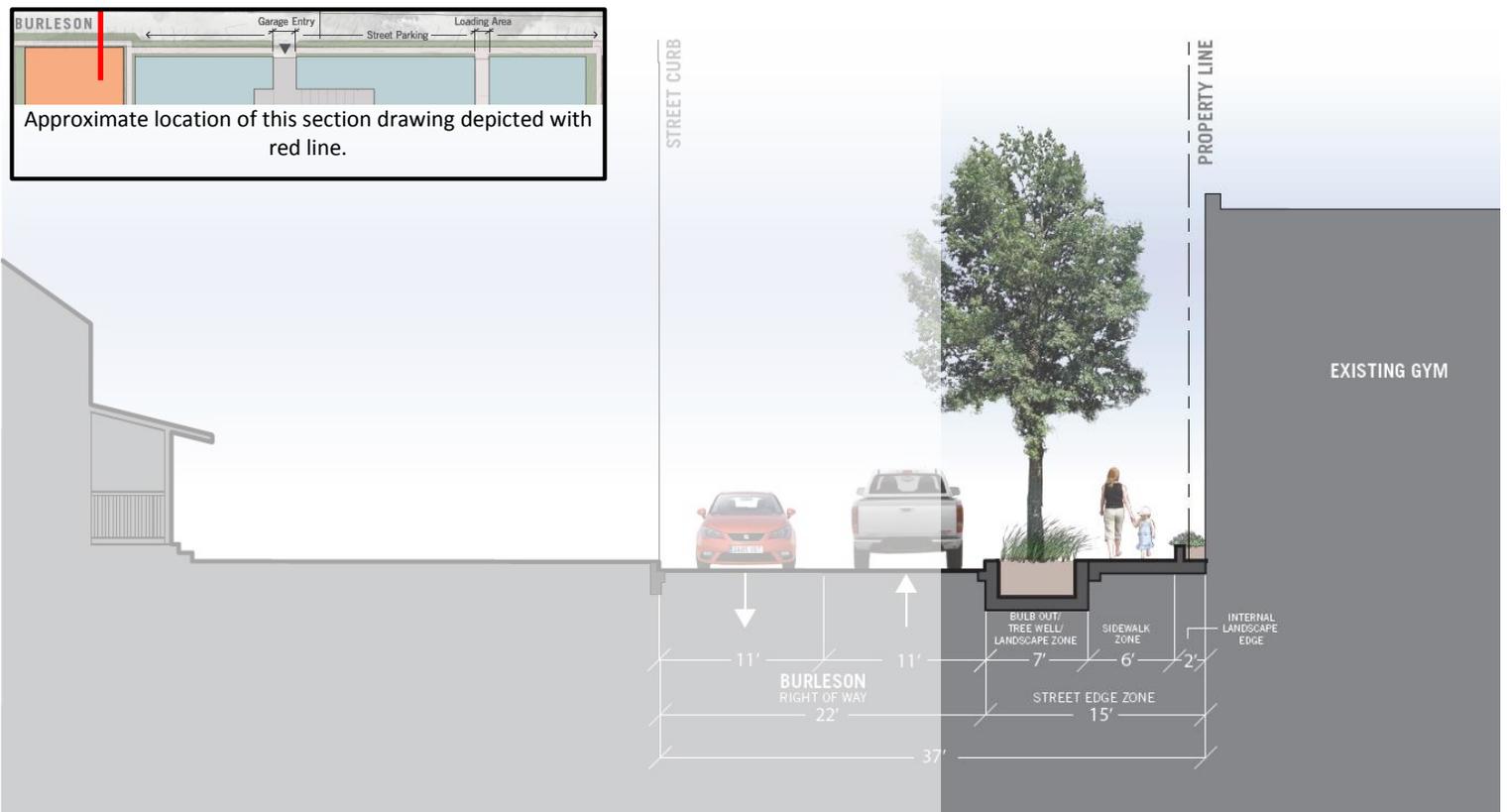


Figure 4-4: Burleson Street streetscape design concept adjacent to existing gymnasium building.

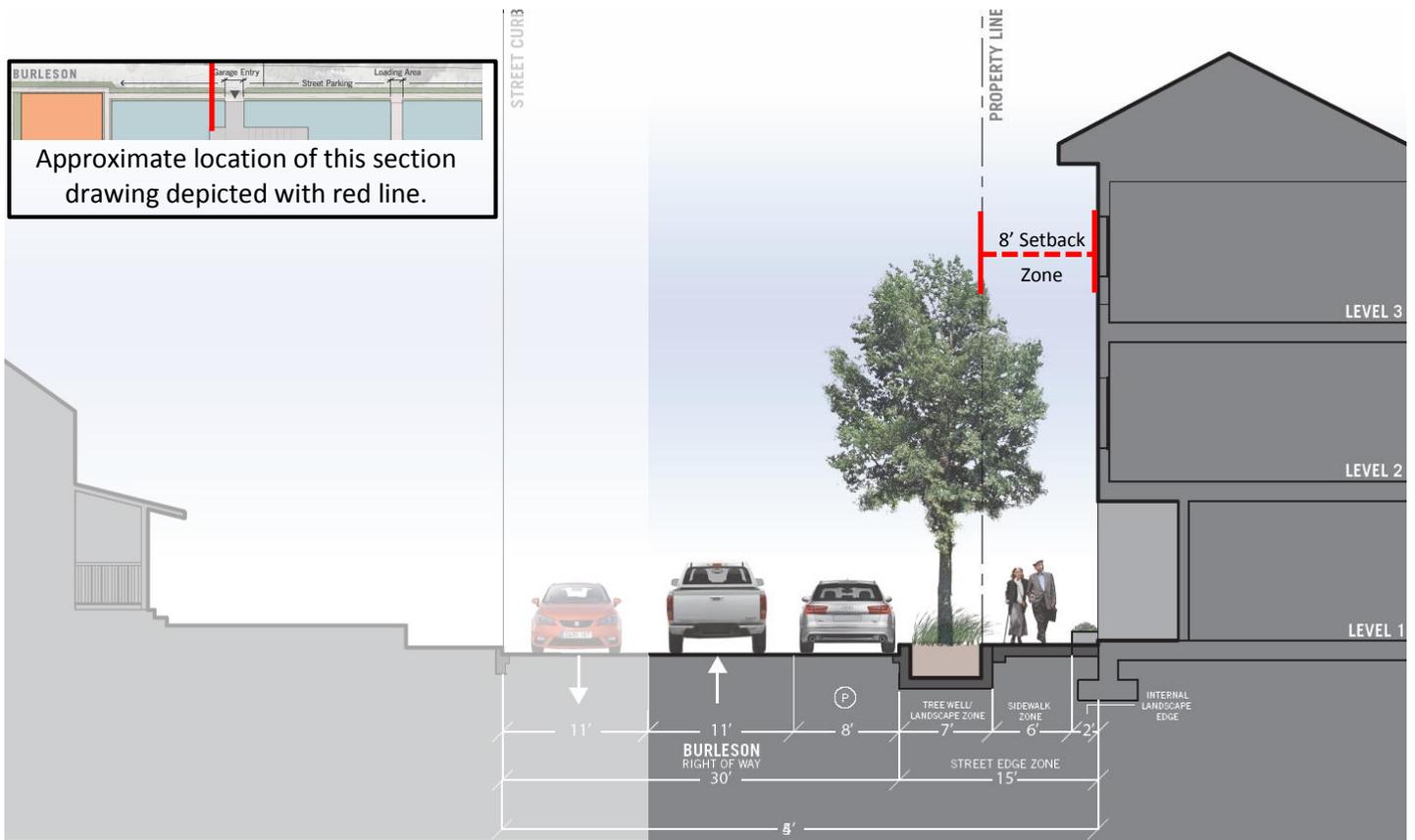


Figure 4-5: Burleson Street streetscape design concept adjacent to multifamily building, where building expression is closer to the right-of-way.

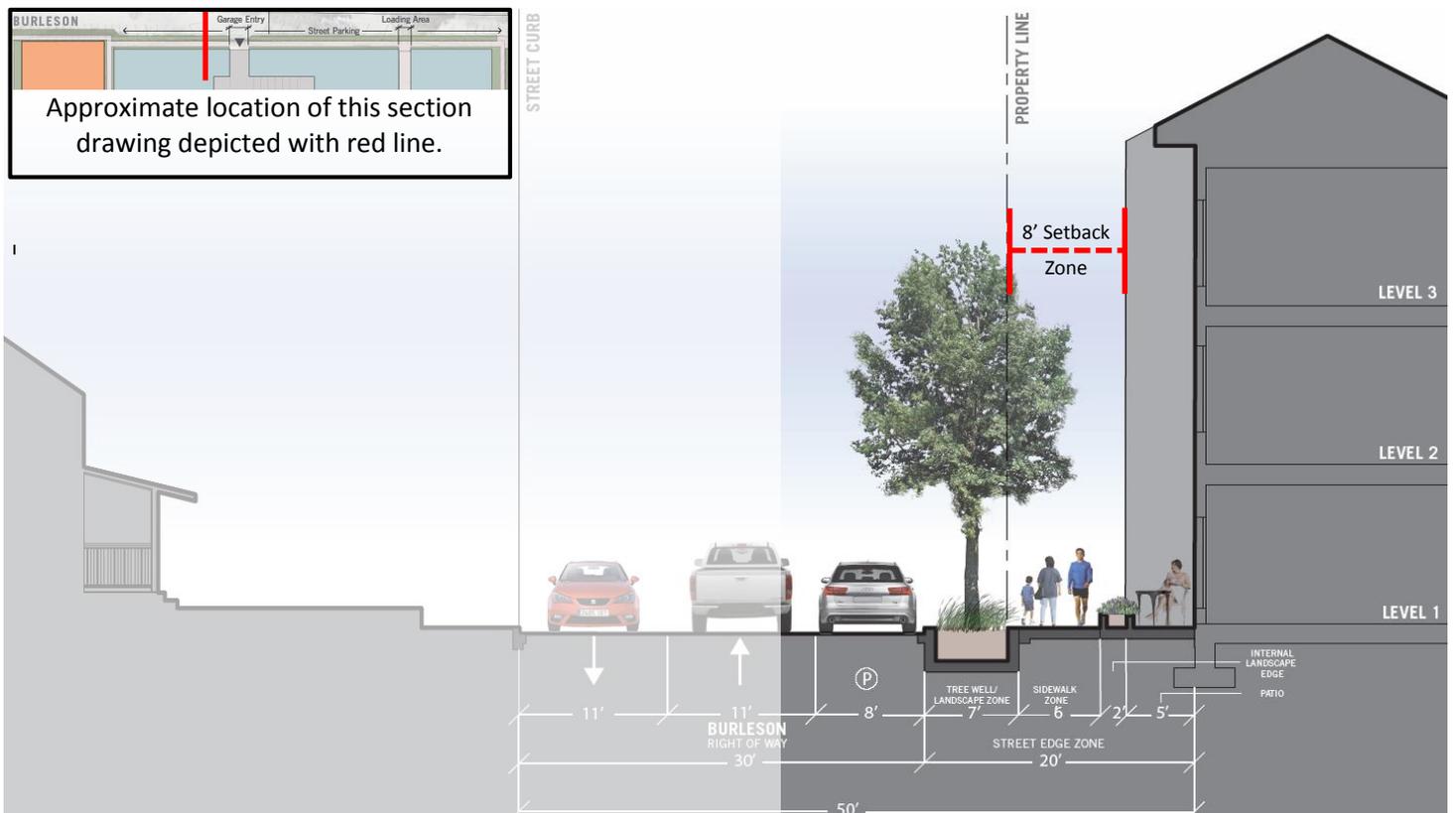


Figure 4-6: Burleson Street streetscape design concept adjacent to multifamily building, where building expression is farther from the right-of-way.

4.4 Blanco Street Streetscape

The Blanco Street frontage includes the hotel entrance and porte-cochere, a loading dock, waste management facilities, and parking. In order to better define these elements, a basic depiction of the likely format of that area is provided below as *Figure 4-7*. Final design will be within the limitations described below and elsewhere in this PDD Standards Book.

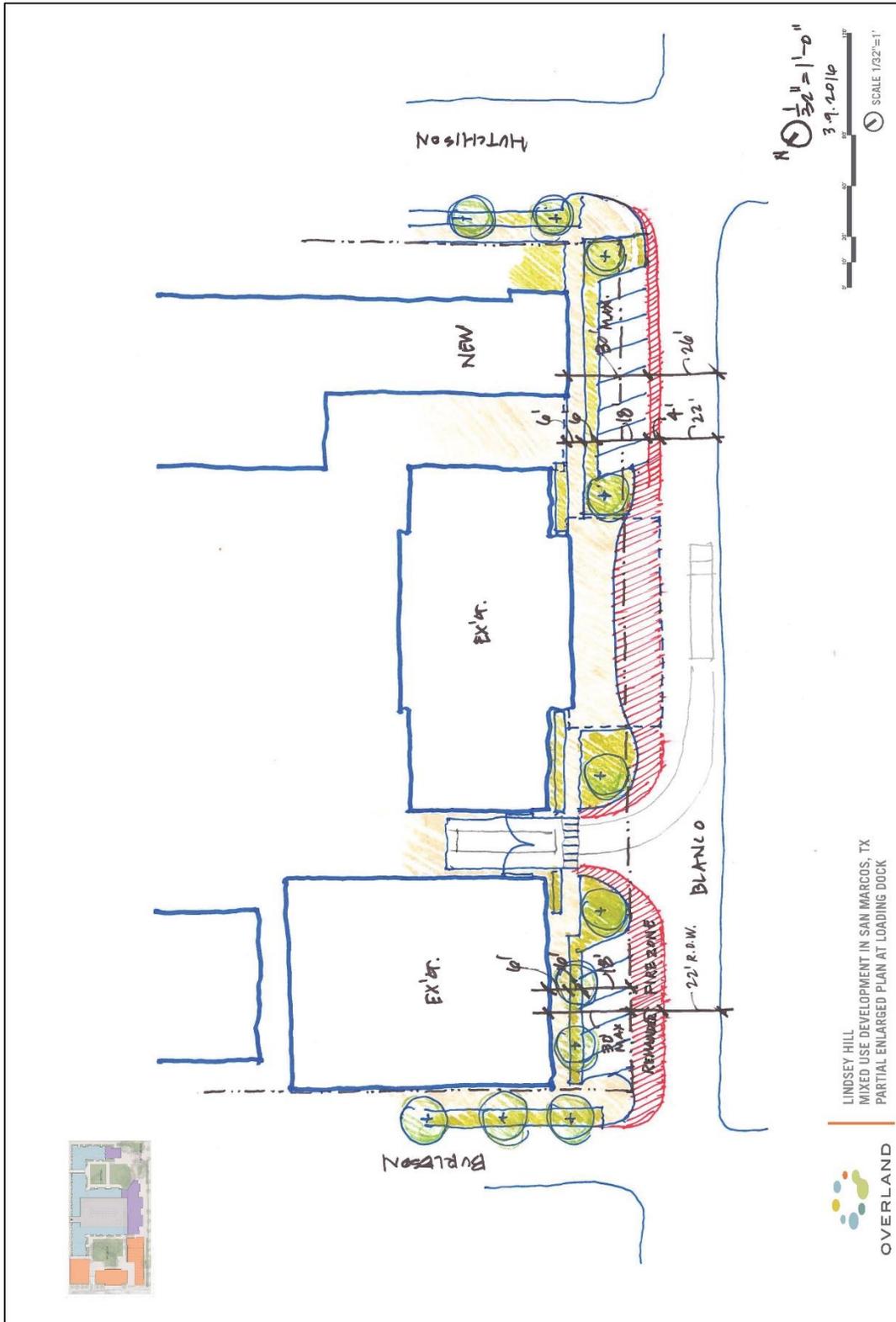
The following special features shall be applicable to the Blanco Street frontage (with more specific details on these features included in various sections within this PDD Standards Book).

Of additional note, the 2015 version of the International Fire Code (“IFC”) requires that buildings over thirty feet (30’) in height, be setback no more than thirty feet (30’) from the nearest adjacent fire lane. The City of San Marcos Fire Marshal has some discretion regarding this requirement. Therefore *Figure 4-8* provides an alternative Blanco Street design, in the event that the above IFC requirement is determined to be an issue at the time of site and building permitting.

Blanco Street Frontage Special Features:

1. The hotel porte-cochere may penetrate the public right-of-way.
2. The hotel porte-cochere must be set back a minimum of two feet (2’) from any adjacent travel lane along Blanco Street.
3. Passenger vehicle loading and unloading may occur in the public right-of-way under and near the porte-cochere.
4. Valet parking may occur in and around the porte-cochere.
5. A truck loading dock will be provided along the Blanco frontage.
6. Waste and recycling pickup may occur in the area adjacent to or within the truck loading dock.
7. Landscaped areas at and adjacent to the Blanco Street parking area shall be as depicted in *Figure 4-7*.
8. Street trees along the Blanco frontage may be clustered to allow for the needs of the porte-cochere, parking, loading dock, and other Blanco frontage elements.

Figure 4-8: Alternative Blanco Street frontage conceptual design plan.





 OVERLAND

LINDSEY HILL
 MIXED USE DEVELOPMENT IN SAN MARCOS, TX
 PARTIAL ENLARGED PLAN AT LOADING DOCK

4.5 Streetscape Standards Matrix

Pedestrian Streetscape Standards

	City Standard	Hutchison	Moore Street	Burleson Street	Blanco Street
Sidewalk Width (feet, minimum)	4	8	6	6	6
Sidewalks constructed of cast-in-place concrete, stamped concrete, pavers, tile, and/or other similar materials that meet City standards. ^d		R	R	R	R
Pedestrian-scaled lighting along sidewalks.		R	R	R	R
Street Tree Zone Width (feet, minimum)		7	7	7	7 ^f
Spacing Between Street Trees Required in Street Tree Zones (feet, maximum)		30	30	30	30 ^f
Waste Receptacles in Street Tree Zone ^{c, e}		R	R	R	R
Bicycle Racks in Street Tree Zone ^{c, e}		R	P	R	R
Internal Landscape Edge ^b (feet, minimum)		2	2	2	2

Vehicular Right-of-Way Standards^g

	City Standard	Hutchison	Moore Street	Burleson Street	Blanco Street
Total Vehicular ROW Width Including Parking Lane(s) (feet) ^g	53	40	37	30 ^j	40
Number of Vehicle Travel Lanes	2	2	3	2	2
Travel Lane Width (feet)	10 - 11 ⁱ (11 - 12 ^h)	12 (with sharrow)	11 (east bound) 15 (west bound)	11	11
Parking	Yes - 2 sides (Restricted ^h)	2 sides	None	South side only ^j	East side only
Parking Type		Parallel	N/A	Parallel ^j	Head-in or angled
Parking Lane Width (feet)		8	None	8 ^j	18 (depth)
Vehicles May Back Into Public Right-of-Way		U	U	U	p ^k
Parking Spaces may be reserved or closed by Lindsey Hill, and may be blocked-off using signs, cones, temporary pylons, etc.		U	N/A	U	P

^b - Internal Landscape Edge adjacent to hotel may be at any elevation, or at multiple elevations (terraced, sloped, etc.). Internal landscape edge adjacent to retail area may be at sidewalk grade, patio grade, or anywhere in between.

^c - Bike racks and waste receptacles shall be located adjacent to retail spaces and Poet's Corner, among other locations.

^d - Except as necessary for handicapped accessibility, internal sidewalks may be constructed with any of these materials or with crushed granite or other pervious or semi-pervious materials.

^e - Bike racks and waste receptacles shall be located convenient to hotel access points and other pedestrian entrances.

^f - Street tree zone shall be designed in a manner that allows for the needs of the porte-cochere, parking, loading dock, and other Blanco frontage elements, including clustering of street trees as necessary.

^g - Proposed vehicular right-of-way alterations to existing city streets are based on City professional staff requests and feedback.

^h - Moore Street only.

ⁱ - For streets with parking lanes.

^j - Condition varies adjacent to existing gym building and in Burleson St. Loading Area. See *Figure 4-4* for further detail.

^k - Blanco St. parking may be sited less than five feet (5') from the public R.O.W. and/or may be partially within the public R.O.W. and partially on the Property. Because of that, vehicles may be required to back into the public right-of-way from the Property.

- 5.0 ARCHITECTURAL GUIDELINES
 - 5.1 Building Setbacks (for new construction)
 - 5.2 Frontage
 - 5.3 Building Height
 - 5.4 Contextual Height Stepdown
 - 5.5 Expression Requirements
 - 5.6 Street Level Glazing & Transparency
 - 5.7 Upper Floor Glazing & Transparency
 - 5.8 Balconies and Porches
 - 5.9 Retaining Walls at Street Level
 - 5.10 Materials
 - 5.11 Mechanical Equipment & Utilities
 - 5.12 Trash Management

Architectural Guidelines

5.1 Building Setbacks (for new construction)

The following minimum setbacks shall be applicable for buildings along each frontage at the Project Site. These setbacks shall be for new construction only and shall not apply to buildings existing prior to the creation of the PDD.

<u>Street Frontage</u>	<u>Building Facade Setback Zone</u>
Hutchison Street	16 feet
Moore Street	10 feet
Burleson Street	8 feet
Blanco Street	17 feet

Measuring Setback Zones; Prohibited Features in Setback Zones. As depicted in *Figure 5-1*, Setback Zones shall be measured from the property line. All new buildings and projections (other than those items described in this PDD Standards Book that are specifically permitted within the Setback Zone) shall be located behind the Setback Zone.

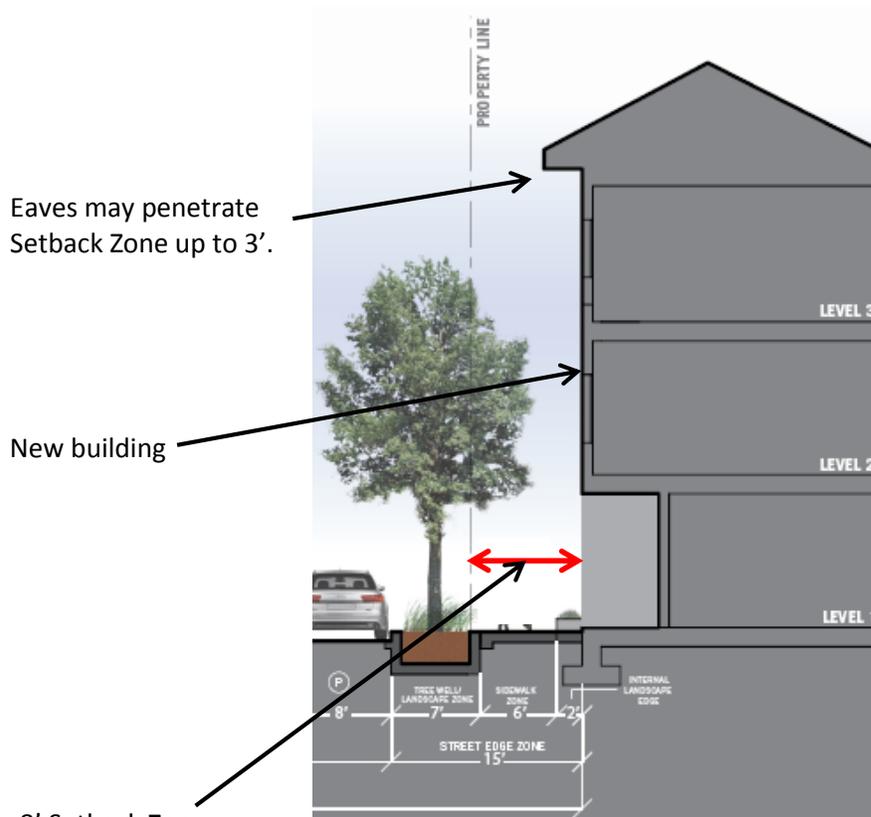


Figure 5-1: The red line depicts the Setback Zone, with measurement of the Setback Zone beginning at the property line.

Permitted Features in Setback Zones: The following are permitted to encroach into the Setback Zone:

- lighting, fans, heaters, outdoor fireplaces, fountains, art, decorative elements, and all other similar elements and amenities;
- architectural features such as eaves, overhangs (including building roof overhangs), gutters and downspouts, bay windows, cornices, uncovered stairs and ramps, planters, retaining walls, canopies, shade structures, arbors, and awnings;
- signage (as specifically described in the Signage section of this book);
- ground-floor forecourts and patios;
- seating, dining areas, knee walls and partition walls, and other elements associated with seating and dining areas; and
- adjacent to the retail areas on Hutchison and Moore Streets, single-level or tiered decks, porches, and patios (and any associated railings, benches, or barriers), and patio-roof coverings.

Frontage (defined below) elements also shall be allowed within the Setback Zone.

Figure 5-2: The following examples depict some of the elements permitted within Setback Zones at Lindsey Hill.



Patio dining, awnings and overhangs, and associated posts and structural elements.



Shade covering, patio roof coverings, dining areas, heaters, lighting.



Patio seating, awnings and associated structural elements, decorative features.



Multi-level decks/patios serving retail ground floor uses, patio dining, fans, lighting, heaters.

5.2 Frontage

Frontage in this PDD Standards Book shall be considered the area between the building façade and the lot line. *Figure 5-3* depicts the various Frontage conceptual types that are permitted at the Project Site and on which street fronts those Frontage styles may be incorporated. Notwithstanding *Figure 5-3*, features including sidewalks, Setback Zones, tree well and landscape zones, and parking may be located within the lot line. Detailed depictions of features located within and outside of the lot lines can be found above in the Street Design & Amenities section. Frontage elements and design shall be subject to the requirements of the Building Setbacks and Street Design & Amenities standards described above.

Figure 5-3: Frontage conceptual types.

	SECTION	PLAN	Permitted Frontage(s)
	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE	
<p>a. Common Yard: a planted Frontage wherein the Facade is set back substantially from the Frontage Line. The front yard created remains unfenced and may be visually continuous with adjacent yards, supporting a common landscape. The deep Setback provides a buffer from the higher speed Thoroughfares.</p>			Hutchison Moore Blanco Burluson
<p>b. Porch & Fence: a planted Frontage wherein the Facade is set back from the Frontage Line with an attached porch permitted to Encroach. A fence may be used at the Frontage Line to maintain street spatial definition. Porches shall be no less than 8 feet deep.</p>			Hutchison Moore Burluson Blanco
<p>c. Terrace or Lightwell: a Frontage wherein the Facade is set back from the Frontage line by an elevated terrace or a sunken Lightwell. This type buffers Residential use from urban Sidewalks and removes the private yard from public Encroachment. Terraces are suitable for conversion to outdoor cafes. Syn: Dooryard.</p>			Hutchison Moore Burluson Blanco
<p>d. Forecourt: a Frontage wherein a portion of the Facade is close to the Frontage Line and the central portion is set back. The Forecourt created is suitable for vehicular drop-offs. This type should be allocated in conjunction with other Frontage types. Large trees within the Forecourts may overhang the Sidewalks.</p>			Hutchison Moore Burluson Blanco
<p>e. Stoop: a Frontage wherein the Facade is aligned close to the Frontage Line with the first Story elevated from the Sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor Residential use.</p>			Hutchison Moore Burluson Blanco
<p>f. Shopfront: a Frontage wherein the Facade is aligned close to the Frontage Line with the building entrance at Sidewalk grade. This type is conventional for Retail use. It has a substantial glazing on the Sidewalk level and may have an awning that may overlap the Sidewalk to within 2 feet of the Curb. Syn: Retail Frontage.</p>			Hutchison Blanco Moore
<p>g. Gallery: a Frontage wherein the Facade is aligned close to the Frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the Sidewalk. This type is conventional for Retail use. The Gallery shall be no less than 10 feet wide and should overlap the Sidewalk to within 2 feet of the Curb.</p>			Blanco Hutchison Moore

In addition to the above Frontage styles, the basic layouts of the frontages depicted within the Street Design & Amenities section of this Book are permitted at the Project Site, regardless of whether they fit precisely within the styles depicted in *Figure 5-3*. Additionally, the items listed above as Permitted Features in Setback Zones also are permitted to occur within the Frontage.

Pedestrian Streetscape and Building Entrances Checklist

1. Special focus on architectural variety at first and second levels.
2. Street facing windows on all levels.
3. A minimum of fifty percent (50%) of windows in multifamily buildings shall be operable.
4. A minimum of fifty percent (50%) of ground-floor apartment entrances along Burleson Street to be along the street. A minimum of fifty percent (50%) of ground-floor apartment entrances along Moore Street to be along the street.
5. Awnings or overhangs protecting street level entries.

5.21 Retail Frontage



Retail Frontage Requirements and Checklist:

1. Outdoor seating and dining opportunities on terraces and decks.
2. Retail decks and/or terraces are permitted to front Hutchison and Moore Streets only.
3. Decks/terraces may be a single level or may be tiered with multiple levels.
4. Retail areas may also include decks and/or terraces along the edges of and facing into Poet's Corner.

5.22 Hotel Frontage

So long as Internal Landscape Edge adjacent to hotel maintains at least eight feet (8') of landscaped width, portions of that zone may be paved and used for building areas, patios, decks, sidewalks, etc. Similarly, lighter-weight, upper-

level terraces are allowable above the Internal Landscape Edge. Upper floor hotel balconies not contained within the footprint of the building shall follow the requirements outlined in the Balconies and Porches section of this Book.

5.23 Multifamily Frontage

Balconies may be incorporated into portions of the upper floor designs. Upper floor balconies not contained within the footprint of the building shall follow the requirements outlined in the Balconies and Porches section of this Book.

5.3 Building Height

Building heights and the calculation of building heights shall be as described in this PDD Standards Book, notwithstanding any provision of the LDC and without the need for a conditional use permit (“CUP”).

Building Heights Requirements and Checklist:

1. Maximum building height within the PDD shall be five (5) stories with a maximum height of seventy-five feet (75') for habitable building areas (other than the tower element described below). Non-habitable areas (e.g., roof elements, etc.) may extend above the seventy-five foot (75') height limit.
2. For gabled, hipped, and pitched roofs, building height shall be measured from finished floor elevation at grade level to a point at the bottom of the roof eave at the upper most floor. *Figure 5-4* depicts how to measure building height for these types of roofs.
3. For roofs with a parapet, building height shall be measured from finished floor elevation at grade level, to the highest point of the parapet wall.
4. Architectural features such as attics, spires, cupolas, towers, gabled and pitched roofs, green roofs, rooftop gardens, and mechanical equipment shall not be calculated as part of building height calculations.
 - a. For attics, spires, cupolas, towers (other than the Hotel Tower Element described and governed by the guidelines expressed in that section below), green roofs, rooftop gardens and mechanical equipment, a maximum additional height of up to twelve feet (12') shall be permitted. This equals a total height of up to eighty-seven feet (87')
 - b. For gabled, hipped, and pitched roofs, a maximum additional height of up to sixteen feet (16') shall be permitted, as measured vertically from a point at the bottom of the eave, to the roof ridge. This equals a total height of up to ninety-one feet (91')
5. The following design guideline pertains to ground level residential units on the Burlison frontage (“Burlison 1st Floor Units”). As illustrated in Figure X.X (to be provided by applicant), if the proposed finished floor elevation of any Burlison 1st Floor Unit exceeds five feet (5') measured from the grade of the immediately abutting sidewalk, the portion of the building in which that unit is located shall be set back from the property line a minimum of twelve feet (12'). To accommodate this change in grade, a retaining wall having a maximum height of five feet (5') may be built adjacent to the sidewalk, followed by a landscaped terrace. The landscaped terrace area may be utilized by adjacent

- apartments. Railings that follow the requirements set forth in the Balconies and Porches section of this Book may be installed above the retaining wall. Knee walls up to five feet (5') high may be installed perpendicular to the building within the landscaped terrace in order to separate one apartment's landscaped terrace from another.
6. Storage spaces, raised areas necessary to account for changes in grade, basements, and other similarly usable spaces that may be able to be tucked under buildings, but are not meant for general habitation shall not be included when calculating building height (whether or not those areas include functional space). An example of this condition is illustrated below in *Figure 5-5*.
 7. Retail spaces may have a maximum interior height up to 25'.

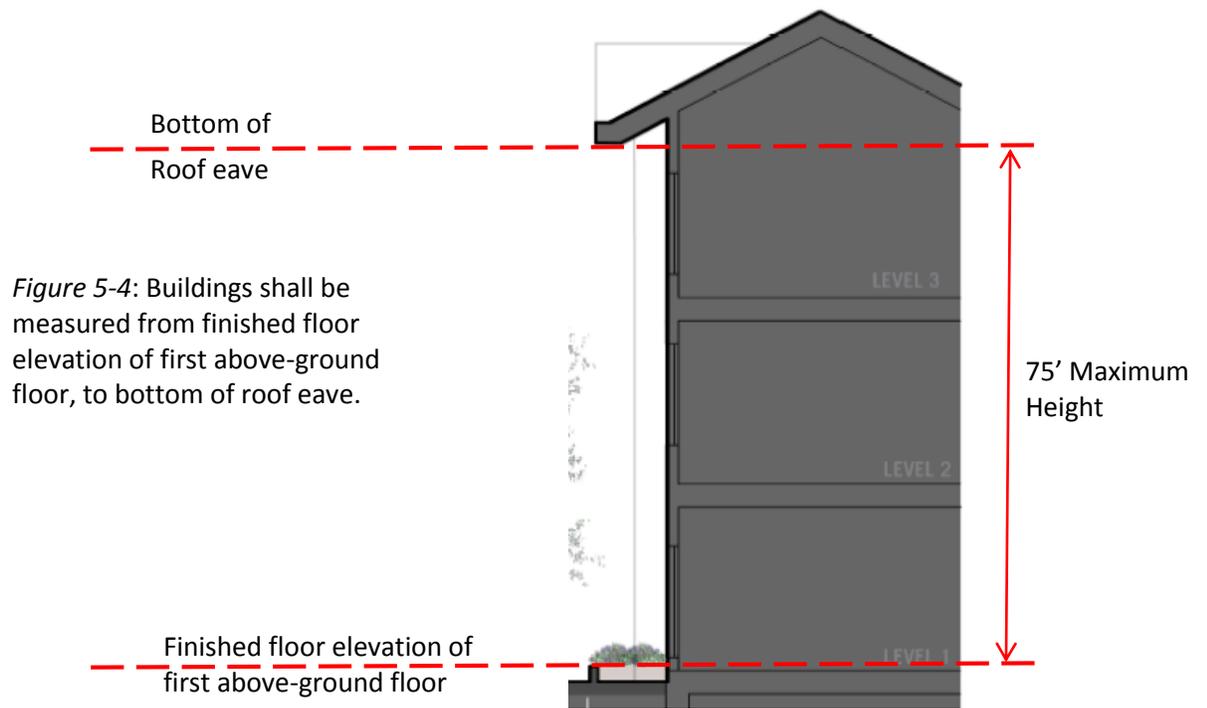


Figure 5-4: Buildings shall be measured from finished floor elevation of first above-ground floor, to bottom of roof eave.

Figure 5-5: Example of utilizing grade change to create partially subterranean space that could be provided for bicycle storage, pet washing station, general storage, basement space, etc. These spaces may not be used for general habitation and shall not be included in overall building height calculations.

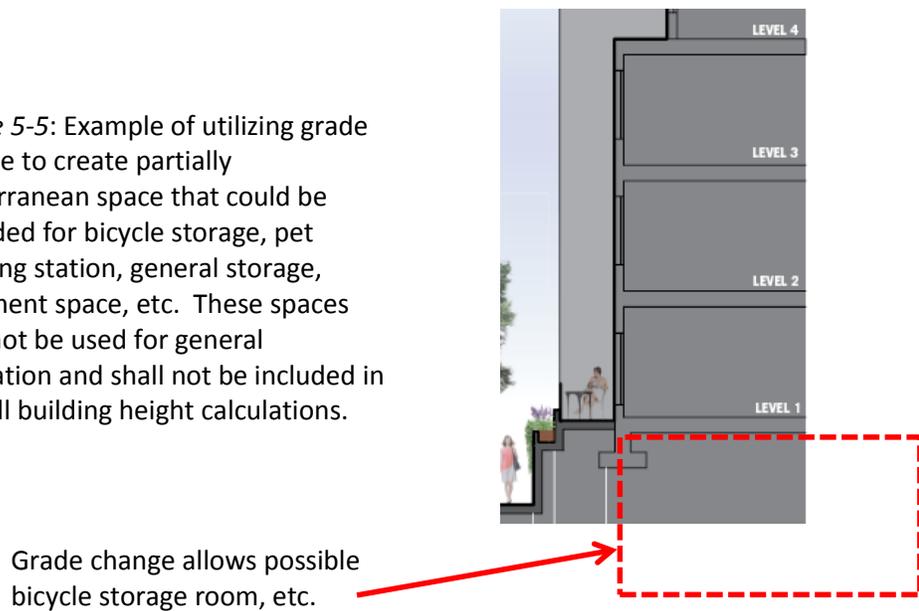
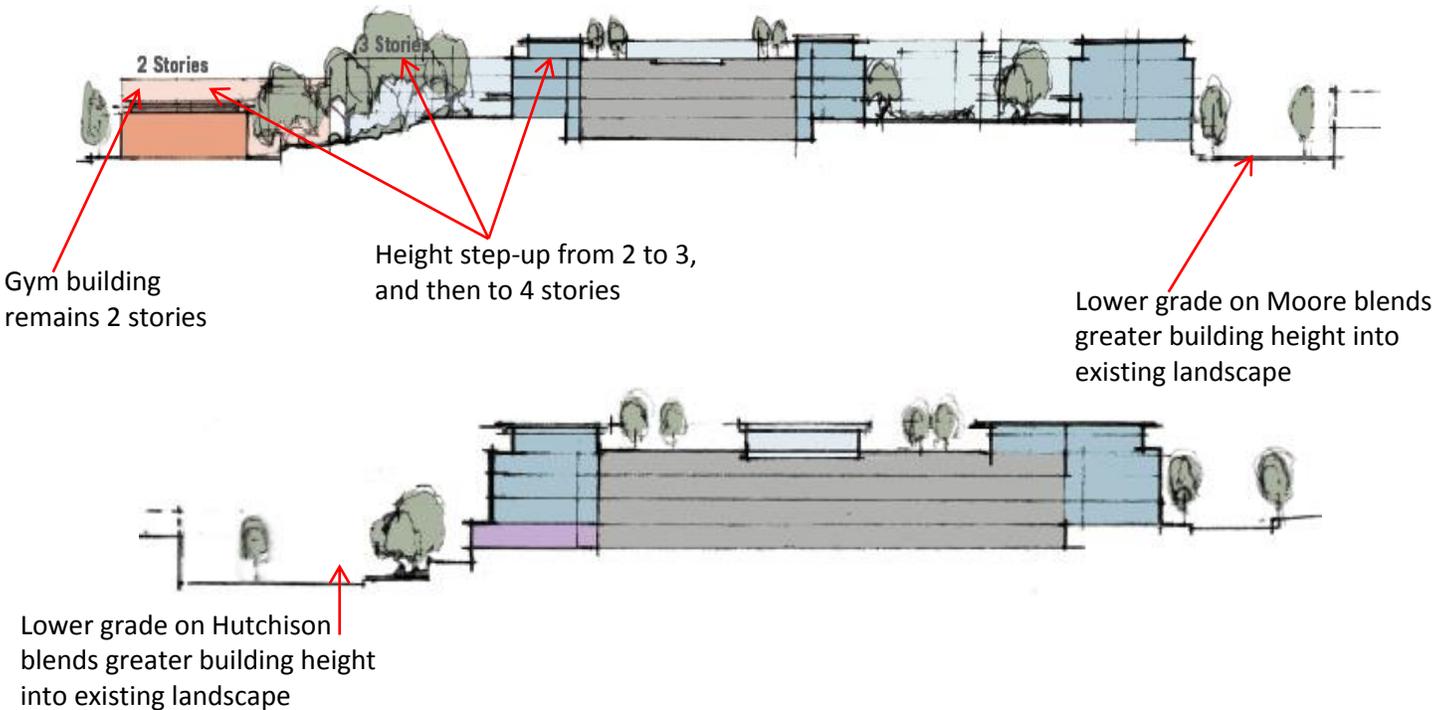
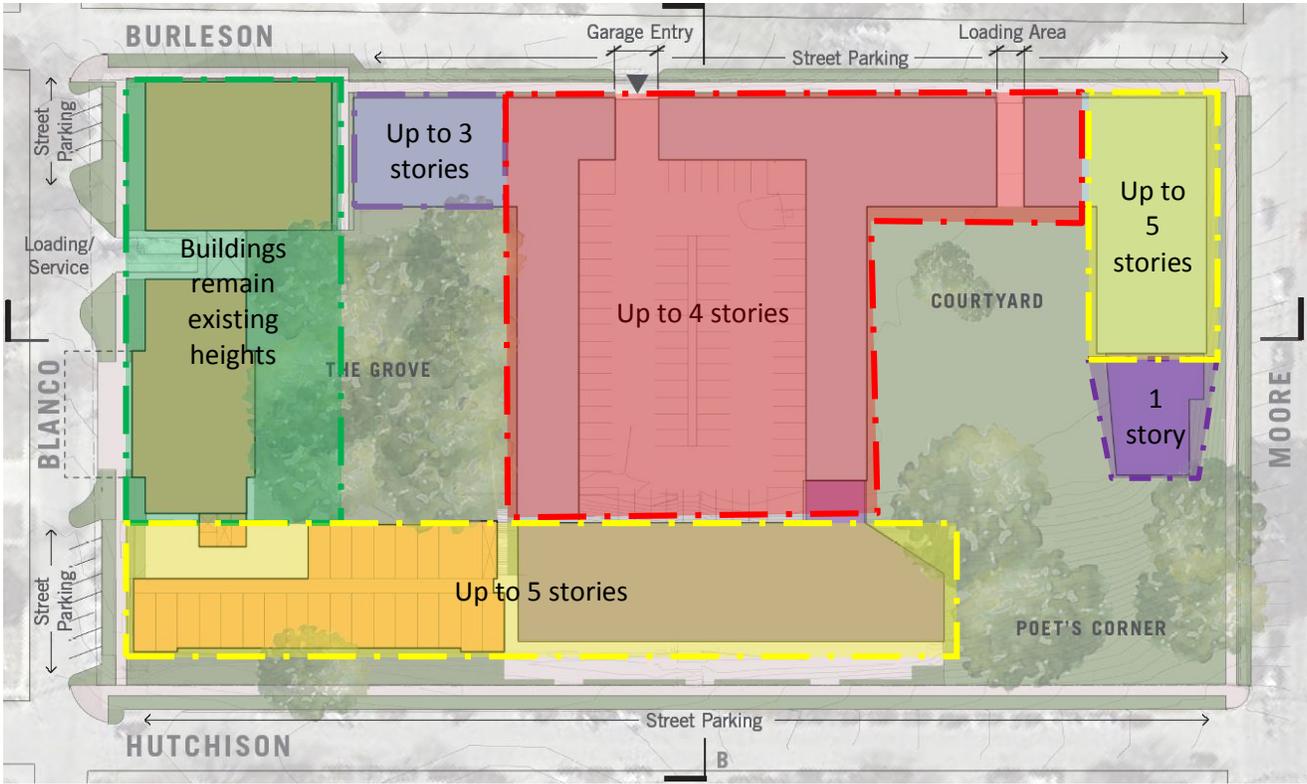


Figure 5-6: Maximum building heights throughout the Project Site.



Hotel Tower Element

The hotel at Lindsey Hill will be sited on the far side of the Project Site from Downtown San Marcos. To provide added visibility and a unique distinctive feature for the hotel and for Lindsey Hill, the developer contemplates that a tower element may be added to the top of the hotel building. *Figure 5-7* depicts the approximate location of the hotel building's tower element.

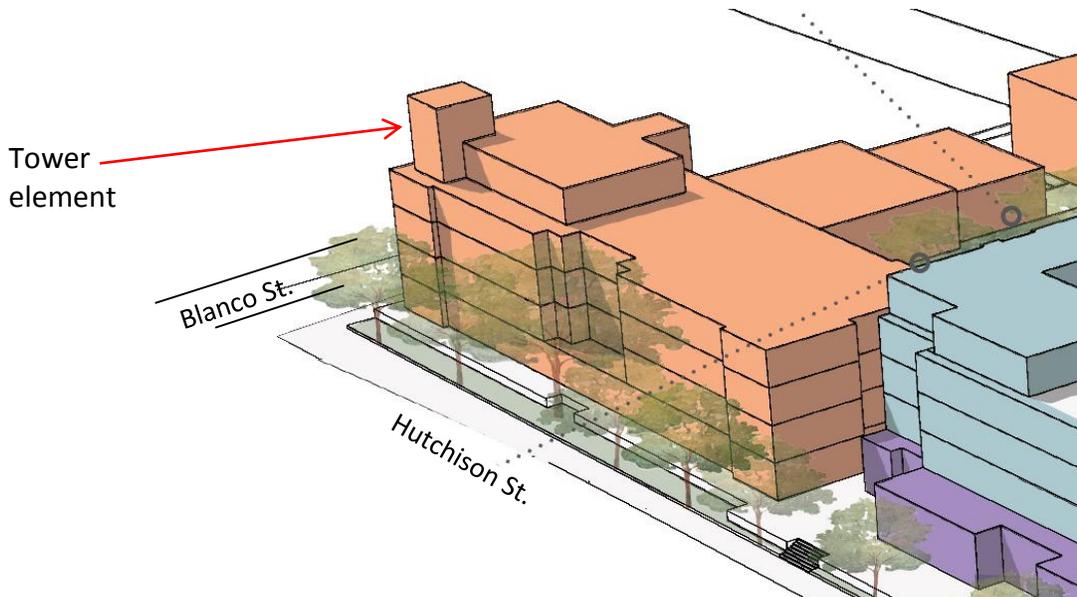


Figure 5-7: Approximate location of hotel tower element.

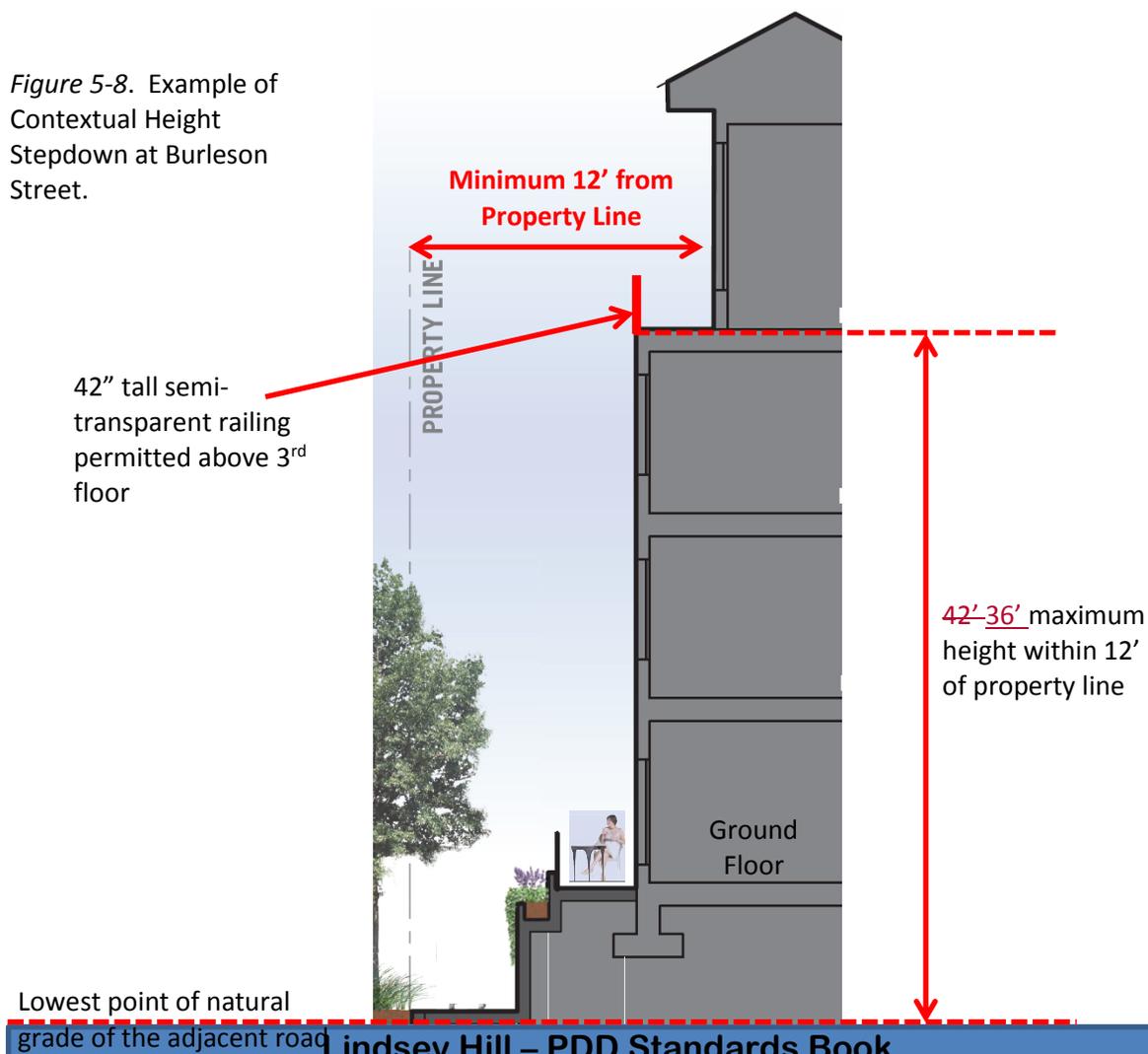
Tower Element Requirements and Checklist:

1. The main structure of the tower element may extend up to twelve feet (12') above the hotel's roof structure. The tower element's roof may extend above that 12' height limit and shall be designed in a logical proportion to the tower element itself.
2. Tower element may be usable, habitable space.
3. Tower element may be an extension of the building design, or a decorative element like a water tower or other feature.
4. Special signage (described further in the Signage section of this PDD Standards Book) shall be allowed as part of the tower element.
5. The tower shall maintain the required setbacks for both the Hutchison and Blanco Street frontages discussed above.
6. The tower element shall also maintain the Contextual Height Stepdown requirements as described below in Section 5.4.

5.4 Contextual Height Stepdown

For new construction along all street frontages, a maximum building height of thirty-six feet (~~42'36'~~) is permitted within twelve feet (12') of the property line. Due to grade change along Burleson Street however, a maximum building height of forty-two feet (42') is permitted along the eastern-most 100 feet of the Burleson Street frontage. This requirement applies to any portion of the building, including multi-story balconies not contained within the primary building footprint. Height at step down shall be measured from the lowest point of the natural grade of the adjacent road, along a line that is, as close as possible, perpendicular to the roadway, to the building's highest point that is within twelve feet (12') of the property line (except for buildings with pitched roofs, which shall be measured to the underside of roof eaves). Overhanging eaves of buildings affected by the contextual height stepdown requirement, may extend no more than 24 inches into the required 12 foot setback. Notwithstanding the foregoing, a forty-two inch (42") tall semi-transparent railing (constructed of the railing materials permitted in the Balconies and Porches section of this Book) is permitted above the maximum height in order to allow use of the stepped-back area above. An example of the Contextual Height Stepdown is depicted in *Figure 5-8*.

Figure 5-8. Example of Contextual Height Stepdown at Burleson Street.



5.5 Expression Requirements

All new buildings within the PDD shall incorporate a minimum of two Horizontal Expression Tools from the matrix below. Buildings within the PDD may incorporate more than two of the Horizontal Expression Tools if desired. Additionally, all new buildings shall incorporate Wall Offsets as a Vertical Expression Tool.

<u>Horizontal Expression Tool</u>	<u>Pictorial Depiction</u>	<u>Frontage</u>
<p>a. Varied Roof Height: An offset in parapet height (for flat roofs) or varied roof structure heights (for gabled or hipped roofs) of at least 2 ft. spaced at a minimum of every 60 ft. across the building frontage.</p>		<p>Hutchison Moore Burleson Blanco</p>
<p>b. Canopy: Canopies or awnings which run across the full width of fenestrations on the first floor façade.</p>		<p>Hutchison Blanco</p>
<p>c. Second Floor Expression Line: A line prescribed at a certain level of a building for the major part of the width of a façade, expressed by a variation in material or by a limited projection such as a molding or balcony.</p>		<p>Hutchison Moore Burleson Blanco</p>
<p>d. Cornice: A cornice detail of at least 18 in. height and 6 in. in depth for the entire width of the frontage.</p>		<p>Hutchison Blanco Moore Burleson</p>

<u>Vertical Expression Tool</u>	<u>Pictorial Depiction</u>	<u>Frontage</u>
<p>Wall Offset: Façade modules of a maximum length of 60 ft. with a minimum of a 4 ft. offset from an adjacent module.</p>		<p>REQUIRED ON ALL FRONTAGES Hutchison Moore Burluson Blanco</p>

5.6 Street Level Glazing & Transparency

In order to maintain the residential character and scale of the existing neighborhood, the following glazing and transparency requirements will be applicable for the street level (first floor) of building facades facing public streets.

Commercial street level (first floor) of building facades facing public streets:

1. Retail facades shall be glazed with no less than seventy percent (70%) clear glass on the first story.
2. Non-retail facades shall be glazed with clear glass no less than fifty percent (50%) of the first story.
3. Spacing between windows shall not exceed ten feet (10') on all new buildings.
4. For existing buildings, existing windows may be enlarged and/or relocated and additional windows may be added. However, the total window area shall not be decreased on such existing buildings. This requirement shall apply to existing windows on all floors and all levels of existing buildings.

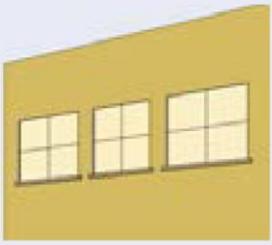
Residential street level (first floor) of building facades facing public streets:

1. All residential facades shall be glazed with clear glass no less than thirty percent (30%) of the first story.
2. Windows shall have a lower sill not more than five feet, six inches (5'6") above finished floor.
3. Spacing between windows shall not exceed ten feet (10')

5.7 Upper Floor Glazing & Transparency

For any new construction at the PDD, all upper floors of street-facing facades shall have a minimum glazed area of thirty percent (30%) and shall use a minimum of one upper floor window design tool specified in the table below.

Upper Floor Window Design Table

<p>a. Window Inset: The window pane is inset a minimum of 3 in. behind the surface of the wall.</p>	
<p>b. Window Frame: Each window opening is framed with trim that is a minimum of 1 in. depth and 2 in. width.</p>	
<p>c. Window Sill: Each window opening is defined by a sill, which extends a minimum of 2 in. from the wall surface, with a height of 3 in. and runs a minimum width equal to each window.</p>	
<p>d. Traditional Vertical Proportions: The window is proportioned similar to that of traditional buildings, with a height to width ratio of between 1.75:1 and 2.5:1. Traditionally proportioned windows may be "ganged" to create larger fenestration areas where the dividers between the windows have a depth of at least 2" and project at least 2" in front of the surface of the glass.</p>	
<p>e. True Divided Lights: Windows shall use true muntins.</p>	

5.8 Balconies and Porches

Balconies and porches either within the building façade or protruding from the building edge may be used throughout the Project, including along street frontages and adjacent to Lindsey Hill’s open spaces.

Balconies and Porches shall not protrude into Setbacks (other than as specifically described above) and may not overhang sidewalks. *Figure 5-9* depicts allowable design for upper floor balconies that are not integrated into a building façade.

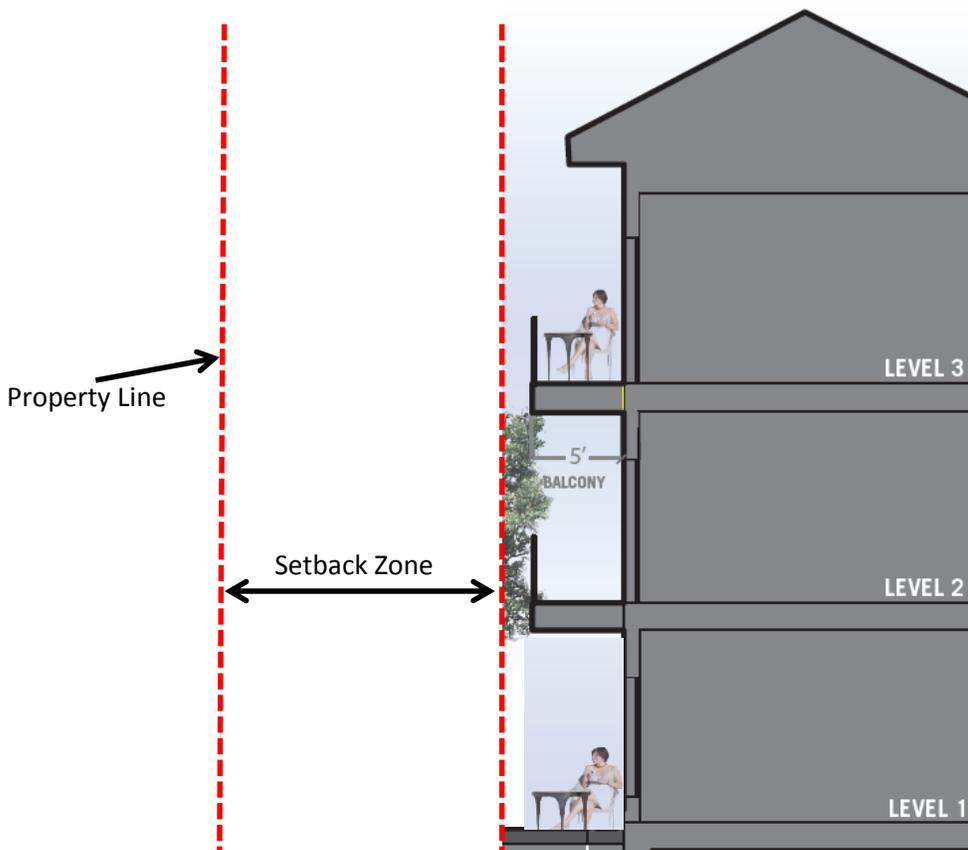


Figure 5-9: Allowable conceptual design for upper level balconies not integrated into building façade. Note that balconies must be outside of Setback Zones.

Balconies and Porches Checklist:

1. Railings must allow for visibility between the balcony and the street or other spaces below.
2. Balconies not contained within the primary building envelope and attached to the building’s exterior shall be composed of ornamental metal, including cast iron. Concrete or wood however, may be used for decking. Railings for these balconies shall also be constructed of ornamental metal, including cast iron.

3. Balconies and railings shall be designed so that they blend into the buildings' architectural form.

5.9 Retaining Walls at Street Level

Additional design consideration shall be given to retaining walls, also referred to as low walls or knee walls that may occur at street level adjacent to sidewalks due to the Project Site's diverse and varying topography.

Requirements for Retaining Walls at Street Level Adjacent to Sidewalks:

1. Shall not exceed five feet (5') in height, as measured from the grade of the immediately abutting sidewalk.
2. Shall be articulated or broken by pilasters, piers, benches, planters, entrances, steps, or a similar feature or architectural design treatment at intervals of not less than twenty feet (20').
3. Shall be articulated by a landscape bed that is a minimum of twenty-four inches (24") in depth measured from the sidewalk face of the retaining wall.

Figure 5-10 depicts the landscape bed and retaining wall height requirements.

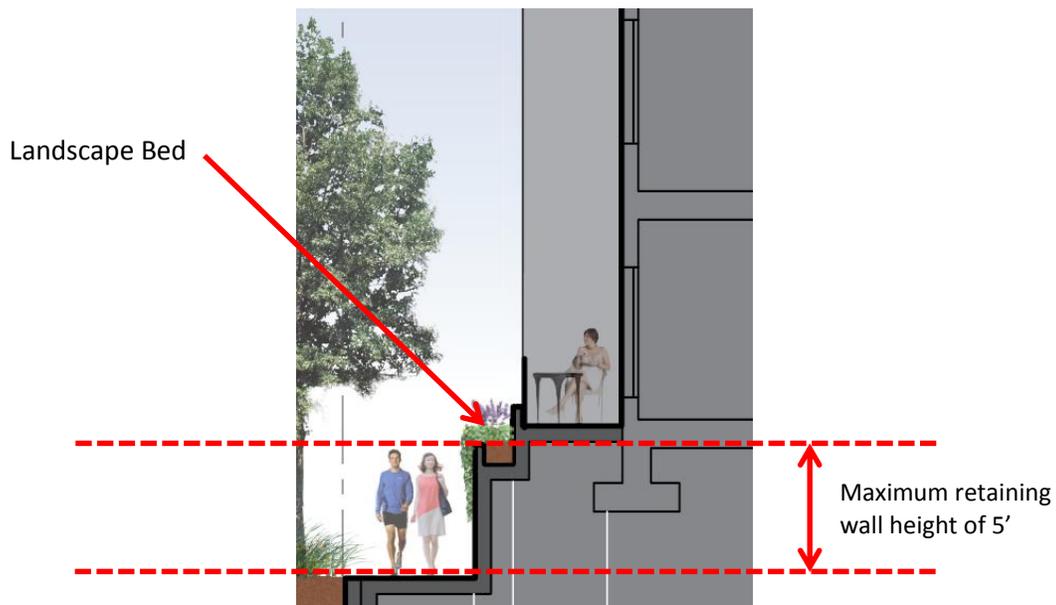


Figure 5-10: Sidewalk-adjacent retaining wall requirements.

5.10 Materials

New multifamily buildings shall be subject to LDC Section 4.4.3.2(2)(b) Multifamily Residential Design Standards, Materials. In addition to the materials permitted in LDC Section 4.4.3.2(2)(b), the following material types are permitted to be mixed into new building exterior design on a secondary basis (less than ~~twenty-ten~~ percent [~~2010~~%] of any façade) in order to allow for additional architectural detail: stained or painted wood (but not pressure treated wood), rustic wood (rough sawn), polyurethane, ~~fiberglass or other synthetic materials~~, decorative, textured or split-face concrete block, precast stone, glass fiber reinforced concrete (GFRC), Nichiha, tile, concrete (pre-cast or otherwise), and/or metal. EIFS is not a material permitted at the Project Site..

5.11 Mechanical Equipment & Utilities

With the exception of mechanical equipment installed at the loading dock behind the loading dock gates, all mechanical equipment shall be subject to the provisions of Section 4.4.3.2(1)(f) of the LDC. Rooftop mechanical equipment shall not be calculated as part of building height calculations and may extend to an additional height of up to twelve feet (12') above the maximum allowable building height for that portion of the building, and without any additional setback requirement. Rooftop mechanical equipment shall not be permitted to be mounted to gabled, hipped, or sloped roofs. Regardless, all rooftop mechanical equipment, including new and existing mechanical equipment mounted to existing buildings, shall be screened in accordance with Section 4.4.3.2(1)(f) of the LDC.

New electrical, cable, telephone, and other similar lines required to service the Project shall be buried. Such new lines serving the Project shall not be aerially mounted.

5.12 Trash Management

At Lindsey Hill, waste and recycling will be located within the loading dock and behind the loading dock gates, approximately as depicted in *Figure 4-7*. This centralized waste handling will reduce large vehicle traffic entering the streets surrounding the Project Site and will consume less land area that instead can be utilized for open space and usable building areas.

In order to provide waste disposal convenience for the Project's multifamily residents, trash chutes and waste closets will be available inside the multifamily building or in the parking garage within 350 feet of each multifamily unit.



Acceptable utility screening examples above, unacceptable below.



- 6.0 APPENDIX
 - 6.1 Use Standards
 - 6.2 Variances & Enhancements Matrix
 - 6.3 Land Survey
 - 6.4 Legal Description
 - 6.5 Hillside Slope Variance Depiction

Appendix

6.1 Use Standards

The specific changes in use listed below will clarify uses that are contemplated to be part of the Project and will create options and opportunities to allow Lindsey Hill to become an urban, walkable, bikeable project, including allowing uses that will keep the Project activated and lively.

Permitted Uses

The PDD permits all of those permitted and conditional uses, as modified, that are allowed in the MU-Mixed Use District within the LDC, which uses are outlined below in Section 6.11 – San Marcos Land Development Code Land Use Matrix. Additional Permitted Uses are outlined below in Section 6.12 – Additional Permitted Uses.

6.11 San Marcos Land Development Code Land Use Matrix

(Modifications from base standards are indicated in red)

Land Use Matrix

P = Permitted Uses

C = Conditional Uses

	MU
Barns and Farm Equipment Storage	
Bulk Grain and/or Feed Storage/Processing	
Farmers Market	P
Farms, General (Crops)	P
Farms, General (Livestock/Ranch)	
Hay, Grain, and/or Feed Sales (Wholesale)	
Livestock Sales/Auction	
Plant Nursery (growing for commercial purposes but no retail sales on site)	
Stables	
Stables (Private, Accessory Use)	
Accessory Building/Structure (No larger than 625 s.f. in size and 12' in height)	P
Accessory Building/Structure (Larger than 625 s.f. in size OR 12' in height)	C
Accessory Dwelling (One Accessory Dwelling Per Lot)	C
Bed and Breakfast Inn	P
Caretaker's/Guard's Residence	P
Community Home	P
Residential Hall or Boarding House	C
Duplex/Two-Family/Duplex Condominiums	C
Family Home Child Care	P
Four Family (Quadraplex) or Three Family (Tri-Plex)	C
Fraternity or Sorority Building	C
HUD code-Manufactured Home	
Loft Apartments	P
Multifamily (Apartments)	€ P
Single Family Detached House	P
Single Family Industrialized Home	P
Single Family Townhouse (Attached)	P
Single Family Zero Lot Line/Patio Homes	C
Armed Services Recruiting Center	P
Bank or Savings and Loan (w/o Drive-thru)	P
Bank or Savings and Loan (w Drive-thru)	C
Check Cashing Service	C
Offices (Health Services)	P
Offices (Medical Office)	P

	MU
Offices (Professional)	P
Call Service Center	
Appliance Repair	C
Artist or Artisans Studio	P
Ambulance Service (Private)	
Automobile Driving School (including Defensive Driving)	P
Automatic Teller Machines (ATM's)	P
Barber/Beauty College (barber or cosmetology school or college)	P
Barber/Beauty Shop, Haircutting (non-college)	P
Bed and Breakfast (No Permanent Residence)	P
Communication Equipment (Installation and/or Repair - No outdoor sales or storage)	
Dance/Drama/Music Schools (Performing Arts)	P
Extended Stay Hotels/Motels (Residence hotels)	€ P
Exterminator Service (No outdoor sales or storage)	P
Funeral Home	
Hotel/Motel	P
Kiosk (Providing A Retail Service)	P
Laundry/Dry Cleaning (Drop Off/Pick Up)	P
Martial Arts School	P
Medical Supplies and Equipment	P
Mini-Warehouse/Self Storage Units	C
Off-Premises Freestanding Sign	
Photocopying/Duplicating/Copy Shop	P
Studio for Radio or Television (without tower)	P
Tool Rental (Indoor Storage only)	P
Tool Rental (with Outdoor Storage)	
Washateria/Laundry (Self Serve)	
All Terrain Vehicle (go-carts) Dealer/Sales	
Antique Shop (with outside storage)	€ P
Auto Dealer (Primarily New/Used Auto Sales as accessory use only)	
Auto Dealer, Used Auto Sales	
Auto Supply Store for New and Rebuilt Parts	C
Bike Sales and/or Repair	P
Building Material Sales	

	MU
Cabinet Shop (Manufacturing)	
Convenience Store Without Gas Sales	P
Convenience Store With Gas Sales	C
Department Store	
Food or Grocery Store with Gasoline Sales	C
Food or Grocery Store without Gasoline Sales	P
Gravestone/Tombstone Sales	
Gun Smith	
Home Improvement Center (10,000 s.f. or more)	C
Lawnmower Sales and/or Repair	C
Liquor Sales (retail)	P
Market (Public, Flea)	C
Pharmacy	P
Plant Nursery (Retail Sales/Outdoor Storage)	€ P
Recycling Kiosk	P
Restaurant/Prepared Food Sales	P
Restaurant/Prepared Food Sales with beer/wine off-premises consumption	P
Restaurant/Prepared Food Sales with drive thru	C
Retail Store (Misc.) with Drive Thru Service	C
Retail Store (Misc.) without Drive Thru Service (Under 100,000 s.f. Bldg.)	€ P
Retail Store (100,000 s.f. or more Bldg.)	
Retail Store (over 10,000 s.f. or more Bldg.) outside sales	C
Retail Store (under 10,000 s.f. or more Bldg.) outside sales	€ P
Retail Store (under 10,000 s.f. or more Bldg.) no outside sales	P
Security Systems Installation Company	
Shopping Center (Over 5 Acres)	
Studio Tattoo or Body Piercing	C
Temporary Outdoor Retail Sales / Commercial Promotion (4 day time limit; Permit Required by Building Official)	P
Upholstery Shop (Non-Auto)	
Veterinarian (Indoor Kennels)	€ P
Woodworking Shop (Ornamental)	P
General Vehicular Sales and Service	C
Auto Body Repair	
Auto Muffler Shop	
Auto Paint Shop	
Auto Repair (General)	C
Auto Repair as an Accessory Use to Retail Sales	C

	MU
Auto Tire Repair/Sales (Indoor)	C
Auto Wrecker Service/Tow Yard	
Car Wash (Self Service; Automated)	C
Full Service Car Wash (Detail Shop)	C
Heavy Load Vehicle Sales/Repair	
Limousine/Taxi Service	C
Public Garage/Parking Structure	€ P
Tire Sales (Outdoors/Storage)	
Transit Terminal	
Truck Terminal	
Amusement Services or Venues (Indoors)	€ P
Amusement Services or Venues (Outdoors)	C
Bar	C
Billiard/Pool Facility/Nightclub (Three or More Tables) No alcohol consumption	
Smoking Lounge	C
Broadcast Station (with Tower)	
Charitable Gaming Facility	
Civic/Conference Center	P
Country Club (Private)	C
Dance Hall/Dancing Facility	
Day Camp	
Driving Range	
Fair Ground	
Health Club (Physical Fitness; Indoors Only)	P
Motion Picture Theater (Indoors)	€ P
Motion Picture Studio, Commercial Film	
Museum (Indoors Only)	P
On-Premises Consumption of Alcohol	C
Park and/or Playground	P
Travel Trailers/RVs (Short Term Stays)	
Rodeo Grounds	
RV/Travel Trailer Sales	
Tennis Court (Lighted)	C
Theater (Non-Motion Picture; Live Drama)	€ P
Adult Day Care (No Overnight Stay)	P
Assisted Living Facility/Hospice	P
Cemetery and/or Mausoleum	

	MU
Child Day Care (Business)	P
Meeting Place/Nonreligious	P
Place of Religious Assembly/Church	P
Clinic (Medical)	P
Electrical Generating Plant	
Electrical Substation	C
Emergency Care Clinic	P
Fraternal Organization/Civic Club	P
Governmental Building or Use (Municipal, State or Federal)	P
Heliport	C
Helistop (Non-Emergency)	
Household Care Facility	C
Hospital (Acute Care/Chronic Care)	C
Nursing/Convalescent Home	P
Philanthropic organization	P
Post Office (Private)	P
Post Office (Governmental)	P
Rectory/Parsonage with Place of Worship	P
Retirement Home/Home for the Aged	P
School, K through 12 (Private)	€ P
School, K through 12 (Public)	P
School, Vocational (Business/Commercial Trade)	€ P
University or College	€ P
Auction Sales (Non-Vehicle)	
Bio-Medical Facilities	
Caterer	P
Extermination Service	
Feed and Grain Store	
Furniture Manufacture	
Maintenance/Janitorial Service	C
Manufactured Home Sales	
Metal Fabrication Shop	
Moving Storage Company	
Portable Building Sales	
Taxidermist	
Transfer Station (Refuse/Pick-up)	
Veterinarian (Outdoor Kennels or Pens)	
Warehouse/Office and Storage	
Welding Shop	
Aircraft Support and Related Services	
Airport	
Manufacturing	

	MU
Contractor's Office/Sales, With Outside Storage including Vehicles	
Distribution Center	
Electronic Assembly/High Tech Manufacturing	
Engine Repair/Motor Manufacturing Re-Manufacturing and/or Repair	
Food Processing (no Outside Public Consumption)	
Laboratory Equipment Manufacturing	
Leather Products Manufacturing	
Machine Shop	
Manufacturing Processes not Listed	
Marble or Stone Finishing	
Micro Brewery (onsite mfg. and sales)	C
Motor Freight Terminal	
Outside Storage (as primary use)	
Paint Manufacturing	
Petroleum Bulk Storage	
Plastic Products Molding/Reshaping	
Research Lab (Non-Hazardous)	P
Sand/Gravel Sales (Storage or Sales)	
Sign Manufacturing	
Stone/Clay/Glass Manufacturing	
Wrecking/Junk Yard	

6.12 Additional Permitted Uses

The following are additional By Right/permitted uses within the PDD:

1. Civic, civic support, and similar gathering uses including but not limited to outdoor parks, gardens, civic spaces, public art, fountains, libraries, and indoor or outdoor auditoriums and assembly spaces, community gathering spaces, and other similar spaces whether at grade, on amenity decks, or on rooftops, and any storage, improvements, or amenities related to such spaces.
2. Bus shelters, fire and police stations.
3. Outdoor conference, meeting, gathering, exhibit, theater, movie, entertainment, and event spaces as part of the hotel facility, apartment complex, open spaces, or mixed-use complex.
4. Indoor or outdoor galleries and exhibition type uses.
5. Push carts and transient vendors.
6. Art markets, craft and food markets and festivals, and other similar public markets.
- ~~7. Surface parking areas (permitted only within the area along the Blanco Street frontage where on-street parking will be partially within the Project Site's boundary).~~
- ~~8.7.~~ Wireless transmitters and radio/television communications towers extending not more than 50' above the highest element where it is sited, provided that such elements are located horizontally within 150' of the right-of-way line of Moore St.
- ~~9.8.~~ Manufacturing as part of an artist's or artisan's studio and/or retail operation, including within any live/work spaces.
- ~~10.9.~~ R&D offices and research laboratories (up to and including BSL-2 labs as defined by the Centers for Disease Control).
- ~~11. Micro breweries and micro distilleries as part of an on-site retail or food service operation (see Note below).~~
- ~~12.10.~~ Outdoor/sidewalk sales as an ancillary part of a retailer's existing operations within the PDD or in connection with special events at the Project Site.
- ~~13.11.~~ Electrical substations, generators, and/or similar or related equipment ancillary to the needs of Lindsey Hill, but not for offsite purposes.
- ~~14.12.~~ Indoor and outdoor health and fitness facilities (including, but not limited to indoor facilities that may open to the outdoors).
- ~~15.13.~~ Storage units and areas for tenants/residents of Lindsey Hill (so long as such storage areas are not visible from the public right-of-way).
- ~~16.14.~~ Retail buildings, retail stores, open-market buildings.
- ~~17. Sale of alcohol for on-premises consumption (including, but not limited to within bars and restaurants, etc.) shall be a permitted use, provided such businesses follow governmental licensure requirements (see Note below).~~

~~**Note:** Uses incorporating the sale of alcohol are intended to be permitted uses within the PDD. However, to the extent that any such use is required to follow the City of San Marcos Conditional Use Permitting process for the sale of alcohol (the "CUP Process"), that process first shall be followed such that the use will be considered conditional (but conditioned only on the satisfaction of the CUP Process) until the CUP Process is satisfied. Upon satisfaction of the CUP Process, that use will be a permitted use.~~

6.2 Variances & Enhancements Matrix

Note: This document is offered as a “quick-reference” tool to make navigation of the PDD Standards Book easier and more convenient. Please refer to PDD Standards Book for complete details. Variances, enhancements, modifications, and other rights described either in the PDD Standards Book or in this matrix shall apply to the PDD, even if any such item is not expressly mentioned both in this document and also in the PDD Standards Book.

PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
6.1, 6.11, 6.12	LDC 4.3.1.2 4.2.2.1(b)	Permitted Uses	Land Use Matrix includes numerous conditional and permitted uses.	Various use changes consistent with Lindsey Hill’s vision and mixed-use goals.	Modification
5.1	LDC 4.1.6.1 4.2.2.1(d)(4) 4.4.1.1(d)(3)	Setbacks, Front Yard Measurement	25’ front yard setback 7.5’ side yard setback 15’ corner side setback 5’ rear yard setback Additional rear setback for heights greater than 24’.	Setbacks by frontage and as described in PDD Book. Hutchison – 16’ Moore – 10’ Burleson – 8’ Blanco – 17’ No additional setback required for permitted building heights.	Modification
5.3	LDC 4.1.6.1 4.2.2.1(d)(5) 4.4.1.2(a)	Maximum Building Height; Calculation of Building Height	4 stories, with CUP required for greater height. Heights calculated based on street grade and roof height.	5 stories, no CUP needed.	Modification
5.3 5.11	LDC 4.4.1.2(b)	Mechanical Equipment, Pitched Roofs, and Decorative Features Heights	Up to 50’ above average grade line of building, but in some cases with additional setback requirements.	Up to 12’ above building for mechanical equipment. Pitched roofs up to 16’ of additional height. Tower Element has specific allowances, but a 12’ max additional main structural height.	Modification

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PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
5.3 5.4	LDC 4.4.3.2(1)(o)(i) 4.4.3.2(1)(a)	Residential Compatibility	Maximum building height of 30 feet shall apply to portions of a structure within 70 feet of a single family zoning designation (measured from centerline of street).	Maximum height of 42' <u>36'</u> is permitted within 12 feet of the property line along all street frontages. <u>Maximum height of 42' permitted at specific locations.</u> For pitched roofs, height measured to underside of eave.	Modification
5.3	LDC 4.1.6.1	Maximum Floor Height	14' from finished floor to finished ceiling.	Retail spaces have a max interior height of 25'. Gym and auditorium buildings permitted to keep existing ceiling heights.	Modification & Enhancement
1.1 3.7	LDC 4.1.6.1 4.2.2.1(c) 5.1.1.5 Table 4.1.6.1	Impervious Cover	60% maximum	85 <u>80</u> % maximum impervious cover permitted. Impervious cover over 60% to include pollutant removal through PBMPs and enhanced TSS removal.	Modification & Enhancement
1.1	LDC 4.1.6.1	Units Per Acre	5.5	164 total multifamily units permitted. (34/ac)	Modification
	LDC 5.1.1.3 7.5.1.7	Runoff Attenuation; Impervious Cover Calculation		Project receives credit for remaining and/or replacement of existing impervious coverage.	Modification
1.1	LDC 5.1.1.5 and 7.5.1.4	Hillside Impervious Cover	Limits hillside impervious cover based on slope: 35% coverage permitted for 15-25% slopes, 20% coverage permitted for slopes greater than 25%.	15-25% slopes and slopes greater than 25% permitted additional impervious coverage pursuant to Section 6.5.	Modification
4.0 5.1	LDC 4.4.1.1(d)(3) Figure 4-8	Front Porch Dimensional Limits	Up to 30" above grade within front yard. Porch without posts may extend into front yard up to 4'.	No height or distance limits within front yard, but must follow Building Setbacks section requirements.	Modification & Enhancement

PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
5.5 5.6 5.7	LDC 4.4.3.2(2)(a) 4.4.2.2(c)	Façade Design Criteria	Requires certain minimum façade articulation and design elements in order to maintain visual interest.	Supplements with additional façade articulation methods, windows, balconies, and other façade-related requirements.	Enhancement & Clarification
5.10	LDC 4.4.3.2(2)(b)(i)	Multifamily Exterior Materials	Provides a list of materials allowed to be utilized within multifamily building exteriors.	Maintains the existing LDC requirement such that those materials continue as the primary building material options, but provides for additional materials that may be utilized for additional architectural detail (limited to 20 10% of exteriors).	Modification & Enhancement
Figure 1.1	LDC 4.4.3.2(2)(f)	Street-Facing Garages	No garages allowed on facades facing street.	Structured parking garage entrance (but no other garages) will face street.	Modification
5.6	LDC 4.4.3.2(2)(g)	Residential Glazing & Transparency	Multifamily street facing facades shall have minimum glazed area of 20%.	Residential facades shall have minimum glazed area of 30% at ground level.	Enhancement
5.6	LDC 4.4.2.2	Commercial Glazing & Transparency	No standard.	1. Retail facades shall have minimum glazed area of 70% at ground level. 2. Non-retail facades shall have minimum glazed area of 30 50% at ground level. 3. Specific allowances for repurposed buildings.	Enhancement
5.2	LDC 4.4.3.2(2)(g)	Operable Windows	All walls and elevations on all floors of MF buildings must contain operable windows.	A minimum of 50% of multifamily building windows shall be operable.	Modification

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PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
3.2	LDC 6.1.1.4(b) Table 6.1.1.4	Minimum Landscaped Percentage	20% of total lot shall be devoted to landscaping in Mixed-Use Districts.	PDD requires 15% minimum total lot landscaping.	Modification
4.5 4.4	LDC 6.1.1.4(b)(2)	Street Trees; Street Tree Clustering	Street trees required every 50' on average.	Street trees required every 30', except along Blanco Street where street trees may be clustered.	Modification & Enhancement
3.2	LDC 6.1.1.4(c)	Location of Landscaping	75% of required landscaped area in Table 6.1.1.4 to be within streetscape areas.	No minimum percentage required along streetscapes. Landscaping shall exhibit a visible priority for areas with greatest visibility from the street.	Modification
4.0 4.2 4.3 5.1	LDC 6.1.2.1(b)(1) 6.1.2.1(b)(2)	Non-residential and Multifamily Screening and Setback Requirements	6' opaque fence and additional setbacks required for screening of MF or commercial uses.	Screening and setbacks throughout the Project Site shall be as required within the PDD Standards Book.	Modification & Enhancement
4.4	LDC 4.2.2.1(d)(7) 6.1.2.2	Parking Area Screening	Nonresidential parking areas to be screened from SF uses. Parking spaces shall be buffered from street view and from adjacent properties.	No screening or buffering required for portion of Blanco St. parking area that is within the Project Site.	Modification & Clarification
4.2	LDC 6.1.2.3	Screening Along Roadways	Where rear or side yards of a subdivision are adjacent to an arterial, a 6' tall masonry wall or other form of screening is required. Any such screening requires a maintenance easement to HOA or City.	Each frontage of the Project shall be considered a front yard.	Clarification
3.42 3.53	LDC 6.1.3.1(b)(1)	Gates for Vehicular Access	Gates for vehicle access to be set back 24' from property line.	Parking garage and loading dock gates shall be setback a minimum of 4' from the building face.	Modification

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PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
3.11 3.53 4.2	LDC 6.1.3.3(a)(4)	Fences Between Buildings on the Same Lot	Maximum fence height of 8’.	Maximum fence height of 10’, with specific transparency requirements.	Modification
3.11	LDC 4.4.3.2(1)(i)i)	Pedestrian Access	Pedestrian entrances shall connect sidewalks to internal walkways and shall not be gated.	The Pedestrian entrances illustrated in <i>Figure 3-1</i> may be gated to separate public and private areas.	Modification
2.1	LDC 7.6.1.2 City Council Resolution	Parkland Dedication	Parkland dedication, or a fee-in-lieu shall occur when new residential subdivisions are developed. Credit toward fee-in-lieu shall be given for developer-provided improvements that benefit the quality and character of the subdivision.	Project will meet Open Space requirement for PDD, but not LDC. Fee-in-lieu to be paid, but fee-in-lieu credit shall be given to Project for Poet’s Corner improvements that benefit the character and quality of the Project.	Enhancement
3.4	LDC 6.2.1.2	Parking	Describes the parking requirements under the LDC.	Parking standards shall follow the requirements of the PDD Standards Book (which is based on a parking study).	Modification
3.53	LDC 6.2.1.2(j)	Loading Space and Maneuvering	All vehicular maneuvering required for use of loading spaces shall occur outside of ROW.	Loading dock maneuvering allowed within ROW, but limited to PDD Standards Book requirements. Minimum of single-berth loading dock permitted for the PDD.	Modification
3.41	LDC 6.2.2.1(a)	Public Street Parking	Public street parking shall not be counted toward off-street parking requirements.	Street spaces touching the PDD on Blanco, Burleson, and Hutchison shall be counted toward off-street parking requirements.	Modification
3.42	LDC 6.2.2.1(d)	Parking Space Dimensions	Minimum 9’ parking space width for non-parallel spaces.	Minimum 8’6” parking space width for non-parallel spaces.	Modification

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PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
3.53 4.5	LDC 6.2.2.1(g)	Backing into Public Streets and Sidewalks	No parking space shall require backing into a public street or across a sidewalk.	Blanco Street spaces (which mostly are within the ROW, but may partially be within the Project Site) only will back into public street. Loading dock will require backing across sidewalk.	Modification
4.5	LDC 6.2.2.1(n)	Parking Space Setbacks	Parking spaces must be set back a minimum of 5' from ROW.	Blanco Street spaces are partially on the Project Site and partially within ROW.	Modification & Enhancement
3.51 4.4 4.5		Use of Public ROW for Porte-Cochere and/or Valet Parking		Notwithstanding anything in this Book to the contrary, any use of public right-of-way for valet parking or the porte-cochere shall be subject to a mutually-agreeable lease or use agreement with the City, and the City shall be under no obligation to enter into any such agreement or lease.	
3.6	LDC 6.3.1.2	Sign Standards Applicability	When two sign standards conflict, the more restrictive standards shall apply.	If PDD sign standards conflict with Base Regulation standards, PDD standards shall govern and control.	Clarification
3.42 4.5	LDC 6.3.1.7(a)(5)	Signage Restrictions	Parking spaces may not be blocked off by signage except for public safety needs.	Spaces or sections within the parking garage and/or on Blanco Street can be blocked off for repair, special events, valet, etc. using signs, cones, pylons, etc.	Modification
3.6 5.3	LDC 6.3.1.7(a)(7)	Signage Restrictions	Only government or holiday signs can be on roofs.	Tower element signage permitted per PDD Book.	Modification

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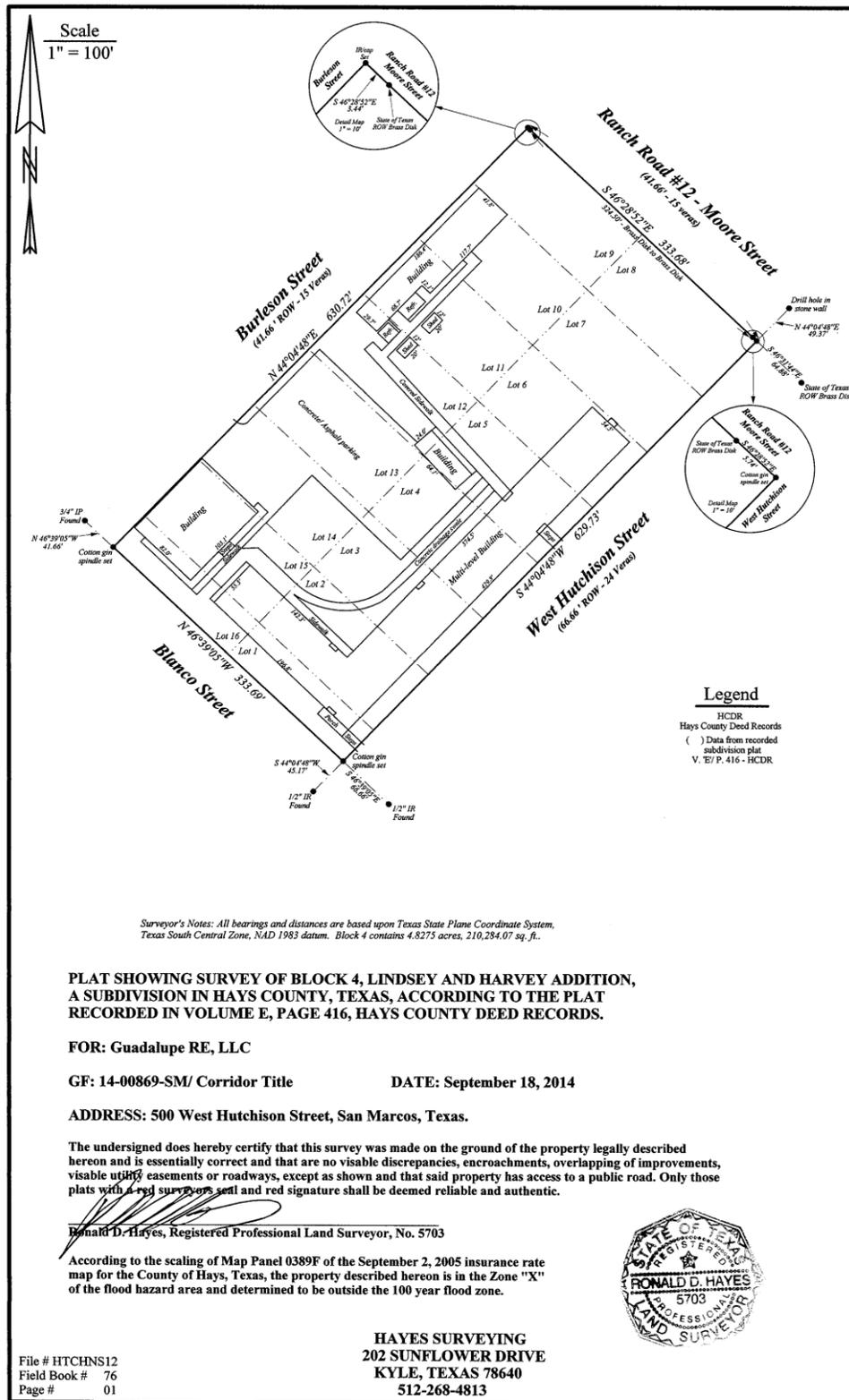
PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
3.6	LDC 6.3.1.7(a)(10)	Signage Restrictions	Sidewalk signs permitted only in CBA, but must maintain 5' wide clear sidewalk path.	Sidewalk signs (sandwich board, spring base, etc.) permitted at the PDD. 6' wide clear sidewalk required.	Modification
3.6	LDC 6.3.1.7(a)(11)	Signage Restrictions	All signs to have a 5' minimum setback from all property lines.	Monument signs, retaining wall signs, and canopy signs shall be installed behind sidewalks. Sidewalk signs permitted within sidewalk areas.	Clarification
3.6	LDC 6.3.3.2(a)	Signage Area Calculation	Base Regulation does not provide clarity on how to measure façade to determine allowable sign area.	With some modification, line by line incorporation of SmartCode signage regulations, which are more restrictive.	Enhancement
3.6	LDC 6.3.3.2(a)(1)	On-Premises Attached Signs	Attached signs shall not extend above roofline or into setback zone.	Hotel tower element signage and certain projecting signs may extend above roofline. Canopy, projecting, hanging, retaining wall, sidewalk, and monument signs permitted within setback zones on a limited basis.	Modification
3.6	LDC 6.3.3.2 6.3.3.3	Sign Types	LDC includes two broad categories of permitted on-premises signage.	PDD Standards Book provides detailed descriptions of permitted signs in Section 3.6.	Enhancement & Clarification
3.6	LDC 6.3.3.3(a)	Sign Types	Pole signs permitted in certain locations.	Pole signs are not permitted at the PDD.	Enhancement
3.6	LDC 4.4.3.2(1)(n)ii) (4)	Sign Illumination	Monument signs shall not be internally lit.	Parking monument sign, and signage located along Moore St. may be neon (or LED equivalent), halo, or internal illumination.	Modification

PDD Sections	CoSM Sections	Subject	Existing Regulation	PDD Modification	Modification, Clarification, Enhancement
3.6	LDC 6.3.3.5(c)	Temporary Signs	Temporary signs limited to one per street frontage and two total per lot.	A maximum of two temporary signs shall be permitted on each of the Hutchison, Moore, and Blanco Street frontages.	Modification
1.1 1.16 3.12 3.4 3.5 4.4 4.5	LDC 4.4.3.2(1)(c)	Parking Site Design	Provides requirements for parking lot locations, curb stops, bicycle parking, and visitor parking signage and location.	Permitted parking locations and parking requirements are detailed in PDD Standards Book. Bicycle parking shall not be calculated redundantly. No distance-to-door requirements for bike parking. Covered bike parking shall be in parking garage. No requirement to label visitor parking.	Modification & Clarification
	4.4.3.2(1)(j)	Neighborhood Parking Programs	Developer to pay a fee toward neighborhood parking program based on project impact.	No fees required based on City's review and approval of the Project's parking program as being appropriate for Project's overall mixed-use needs.	Clarification
	LDC 4.4.3.2(1)(k)	Public Transit Facility Lighting	Lighting to be provided along pedestrian walkway connections and adjacent to transit stops.	Lighting to be provided for pedestrian walkway connections and transit stops located at or immediately adjacent to the Project Site.	Clarification
5.12	LDC 4.4.3.2(1)(m) ii	Trash/Dumpsters for Multifamily	Trash and recycling dumpsters to be provided within 500 feet of entrances to ground floor units.	Trash chutes are required within 350' of each multifamily unit.	Enhancement

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6.3 Land Survey



6.4 Legal Description

METES & BOUNDS DESCRIPTION FOR BLOCK FOUR (4), LINDSEY AND HARVEY ADDITION

A tract of land containing 4.8275 acres, being all of Block Four (4), of the Lindsey and Harvey Addition, a subdivision in Hays County, Texas, according to the plat recorded in Volume "E", Page 416 of the Hays County Deed Records, and being more particularly described by metes and bounds as follows:

BEGINNING at a cotton gin spindle set at the intersection of the northeast right-of-way of Blanco Street with the southeast right-of-way of Burleson Street for the northwest corner of Block 4;

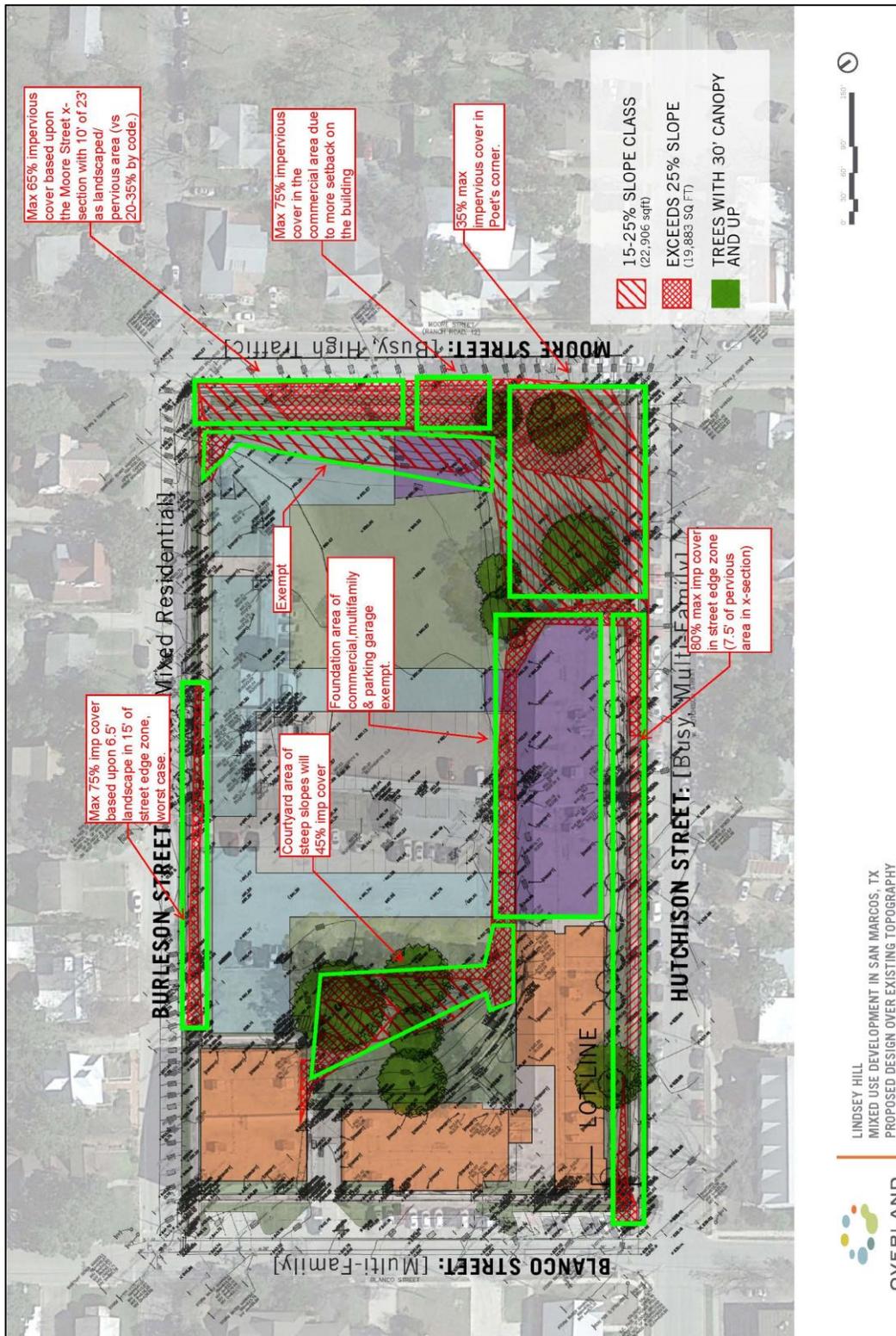
THENCE N 44° 04' 48" E, along Burleson Street right-of-way, a distance of 630.72 feet to an iron rod with a cap set at the intersection of the southeast right-of-way of Burleson Street with the southwest right-of-way of Ranch Road #12, Moore Street, for the northeast corner of Block 4;

THENCE S 46° 28' 52" E, along the Ranch Road #12 right-of-way, a distance of 333.68 feet to a cotton gin spindle set at the intersection of the southwest right-of-way of Ranch Road #12 with the northwest right-of-way of West Hutchison Street for the southeast corner of Block 4;

THENCE S 44° 04' 48" W, along the West Hutchison Street right-of-way, a distance of 629.73 feet to a cotton gin spindle set at the intersection of the northwest right-of-way of West Hutchison Street with the northeast right-of-way of Blanco Street for the southwest corner of Block 4;

THENCE N 46° 39' 05" W, along the Blanco Street right-of-way, a distance of 333.69 feet to the POINT OF BEGINNING, containing all of Block 4, 4.8275 acres.

6.5 Variance from LDC Section 5.1.1.5 & 7.5.1.4 "Hill-Side Slopes"



City of San Marcos

ZONING CHANGE APPLICATION

	<u>APPLICANT</u>	<u>PROPERTY OWNER</u>
Name:	Guadalupe RE, LLC	Same as Applicant
Mailing Address:	c/o David Lerman 1519 Spruce St. Philadelphia, PA 19102	_____
Telephone No.:	215-545-4545	_____
E-mail address:	dlerman@arqeo.com	_____

PROPERTY DESCRIPTION:

Street Address: 500 W. Hutchison St.

Subdivision: Lindsey & Harvey Addition Block: 4 Lot(s): _____

Other Description (if unplatted) _____ metes and bounds description attached
 * a metes and bounds description is required if property is a partial lot or is not platted

Appraisal District Tax ID No.: R 143037 Acres: 4.8275

Lien Holder(s) - for notification purposes:
 Name: Rosalie C. Lerman

Mailing Address: c/o David Lerman, 1519 Spruce St., Philadelphia, PA 19102

(If more than one lien holder, please provide information on a separate page)

A certificate of no tax delinquency must be attached to this application

ZONING CHANGE INFORMATION:

Zoning Designation: Current: P, Public and Institutional Requested: MU, Mixed Use

Master Plan Designation: Area of Stability Land Use Map Amendment Required? No

Present Use of Property: SMCISD school and back office functions

Desired Use of Property/Reason for Change: Mixed use PDD development (for more information, please see the PDD application submitted for this property)

I certify that the information in this application is complete and accurate.

I am the property owner of record; or

I have attached authorization to represent the owner, organization, or business in this application.

Signature: Guadalupe RE, LLC Date: 4/27/2015
 By: Mark Berins, authorized agent

Printed Name: _____

APPLICATION FOR CITY OF SAN MARCOS ZONING CHANGE

FEE INFORMATION:

Fee Schedule:

Zoning Change to MF 12, 18, 14	\$1,000 plus \$50 acre (\$3,000 maximum) + \$10.00 Technology Fee
Zoning Change to all other categories	\$750 plus \$50 acre (\$2,000 maximum) + \$10.00 Technology Fee
Zoning variance/Special Exception	\$600 + \$10.00 Technology Fee
Renotification fee	\$75 + \$10.00 Technology Fee

APPLICATION PROCESS:

Please be advised that this is a 2-3 month process. The Planning and Zoning Commission will conduct a public hearing to consider your request. Prior to the hearing, the City will mail notices to all property owners within 200 feet of the subject tract, to the listed applicant and property owner, to any lien holders, and to the appropriate neighborhood representative. A sign advertising the change will also be placed on the property by the City.

At the public hearing the applicant, or a representative for the applicant, should be present to answer any questions the Commission may have. Failure to appear could result in your request being tabled or denied. Those in support of the request and those in opposition will be given an opportunity to speak. Following the close of the public hearing, the Planning and Zoning Commission will make a recommendation to either approve or deny the request.

This recommendation is then forwarded to City Council for their action. A notice is published in the newspaper 15 days prior to their hearing. City Council will conduct a public hearing and either adopt an Ordinance to approve the change or deny the request. You will be notified by mail of the date of the City Council public hearing. If an ordinance is adopted, at least one further meeting is required to give Council an opportunity to reconsider the request. If there is no reconsideration, the process is complete. If there is reconsideration, a third reading of the ordinance would be required for approval.

To be completed by Staff:

Property is located in: Edwards Aquifer Recharge Zone Historic District River Corridor
 Concurrent Land Use Amendment is Required: Yes No

Meeting Date: _____ Deadline: _____ Accepted By: _____ Date: _____

AGREEMENT TO THE PLACEMENT OF ZONING NOTIFICATION SIGNS

The City of San Marcos Land Development Code provides that notification signs shall be placed on any property that is the subject of a zoning change, zoning variance, or conditional use permit. The signs shall be placed on the property by the Development Services-Planning staff prior to the 10th day before the scheduled public hearing based on the following criteria:

- Signs shall be placed on each street for property having multiple street frontages
- Signs shall be placed in a visible, unobstructed location near the front property line

Signs shall remain in place until final action is taken on the application, unless the case is formally withdrawn by the applicant prior to a final decision. Staff will remove the signs.

It is the responsibility of the applicant to periodically check the sign locations to verify that signs remain in place and have not been vandalized or removed. It is the responsibility of the applicant to immediately notify the Development Services-Planning Department of any missing or defective signs.

It is unlawful for a person to alter any notification sign, or to remove it while the case is pending. However, any removal or alteration that is beyond the control of the applicant shall not constitute a failure to meet notification requirements.

I have read the above statement and agree to allow the placement of notification signs as required on the project covered by the attached request. The Development Services-Planning staff has my permission to place these signs on my property. I will notify City staff if the sign is damaged or removed.

GUADALUPE RE, LLC

By: _____

Signature of Applicant
Mark Berins, authorized agent

27
April 27, 2015

Date

FOR STAFF USE ONLY:

Sign (s) were placed by staff on _____ by _____.

Sign (s) were removed by staff on _____ by _____.

GUADALUPE RE, LLC
c/o David Lerman
1519 Spruce St.
Philadelphia, PA 19102
Tel: 215-545-4545

April 27, 2015

Planning and Development Services Department
City of San Marcos
630 E. Hopkins
San Marcos, TX 78666

Re: 500 W. Hutchison Property Ownership and Authorized Agents

Dear Ladies and Gentlemen,

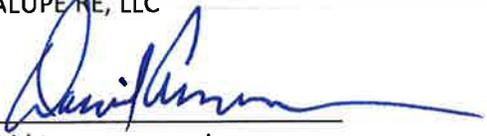
Please accept this letter as certification and confirmation of the following:

1. 500 W. Hutchison in San Marcos, TX is owned by Guadalupe RE, LLC, a Texas limited liability company.
2. David Lerman and Mark Berins are the members and authorized agents of Guadalupe RE, LLC.

Please contact us if we can answer any other questions.

Sincerely,

GUADALUPE RE, LLC

By: 

David Lerman, member

EXHIBIT A

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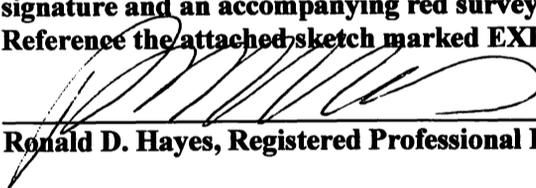
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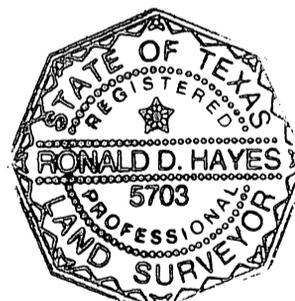
THENCE N 46° 39' 05" W, along the Blanco Street right-of-way, a distance of 333.69 feet to the **POINT OF BEGINNING**, containing all of Block 4, 4.8275 acres.

All bearings and distances are based upon Texas State Plane Coordinate System, Texas South Central Zone, NAD 1983 datum.

This is to certify that this description of land represents an actual survey made on the ground under my supervision in September of 2014. Only those documents with a red surveyor's signature and an accompanying red surveyor's seal shall be deemed reliable and authentic. Reference the attached sketch marked EXHIBIT B, File HTCHSN12.



Ronald D. Hayes, Registered Professional Land Surveyor, No. 5703



TAX RECEIPT

Luanne Caraway Tax Assessor-Collector, Hays County
712 S. Stagecoach Trail
San Marcos, TX 78666
Ph: 512-393-5545 Fax: 512-393-5517



Receipt Number: **SM-2015-823273**

Payor: GUADALUPE RE LLC (00187252)
% DAVID LERMAN
1519 SPRUCE ST
PHILADELPHIA, PA 19102

Owner: GUADALUPE RE LLC (00187252)
% DAVID LERMAN
1519 SPRUCE ST
PHILADELPHIA, PA 19102

Quick Ref ID: R143037
Owner: GUADALUPE RE LLC (00187252) - 100%

Property: 11-4950-0400-00000-3
Legal Description: LINDSEY & HARVEY ADDN, BLOCK 4,
ACRES 4.8275;... TAXABLE %
12/01/2014

Owner Address: % DAVID LERMAN
1519 SPRUCE ST
PHILADELPHIA, PA 19102

Schedule	Charge	Payment Amount
Tax Certificate	10.00	10.00
Total Payment Amount		10.00
	Cash Tendered	100.00
	Total Tendered	100.00
	Change	90.00
	Remaining Balance Due, including other fees, as of 4/22/2015	0.00

Date Paid: 04/22/2015
Effective Date: 04/22/2015
Station/Till: trishal/PATRICIA'S TILL
Cashier:

TAX CERTIFICATE

Luanne Caraway Tax Assessor-Collector, Hays County

712 S. Stagecoach Trail
 San Marcos, TX 78666
 Ph: 512-393-5545 Fax: 512-393-5517

This certificate includes tax years up to 2014

Entities to which this certificate applies:

RSP - Special Road Dist
 CSM - City Of San Marcos

SSM - San Marcos CISD
 GHA - Hays County

Property Information

Property ID : 11-4950-0400-00000-3
 Quick-Ref ID : R143037

Value Information

Land HS : \$0.00
 Land NHS : \$1,051,430.00
 Imp HS : \$0.00
 Imp NHS : \$1,494,860.00
 Ag Mkt : \$0.00
 Ag Use : \$0.00
 Tim Mkt : \$0.00
 Tim Use : \$0.00
 HS Cap Adj : \$0.00
 Assessed : \$2,546,290.00

LINDSEY & HARVEY ADDN,
 BLOCK 4, ACRES 4.8275;...
 TAXABLE % 12/01/2014

Owner Information

Owner ID : 00187252
 GUADALUPE RE LLC
 % DAVID LERMAN
 1519 SPRUCE ST
 PHILADELPHIA, PA 19102
 Ownership: 100.00%

This Document is to certify that after a careful check of the Tax Records of this Office, the following Current or Delinquent Taxes, Penalties, and Interest are due on the Property for the Taxing Entities described above:

Entity	Year	Tax	Discount	P&I	Atty Fee	TOTAL
SSM	2014	2,959.48	0.00	0.00	0.00	0.00
RSP	2014	91.67	0.00	0.00	0.00	0.00
GHA	2014	889.88	0.00	0.00	0.00	0.00
CSM	2014	1,109.62	0.00	0.00	0.00	0.00

Total for current bills if paid by 4/30/2015 : \$0.00
Total due on all bills 4/30/2015 : \$0.00
 2014 taxes paid for entity SSM \$2,959.48
 2014 taxes paid for entity RSP \$91.67
 2014 taxes paid for entity GHA \$889.88
 2014 taxes paid for entity CSM \$1,109.62
2014 Total Taxes Paid : \$5,050.65
Date of Last Payment : 04/10/15

If applicable, the above-described property is receiving special valuation based on its use. Additional rollback taxes that may become due based on the provisions of the special valuation are not indicated in this document.
 This certificate does not clear abuse of granted exemptions as defined in Section 11.43, Paragraph (i) of the Texas Property Tax Code.


 Signature of Authorized Officer of the Tax Office

Date of Issue : 04/22/2015
 Requestor : GUADALUPE RE LLC
 Receipt : SM-2015-823273
 Fee Paid : \$100.00
 Payer : GUADALUPE RE LLC

REPRINTED TAX RECEIPT

Luanne Caraway Tax Assessor-Collector, Hays County
712 S. Stagecoach Trail
San Marcos, TX 78666
Ph: 512-393-5545 Fax: 512-393-5517



Receipt Number: **SM-2015-822733**

Payor: 301 N GUADALUPE, LLC ()
1519 SPRUCE STREET, 1ST FL
PHILADELPHIA, PA 19102

Owner: GUADALUPE RE LLC (O0187252)
% DAVID LERMAN
1519 SPRUCE ST
PHILADELPHIA, PA 19102

Quick Ref ID: R143037

Property: 11-4950-0400-00000-3

Owner: GUADALUPE RE LLC (O0187252) - 100%

Legal Description: LINDSEY & HARVEY ADDN, BLOCK 4,
ACRES 4.8275;... TAXABLE %
12/01/2014

Owner Address: % DAVID LERMAN
1519 SPRUCE ST
PHILADELPHIA, PA 19102

Tax Year/Taxing Unit	Taxable Value	Tax Rate	Levy	Tax Paid	Amount Paid
2014					
San Marcos CISD	209,284	1.414100	2,959.48	2,959.48	2,959.48
Special Road Dist	209,284	0.043800	91.67	91.67	91.67
Hays County	209,284	0.425200	889.88	889.88	889.88
City Of San Marcos	209,284	0.530200	1,109.62	1,109.62	1,109.62

Total Payment Amount 5,050.65
Check Payment (Ref # 1094) Tendered 5,050.65
Total Tendered **5,050.65**

Remaining Balance Due, including other fees,
as of 4/22/2015 **0.00**

Date Paid: 04/10/2015
Effective Date: 04/10/2015
Station/Till: Monica/Monica's Till
Cashier:

Consistency with Vision San Marcos: A Matrix Comparing Lindsey Hill to the Goals and Objectives of Vision San Marcos

[**Please note:** Portions of the passages within the right-hand column of the charts in this document have been taken, verbatim, from *Vision San Marcos*. Other portions are editorialized based on the developer’s analysis and belief as to the intent of those sections. Reference at the beginning of each section of the chart below to “*Vision Statement*” identifies language taken directly from *Vision San Marcos*.]

Economic Development

Vision Statement: We envision San Marcos with economic, educational and cultural opportunities that develop a stronger middle class and grow our local economy. We foresee a vibrant community that strategically leverages the University and all available community assets to support environmentally sustainable industry, technological excellence, local business development and the arts.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
Abundant opportunities created by the ingenuity and intellectual capital of University, business, civic and cultural leaders.	Exciting, dynamic, sophisticated, engaging community serves as a magnet and helps attract tier #1 faculty members, business leaders, and arts and culture leaders to San Marcos.
Workforce and Education excellence.	Exciting, dynamic, sophisticated, engaging community serves as a magnet and helps attract tier #1 faculty members, business leaders, and arts and culture leaders to San Marcos.
Emerging markets and industry relationships that generate quality entrepreneurial and employment opportunities.	Lindsey Hill has the potential to increase the amount of Class A office space that may be attractive to target industries.
An enhanced and diverse local economic environment that is prosperous, efficient and provides improved opportunities to residents.	Lindsey Hill is a development that is consistent with the comprehensive plan and provides new housing opportunities to residents and future residents. Lindsey Hill will add that housing in a professional and family-focused downtown residential development.
Fiscally responsible incentives for economic development.	
Promote and Support the Maximum Potential of the San Marcos Municipal Airport.	
Sports tourism, eco-tourism, retail tourism and the community’s 13,000 year heritage as an economic generator.	Lindsey Hill adds to the high-quality system of civic engagement spaces, parks, natural areas, greenways and trails to draw visitors and encourage new business opportunities. Lindsey Hill’s infill location fits into a transit plan that matches the Preferred Scenario Map to encourage connectivity between centers. Lindsey Hill is part of a strategy to prioritize and complete infrastructure upgrades in Downtown in order to enhance accessibility and the physical appearance. Lindsey Hill will be the gateway linking Downtown, the University, and historic neighborhoods.

Environment and Resource Protection

Vision Statement: We envision San Marcos to be a community of outstanding stewards of our irreplaceable unique natural environment. We value our resource and energy efficiency and our community’s health, well being and prosperity.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
Public and Private sectors working together to protect water quality and facilitating appropriate development in the San Marcos and Blanco Rivers watersheds, and over the Edwards Aquifer using measureable and scientific methods.	Lindsey Hill aims to incorporate cost-effective low impact development practices early on and throughout the development process. Lindsey Hill is a dense development within an existing activity center already served by public utilities, but not within an environmentally sensitive zone.
Natural resources necessary to our community’s health, well-being, and prosperity secured for future development.	Lindsey Hill will be the model for a coordinated tree preservation and planting program.
Pro-active policies that encourage recycling and resource and energy efficiency.	Lindsey Hill would like to be the first project participating in a point system to measure the sustainable elements of proposed development in order to qualify for utility, process, and other incentives. Lindsey Hill will be a model and case study example for development that is within a connected network for non-automobile, non-carbon emitting travel – a community designed to favor the pedestrian by making it faster and easier to walk or bike.
A population prepared for and resilient to man-made and natural disasters.	More efficient support by first responders of community population that is clustered rather than spread out.

Land Use

Vision Statement: We envision San Marcos as a community with balanced and diverse land uses that expand our lifestyle choices while protecting and enriching our historical, cultural, and natural resources.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
Direct growth, compatible with surrounding uses.	Lindsey Hill is situated across the street from a described High Density zone in the Future Land Use Map that is based on the development intensities specified in the preferred scenario.
High-density mixed-use development and infrastructure in the activity nodes and intensity zones, including the downtown area supporting walkability and integrated transit corridors.	Lindsey Hill is a dense, mixed-use development in an area that is underserved by civic spaces, but in an intense activity node with strong employment centers (Downtown and the University) and with substantial existing infrastructure including utilities and transit (CARTS stop is one block away) Lindsey Hill will have adequate facilities to accommodate all modes of transportation.

****NOTE: Document prepared by the applicants and provided at their request.**

	Lindsey Hill creates new public engagement spaces that complement the City’s parks, open spaces, and other key activity nodes.
Set appropriate density and impervious cover limitations in the environmentally sensitive areas to avoid adverse impacts on the water supply.	Lindsey Hill is outside of environmentally sensitive areas. Lindsey Hill aims to implement cost-effective rain water retention and stormwater Best Management Practices.

Neighborhoods and Housing

Vision Statement: We envision San Marcos to have a strong, more comprehensive foundation of safe stable neighborhoods while preserving and protecting the historical, cultural, and natural identities of those neighborhoods.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
Neighborhoods that are protected and enhanced in order to maintain a high quality of life and stable property values.	Lindsey Hill will enhance the surrounding neighborhoods and increase property values by creating a high-quality, walkable development that provides quality of life opportunities, including civic engagement space, dining, and entertainment options to neighboring residents.
Housing opportunities for students of Texas State University in appropriate areas and create and implement a plan to accomplish this vision.	Lindsey Hill is not a student-focused development since the historic district is not, in the developer’s opinion, an appropriate area for a student housing project. Lindsey Hill will reduce congestion and parking issues caused in dense housing areas by providing an on-site garage, a walkable and bikeable focus, and will integrate into the existing CARTS transit option.
Diversified housing options to serve citizens with varying needs and interests.	Lindsey Hill will add a new housing type within a mixed-use development.
Well maintained, stable neighborhoods protected from blight or the encroachment of incompatible land uses.	Lindsey Hill will add a new high quality mixed-use multifamily residential development to an existing area comprised mostly of multifamily residential and commercial uses. Lindsey Hill has the potential to catalyze substantial investment and redevelopment in surrounding, partially blighted neighborhoods. Lindsey Hill will employ smart, quality design and utilize long-lasting materials.

Parks, Public Spaces & Facilities

Vision Statement: We envision San Marcos with safe and attractive parks, public spaces and facilities which provide a range of amenities and experiences. We envision a connected system of parks and natural areas that focus on our unique cultural and environmental heritage.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
Well-maintained public facilities that meet the needs of our community.	Lindsey Hill will include quality, carefully designed civic engagement spaces currently unavailable in this area of the City.

****NOTE: Document prepared by the applicants and provided at their request.**

A differentiated collection of connected and easily navigated parks and public spaces.	Lindsey Hill’s civic engagement spaces will become an important part of the City’s connected public spaces.
A vibrant central arts district and robust arts and cultural educational opportunities for everyone.	Lindsey Hill will be a welcoming location for the future Art in Public Places Program, and has been identified by the developer as an area of the city that could be used for murals/public art displays.
Funding and staffing to ensure quality public safety and community services.	
Effective social services delivered to those who can most benefit from them.	

Transportation

Vision Statement: We envision San Marcos to have a connected network of efficient, safe and convenient multimodal transportation options while protecting the environment.

Vision San Marcos Goals & Objectives	How That Goal is Achieved Through Lindsey Hill
A safe, well-coordinated transportation system implemented in an environmentally sensitive manner.	Lindsey Hill’s goals of walkability and bikeability focus on the transit modes that are the most environmentally sensitive (and healthy) transit options. Lindsey Hill is one block from an existing CARTS bus stop.
A multimodal transportation network to improve accessibility and mobility, minimize congestion and reduce pollution.	Lindsey Hill will be a step ahead by catering to non-vehicular transportation improvements that will be part of the updated Transportation Master Plan. Lindsey Hill is correctly situated to be part of a multimodal transportation system that integrates with existing university and proposed regional systems. Lindsey Hill likely will help the City obtain “Bicycle Friendly Community” designation and may enhance the City’s walkability ratings from Walk Score®. Lindsey Hill will be an important part of the future Sidewalk Master Plan. Lindsey Hill’s bike-friendly, walkable design will be ready for the implementation of a complete streets policy for coordination with other transportation related entities to properly integrate all modes of transportation into the transportation network.

****NOTE:** Document prepared by the applicants and provided at their request.

LINDSEY HILL

San Marcos, Texas

PDD Vision Book



****NOTE: Document prepared by the applicants and provided at their request.**

Preface

The Past Becomes the Future

Lindsey Hill holds a prominent place in the history of San Marcos. It was home to the Coronal Institute (est. 1868), one of San Marcos' original key public institutions. Coronal Institute served as school, public gathering place, cultural and performance venue, and "vision-making center" for the City of San Marcos.

Coronal Institute was known throughout the State of Texas, and it helped put San Marcos on the map as a visionary, forward-thinking, dynamic community. The prominence and prestige of the Coronal Institute attracted others—including San Marcos' leading citizens—to build their homes and places of business nearby.

Since the founding of Coronal Institute, Lindsey Hill has functioned as a definable neighborhood with its own history, traditions, character, and role. It has made singular contributions to the city as the "connector" and "community-building" neighborhood, weaving together and bringing harmony to the neighborhoods that grew up around it. That role as community nexus—and the physical majesty, prominence, and prestige of the site—are elements that the development team wishes to re-express and honor at Lindsey Hill.

****NOTE:** Document prepared by the applicants and provided at their request.

Table of Contents

1. Vision and Mission
2. Guiding Principles
3. Elements and Uses
4. Neighborhood Compatibility
5. Environmental and Resource Efficiency

1. Vision and Mission

To create an iconic, walkable, mixed-use community unique to central Texas in a thoughtfully designed urban setting.

Lindsey Hill offers new experiences and opportunities in living, working, dining, and social engagement, and invites use and enjoyment by residents, guests, neighbors, and community members. Most importantly, Lindsey Hill is a community designed with San Marcos' permanent population in mind—an oasis for those who have chosen to call San Marcos home.

2. Guiding Principles

Lindsey Hill's vision will be accomplished through the following guiding principles:

I. **Preservation – Existing topography and specimen trees are the control points around which the entire Project is organized.**

Nature – Magnificent heritage oak trees are located in two distinct stands at the property. Lindsey Hill will preserve and honor these precious trees by enhancing their surroundings and making them the centerpieces of the Project's public spaces.

Topography – The Project Site's beautiful hillside topography will be substantially preserved and sculpted in order to blend the Project into the existing landscape and to create an invitation for the public to enter and enjoy Lindsey Hill's public spaces.

History – Celebrate the Project Site's long history of leadership, progress, and prominence by welcoming the community back to a reinvigorated place of public interaction.

Architectural Legacy – Respond to the neighboring Historic Districts' rich architectural tradition by creating buildings and public spaces that will leave a lasting legacy for the permanent population of San Marcos.

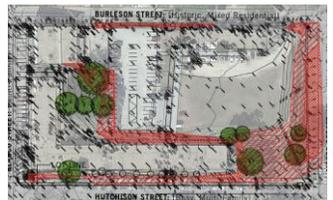
Repurpose – If possible, repurpose and reuse the property's existing gymnasium and auditorium buildings within the redevelopment.

II. **Community – Design Lindsey Hill in a way that brings people together to learn from and engage with one another.**

Civic Engagement – Throughout the Project Site, design accessible spaces that foster civic engagement and social interaction.

Reconnect – Reconnect the Historic Districts to Downtown by creating an active and interactive pedestrian-friendly link.

Public Spaces – Utilize a substantial portion of the Project Site for high-quality public and semi-public open spaces.



****NOTE: Document prepared by the applicants and provided at their request.**

III. Opportunities – Provide a mixture of uses to create new opportunities in living, dining, shopping, work, and play.

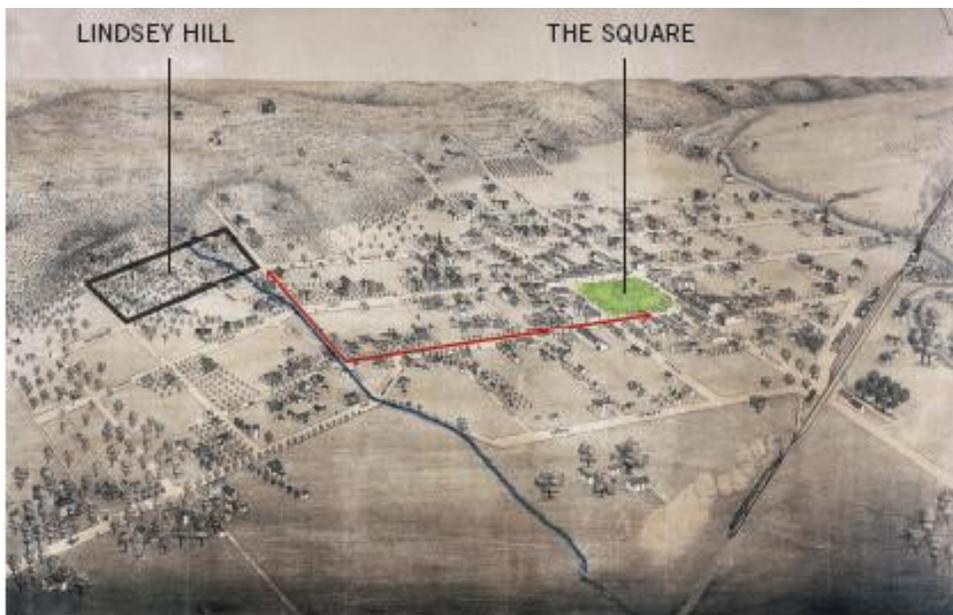
Housing Choice – Provide professional housing that will give a new, Downtown living option to San Marcos’s growing permanent population.

New Businesses – Bring to Lindsey Hill restaurants and other much needed businesses for the use and enjoyment of San Marcos’ residents.

Downtown Residents – Further enliven the Downtown district by adding new activities and increasing the Downtown area’s tourist and permanent populations.

Boutique Hotel – Create a high-quality and interesting Downtown lodging option that currently is unavailable in San Marcos.

Betterment – Provide a new and exciting asset that will catalyze redevelopment and urban investment.



Originally developed in 1868, Lindsey Hill was a pioneer, and the catalyst to high-quality growth and progress in its young neighborhood.

3. Elements and Uses

Lindsey Hill is a mixed-use community that will include active public spaces, restaurant and retail uses, a boutique hotel, multi-family housing, a parking garage, and possible office space.

Consistent with *Vision San Marcos's* Land Use Vision Statement, Lindsey Hill will further the city's desire to create "a community with balanced and diverse land uses that expand our lifestyle choices while protecting and enriching our historical, cultural, and natural resources."¹

The multifamily, hotel, and retail components are core use elements of the Project. The City of San Marcos has encouraged the development team to consider also the inclusion of office space at Lindsey Hill. Because current market demand for that use is unclear, the development team has identified a portion of the multifamily element that could be shifted to office use in the event that during development and construction an office user expresses interest.

Lindsey Hill enjoys a unique urban location that is adjacent to and within easy walking distance of the Square, the Downtown retail and service core, and Texas State University. It is located in the most employment-dense section of San Marcos. It offers residents, tenants, and guests the opportunity to live an environmentally friendly personal and/or business lifestyle. Apartment residents, hotel guests, office occupants, and the public at large, will enjoy on-site dining, retail, and civic and social engagement opportunities. Most importantly, they will be able to walk or bike to and from the supermarket, drugstore, other Downtown merchants, culture, entertainment, and sports venues, and the many other indoor and outdoor activities that make San Marcos consistently rank nationally as a "best places to live and work" city.

3.1 Civic Engagement Spaces

Central to the development team's vision and goals for Lindsey Hill are reinvigoration of Lindsey Hill's historic role as "connector" and community focal point, encouragement of social interaction, and community-building. Achieving these goals requires lively open spaces for public interaction—referred to by the developer as "Civic Engagement Spaces"—featuring areas for planned and spontaneous social interaction, thriving retail, and active dining venues (indoor and outdoor).

¹ San Marcos, Texas, Master Plan *Vision San Marcos: A River Runs Through Us*, 42.

****NOTE: Document prepared by the applicants and provided at their request.**

Lindsey Hill’s Civic Engagement Spaces will encourage active use by residents, guests, neighbors, and community members alike. Activity leads to more activity, which then leads to a greater feeling of comfort and safety for residents, neighbors, and surrounding businesses.² More simply stated, “Something happens because something happens because...”³

Developments of the size and scale of Lindsey Hill often are imagined and executed as self-contained, inwardly-focused “fortress” projects. Lindsey Hill, by contrast, is imagined as the exact opposite: an outwardly-facing, open and welcoming, neighborhood-friendly community.

According to the Parks and Greenspaces map provided within the *Vision San Marcos* exhibits (an excerpt of which is shown as *Figure 3-1*), there currently is no park space located on the western edge of Downtown. Based on the City’s desire for densification of the Downtown area, publicly-accessible recreation space is very much needed within this area.

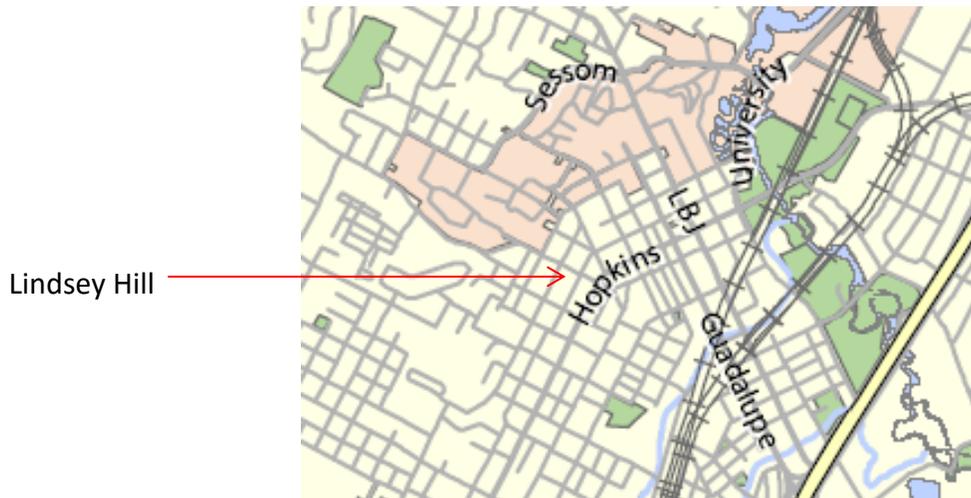


Figure 3-1: Excerpt from Existing Parks and Greenspace Map (source: Vision San Marcos).

Because Lindsey Hill will include green open areas and complementary Civic Engagement Space available to the public, the City of San Marcos will have the benefit of thoughtfully designed public activity space, but without having to expend tax dollars traditionally needed to condemn, purchase, and maintain infill/downtown parkland.

² “The lively city sends friendly and welcoming signals with the promise of social interaction. The presence of other people in itself signals which places are worthwhile. Gehl, Jan, *Cities for People* (Washington, DC: Island Press, 2010) 63.

³ *Ibid.*, p. 65.

3.2 Multifamily Housing

Although permanent population growth is coming to San Marcos (see *Figure 5-4* for growth projections), no new multifamily housing options for that demographic are being added near San Marcos’s Downtown core. *Vision San Marcos* recognizes that “smart growth” best practices encourage development in areas where public infrastructure and capacity (streets, utilities, and public transportation systems) already exist—and preferably in areas proximate to where the greatest employment density exists.

The needs of several distinct audiences who might prefer an “in-town” housing opportunity in San Marcos currently are not being met. Among others:

- National housing data indicate that young professionals—the most coveted demographic to modern industry and business—have a strong preference for walkable, “alive,” environmentally efficient, urban environments. This same data suggest that “employers follow employees”—meaning that communities that appeal to the residential and lifestyle needs of this population also have the best chance of attracting new industry.
- Texas State University reports that more than 40% of its faculty members live outside of San Marcos, with the most frequently cited reason for their out-of-town residence being “lack of attractive housing opportunities close to the university.”
- Older residents who live in the neighboring Historic Districts and who would like to downsize currently have no meaningful options that will allow them to “age in place” and to remain conveniently attached to their familiar points of connection, engagement, and involvement in the community.

Lindsey Hill apartments will “allow for more diverse housing types [in a] mixed-use development,”⁴ addressing the needs of these three constituencies and others seeking a convenient small city living option.

Many residents—as an expression of personal lifestyle decision and commitment—will choose to live at Lindsey Hill so they can be within walking or bicycling distance of their places of employment. Immediate proximity to Downtown and the Square will allow residents to satisfy many of their daily needs on foot or by bicycle—including grocery shopping (HEB is only two blocks away) and the pharmacy (CVS is about six blocks away). These trips will be faster on foot or on bicycle, resulting in fewer automobile trips and a more active and vibrant Downtown.

⁴ San Marcos, Texas, Master Plan *Vision San Marcos: A River Runs Through Us*, 45.

3.3 Boutique Hotel

The development team aspires to create a landmark hotel that is distinctive, memorable, and expressive of the unique qualities of San Marcos, Central Texas, and the Texas Hill Country. Hotel guests will enjoy the pleasure of leaving their cars behind and exploring San Marcos on foot, including Wonder World, the neighboring Historic Districts, Downtown, Texas State University campus, Sewell Park, and the Meadows Center at Spring Lake.

3.4 Office Space

Both the City of San Marcos and the Greater San Marcos Partnership have asked the development team to consider including an office component at Lindsey Hill. The development team believes that welcoming office patrons into Lindsey Hill would be beneficial to the Project, and would create an extraordinary business lifestyle opportunity for companies considering relocation to San Marcos. The presence of office occupants at Lindsey Hill also would further activate and energize the restaurants, retail businesses, and Civic Engagement Spaces at Lindsey Hill, and would drive significant new business to merchants and service providers in the Downtown core.

3.5 Parking Garage

At the center of the site and largely hidden from the surrounding community is a parking garage that, along with other on-site and on-street parking spaces, will handle projected parking needs of Lindsey Hill. The parking garage will be wrapped by apartment “liner units” (and, if applicable, office space lining the Hutchison Street side) surrounding its entirety.

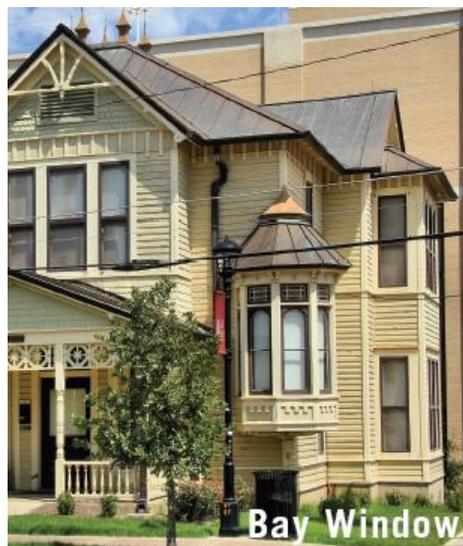
****NOTE:** Document prepared by the applicants and provided at their request.

4. Neighborhood Compatibility

As a project imagined around the needs and desires of San Marcos permanent population, Lindsey Hill’s design, architectural context, size, scale, and uses focus on blending into, enhancing, and connecting the Property’s surrounding neighborhoods.

4.1 Inspiration and Architectural Cues

The development team is inspired by San Marcos’s history, spirit, and architecture, and is committed to responding to those traditions within the Project’s overall design aesthetic. Architectural cues shall be taken from the rich building stock within the City, particularly within neighboring historic districts.



These images represent some of the architectural cues that inform building design at Lindsey Hill.



The Coronal Institute



4.2 Minimizing Impacts on Neighboring Properties

Although most of the properties across Burleson Street are zoned single-family, only those that are at or near the corner of Burleson and Blanco Streets are actually utilized as owner-occupied, single-family homes. The Project design will focus on minimizing, to the extent possible, visual, sunlight, and other impacts on those property owners in the ways next described.

Methods to Minimize Impact on Neighboring Properties:

1. Existing gym building to remain in place.
2. Existing gym building to be re-façaded in order to create a more pleasant view and pedestrian experience.
3. Three-story building height limitation for those buildings within the PDD that are across from the three owner-occupied Burleson Street homes (the first three properties just east of the corner of Burleson and Blanco).
4. Building height steps up to four stories on Burleson toward Moore.
5. Comfortably-sized sidewalks with generous street tree zones.
6. Additional landscaping will be added throughout the street tree and landscape zone on the Burleson frontage.
7. Street-level apartment entrances along Burleson Street.
8. Soft-edged Burleson streetscape that incorporates porches and patios in order to encourage neighbors to engage and interact with one another.

4.3 Streetscapes and Frontages

Lindsey Hill's pedestrian orientation requires thoughtful planning and design in order to create an engaging and welcoming environment within the zones most critical to public view and pedestrian interaction: streetscapes and street frontages.

Retail Frontage: Retail spaces within Lindsey Hill shall be designed in a way that brings life and activity to the Project Site, especially to Poet's Corner. Numerous studies prove that the availability of food and beverages increases use and enjoyment of public spaces. The retail area also functions as an important Civic Engagement Space allowing residents, guests, and the public opportunities to interact within the frontage zone.

Multifamily Frontage: Important (and typically more spontaneous and casual) social interactions occur frequently in residential frontages, especially

****NOTE: Document prepared by the applicants and provided at their request.**

across private and public zones. These interactions have dual value: they promote community and they enhance public safety by giving neighborhood residents a chance to know and feel responsible for one another.

To encourage that type of community interaction, the “street curb to building front door” design at Lindsey Hill aims at creating a positive relationship between private and public realms. They shall be designed in a way that encourages interaction, but also clearly delineates the public versus private frontage, and protects the publicly-viewable private zones.

At ground-floor level, buildings will be designed with a soft edge and in a way that is pedestrian-scaled and comfortable to the pedestrian experience. Many ground floor units will include street-level entrances (or shared street-level entrances) and some ground floor areas of the multifamily buildings may feature patios and/or forecourts facing the street edges. These spaces function like “front-porches” on single-family homes, encouraging casual social interaction “between sidewalk and porch” and providing eyes on the street to increase street safety.

Visible upper floor balconies



Figure 4-1: Representative example of frontage zone features.

6' sidewalks

Visible patios with seating

Protected street-level entries

Landscaped edges

****NOTE: Document prepared by the applicants and provided at their request.**

Moore Street Streetscape: Moore Street presents particular pedestrian challenges because of the busy, state-maintained roadway that is adjacent to the pedestrian corridor. To account for that difficulty, the hillside at this frontage will be reduced in order to allow enough room for safer pedestrian passage. Additionally, depending on topography and in an effort to increase pedestrian safety and comfort, sidewalks may be at street grade or above street grade. Similarly, elevations of the Tree Well and the Upper and Lower Landscape Edges may be adjusted based on topography and other needs.

Balconies and Porches: Balconies are useful in adding interest and definition to large walls, and are desirable for providing outdoor enjoyment and to add eyes on and interaction with the street and other public spaces

4.4 Density and Height

The 164-unit maximum allowable multifamily residential density and 1.5:1 maximum allowable site-wide Floor to Area Ratio (“FAR”) for habitable, conditioned spaces of Lindsey Hill is, in fact, modest considering the Project’s urban context and almost 5-acre size. Instead of trying to absolutely maximize density and yield with the greatest amount of multifamily units on the site, Project design is driven by the developer’s primary goal of showcasing the Project Site’s existing and irreplaceable natural features: its mature specimen trees and interesting hillside topography, with buildings molded around and into those features.

Sufficient density, however, is critical to the development team’s ability to achieve another primary project goal: the creation of active, lively, community-building uses and civic engagement spaces.

Buildings at Lindsey Hill will be constructed on a scale that allows for much-needed central city densification, but in a manner that is: (a) consistent with the likely scale of future redevelopment based on existing zoning; and (b) respectful of existing uses of neighboring properties.

4.5 Use

Lindsey Hill is focused on fostering a mixed-use environment that is appropriate for the Project Site’s Downtown/transitional location. Having uses consistent with other urban/mixed-use environments will allow Lindsey Hill to blend into those uses already found and/or permitted in the areas surrounding and within immediate proximity to the Project Site, and into a Downtown that one day may appear very similar to Lindsey Hill.

5. Environmental and Resource Efficiency

Lindsey Hill’s attributes that will produce an environmentally-sensitive project that meets and exceeds the goals of the base zoning regulations include:

- location outside the Edwards Aquifer environmentally-sensitive recharge zone in a previously developed area;
- location in an area that already is serviced by roadway, utility, and public transportation services infrastructure;
- site design that prioritizes preservation of legacy specimen trees and showcases the shared use of unique topographic features;
- possible repurposing of existing buildings that recall and honor the modern history of the community; and
- walkable and bikeable community that reduces carbon footprint and traffic congestion.

5.1 Specimen Trees

As depicted in *Figures 5-1* and 5-2, the Project Site includes ten heritage oak trees with canopies of at least 30 feet in diameter. The most important element driving the Project’s design is the development team’s priority to preserve, protect, integrate, and allow for the enjoyment of these irreplaceable natural assets while making them the centerpieces of Lindsey Hill’s public spaces. That effort will allow “Significant stands of native trees [to be] preserved and protected from destruction or alteration”⁵ in order to be enjoyed by guests, residents, tenants, and the public.



Figure 5-1: Existing large-canopy trees are depicted with green circles showing each tree’s relative canopy size.

⁵ San Marcos, Texas, Municipal Code Section 4.2.6.3(d)(3).

****NOTE:** Document prepared by the applicants and provided at their request.



Figure 5-2: Aerial image showing actual magnitude of specimen trees and location of existing auditorium and gymnasium buildings.

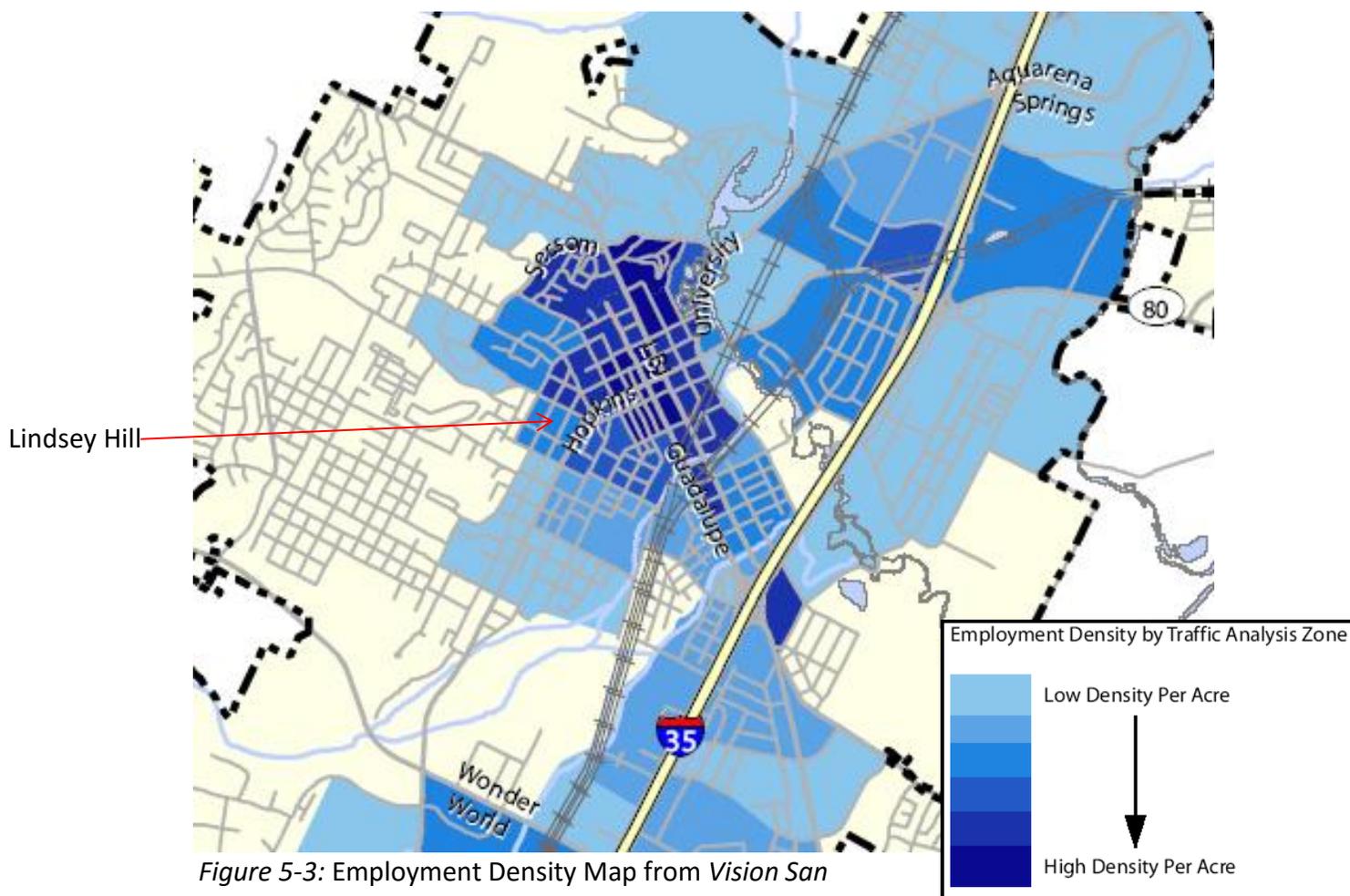
5.2 Building Reuse

With specimen trees as the focal point of The Grove, the existing auditorium and gymnasium buildings wonderfully frame that open space in a way that will create a peaceful, internal courtyard setting. The Project's structural engineer believes that of the buildings on the Project Site, the existing auditorium and gymnasium buildings (depicted in *Figure 5-2*) can be reused and integrated into the Project's hotel component.

If these buildings can be repurposed, the Project and the citizens of San Marcos will benefit by keeping materials out of the landfill and saving a bit of San Marcos history through adaptive reuse of the former San Marcos high school gym and auditorium. The development team also hopes to save and reuse within Lindsey Hill some seating and flooring materials from within those buildings.

5.3 Walkability/Bikeability

Many residents, hotel guests, and commercial tenants will choose to locate at Lindsey Hill as a personal and/or business lifestyle choice and commitment. Lindsey Hill offers substantial opportunity for eliminating and/or reducing car dependency. Many daily needs (getting to work, shopping, dining, recreation, and entertainment) can be accomplished on site or by walking or biking to the Square, Downtown, Texas State University, and other nearby destinations. Lindsey Hill is located adjacent to San Marcos's densest employment clusters based on the Employment Density map found in the exhibits to Vision San Marcos (an excerpt of that map is included as *Figure 5-3*).



5.4 Existing Infrastructure/Developed Neighborhood

As depicted in *Figure 5-4*, significant growth is coming to San Marcos. Lindsey Hill is located within a previously developed neighborhood in an area already serviced by existing high-capacity roadway network, utility infrastructure and capacity, and public transportation network.

These existing resources will allow the City to welcome a portion of that growth in a walkable, bikeable, mixed-use community that:

- has minimal impact on community services and costs, and on the area’s natural environment; and
- helps preserve open spaces surrounding the City of San Marcos.

County	Population 2000	Projected Population 2030	% Change
Bandera	17,645	48,577	175%
Bexar	1,392,931	2,059,112	48%
Blanco	8,418	13,487	60%
Burnet	34,147	60,382	77%
Comal	78,021	190,873	145%
Edwards	2,162	2,364	9%
Gillespie	20,814	28,845	39%
Hays	97,589	302,795	210%
Kendall	23,743	65,752	177%
Kerr	43,653	57,565	32%
Kimble	4,468	4,702	5%
Llano	17,044	17,360	2%
Mason	3,738	3,876	4%
Medina	38,304	62,416	63%
Real	3,047	3,042	0%
Travis	812,280	1,385,236	71%
Uvalde	25,926	33,802	30%
Region	2,623,930	4,340,186	65%

Source: Hill Country Alliance

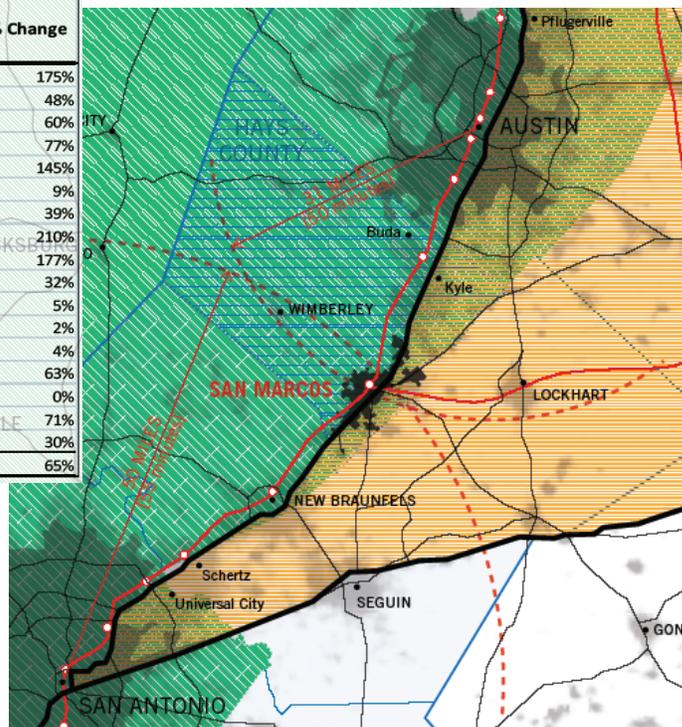


Figure 5-4: San Marcos and Central Texas growth projections and map.

Letter to Planning and Zoning, March 24, 2016

To whom it may concern,

I attended the March 21 meeting concerning the Lindsey Hill Development hoping to get answers to some questions of concern. I left the meeting with the following observations: First, the developers have chosen to save the largest trees and include an enhanced green space for the use of residents and the general San Marcos community, and provide parking.

Secondly, there are multiple, legal safeguards to ensure apartments are for adults, a pressing current and future need for San Marcos residents.

Thirdly, the design, though not yet detailed, is sensitive to the importance of providing an appropriate gateway to a lovely and value historic neighborhood, of which we are also home owners.

We frequently walk through the neighborhood to downtown businesses, coffee shops, restaurants, the square, and even to the activity center and library. As we leave and return, the current vacant school property is the least attractive we pass. We are favorably impressed by this distinct and unique replacement as an answer for active, healthy adults no longer seeking home ownership and as a place where young professionals not ready for ownership can integrate into the community, the growing trend.

Finally, I recognize some of our neighbors (not all) are opposed to this change due to fears for their property or added competition to their business interests. As an adult who has managed many personal changes, I respect their concerns and hope they can respect those of us who see this as a positive opportunity to enrich downtown San Marcos experiences while meeting the real needs of a large and growing segment of San Marcos citizens and visitors.

I recognize and notice the “squeaky wheel” demands the most attention, but it is not the only wheel turning. The inevitable wheel of change and opportunity will either roll toward or away from our community. Leadership is responsible for recognizing all points of view and still determining the best short and long term interests of San Marcos residents and the interests of current children who will either lead or leave the community in the future, following that wheel.

Thank you for your service to our community and listening to various points of view as part of your decision making process.

Barbara Barks

Francis Serna
Administrative Coordinator
Planning and Development Service
City of San Marcos, TX

Ms. Serna,

This letter is intended to share my thoughts related to the Lindsey Hill project being considered for rezoning. It is my hope that you can distribute the document to those that are part of the decision making process related to this project.

The comments in the letter are provided from the perspective of a property owner on Hopkins Street within the Historic District. They also reflect the knowledge of the situation gained from attendance at a meeting on 3/21 at the Lamar Annex in which the developers presented their vision and fielded attendee questions.

I should be counted among those that support the rezoning request based on what the developers have stated is their intent. This does not mean I have no concerns, but it does mean I believe it is possible to address these concerns in the future.

Positive aspects of the project in my view are:

- preservation of trees and public access to allow the space to be more beneficial to the community
- construction of residential rental housing that will be attractive to young working persons and more mature adults wishing to free themselves from home maintenance or large investment in housing (there are very limited options for those wishing to rent and to be free from student dominated housing in San Marcos)
- provision of parking that will service all residents and clients of retail properties without having the garage visible to passersby
- care being given to visual aspects of the structures to assure it blends as much as possible into the surrounding environs
- encouraging the use of non vehicular access to the surrounding destinations, such as downtown and the university

Aspects requiring further action or consideration:

- the presence and size of the hotel component (if it must be included for cost justification then consider fewer rooms or convert some rooms to long term stay or even addition residences for rent)
- sizing the parking garage to assure available parking in the garage
- providing for citizen input following the completion of more accurate renderings of the proposed structures by the developer
- documenting and publicizing the measures that are required by the developers to keep the resident areas student free

Issues paraphrased below arose in the 3/21 presentation from concerned attendees and deserve to be either confirmed or rejected as unsubstantiated:

- the project will reduce property values in the historic district
- traffic will be significantly increased in the area surrounding the project
- the hotel component will introduce addition risk from crime to the community
- you can't successfully keep students out of the residential component of the project

I suspect you will get a significant number of comments for and against this issue. They all deserve to be heard. You face the possibility of making a decision that will not be viewed by the majority of those providing input as the right decision. My desire is that you will rely on information that is rational, evidence based, considered and informed.

Joel Barks
1120 W Hopkins
San Marcos, TX 78666
joelbarks@gmail.com

March 21, 2016

To whom it may concern,

I am definitely going to be in the minority when I say I am in full support of the Lindsey Hill project.

San Marcos is growing, and how and where this growth happens will have a dramatic impact on the quality of life of everyone who lives here now, and in the future. I consider myself a new urbanist and believe that mix-use density, focusing on bicycle/pedestrian-friendly infrastructure, is the key to keeping San Marcos a nice place, not only for its permanent residents, but also for its temporary residents and visitors.

Traffic and parking seem to be the big issues when discussing the Lindsey Hill project. In an ideal world, those living at and visiting Lindsey Hill would be more apt to not drive, and instead choose to walk or ride a bicycle while exploring our downtown and neighborhoods. Lindsey Hill is just half a mile from the Square, and just as close to Texas State- there is no reason why the majority of trips taken can't be done without the use of an automobile. The development of Lindsey Hill not only will improve bicycle/pedestrian infrastructure in our neighborhood, it will hopefully encourage long-term residents to choose to drive less when making a trip towards downtown for shopping, dining, or entertainment.

The previous generations in San Marcos grew up utterly dependent on the automobile. It's a lifestyle that began after World War Two and is the reason behind urban sprawl, rush hour traffic, and neighborhoods where people don't know each other. Folks that grew up in a car-centric environment find it hard to fathom that people would choose to **not** drive somewhere and instead walk, ride a bicycle, or take transit. Not only are people willing to do this, they are doing this in other communities that have been built to be diverse in use and population. Visit any larger city in the US that takes pride in its mixed-use neighborhoods and you will find areas very similar to what the Lindsey Hill project is proposing.

My generation (I'm 31), the generation after me, and the generation after that are not going to be nearly as dependent upon the personal automobile as the generations before us. Already there are ride-sharing services (such as Uber and Lyft) that make owning a car close to unnecessary. Fully automated cars could be on the streets as early as 2019, and one step closer to making private ownership of cars a thing of the past. These two examples show how a future San Marcos can avoid the parking & traffic nightmares other cities are dealing with because they didn't encourage their residents and guests to drive less.

I hope that the Lindsey Hill project **will** start a domino effect that not only ends up making our historic neighborhoods more walkable, but also more mixed-use with a variety of housing, employment, entertainment, dining, and shopping/services that encourage folks already living here (and those who will live here in the future) and visiting to drive less. There are hundreds (thousands?) of examples of mixed-used historic neighborhoods all over the world. I'm positive most of the people opposed/hesitant of the Lindsey Hill project have not only visited these historic neighborhoods, but have enjoyed themselves too. Historic neighborhoods can and do flourish when mixed-use development is introduced. Sometimes it happened hundreds of years ago (and those neighborhoods are doing great), other times it happened last year (and again, those neighborhoods are doing great).

The city of San Marcos has hosted countless meetings, presentations, and information sessions regarding the future of our town. I strongly believe the vast majority of those in attendance are always in favor of mixed-use projections (centers that have green space, retail and housing, are walkable and bikeable, etc...) and opposed to examples of the urban sprawl we already have in San Marcos (vast seas of parking lots, places that are only reachable by car, etc...). The Lindsey Hill project, if proposed anywhere else in San Marcos, would be applauded by those now opposed/hesitant/suspicious.

Perhaps this is only concerning because it is happening **now**. Had more mixed-use been introduced to San Marcos decades ago, it would be the **norm** as folks on Belvin, Hopkins, or San Antonio Street walk to get a cup of coffee, attend a dentist appointment, catch up with a neighbor, or drop their kid off at the Montessori School (after a Shipley's Donut, of course). As someone who is always thinking (daydreaming?) towards the future, I hope that the Lindsey Hill project touches off a transformation in our neighborhood in which there is always a constant flow of friends, neighbors, and visitors passing by on foot or on bicycle.

Thank you,

Matt Akins
The Yellow House
704 W Hopkins
San Marcos, TX
512-431-2278
bikemattak@gmail.com

To The members of Planning and Zoning,

This is about the Lindsey Hill Project. Don't get me wrong, I would rather have a park or green space but the city wasn't going for that. I am VERY PLEASED the University didn't get their hands on it or someone who wanted to build student housing and flip it over to the University. If you read the wording of the scope, you will see it's going to be very difficult to get a student in there with all they are requiring. Renting it instead of selling condos also keeps them in control of who is occupying the building. I am happy about the amount of green space that will be kept and open to all instead of walled and fenced in as it is now. Now I would like address the hotel issue. People seem so fearful of something new, especially the Boutique Hotel. There are a lot of definitions of Boutique hotel and most think of it as just small. I have stayed in hotels 3 to 5 times a month for 35 years with my job and was lucky enough to work for a company that sought out unique places for us to stay. We never wanted to be near an airport or freeway and currently most all the hotels in San Marcos are along the Interstate which is the last place I would want to stay or suggest to friends and family. I have stayed in boutique/unique hotels in Savannah, Charleston, Oregon, Washington and all over Europe that are tucked away in neighborhoods close to town. They ranged from 60 rooms to much larger but they were unique in the experience as well as the location. We need something like this to bring people close to town. We have bars, restaurants, grocery, coffee shops and speciality shopping within blocks of this location. I also think many that stay in the hotel will park their car and it won't be used again until they check out. I live one house down on Blanco from this property and I know there are going to be times when it will be booked to capacity but certainly not on a daily basis! I would think homecoming, graduation, alumni events will all be a time it's heavily booked. I would also think there are plenty of ways to shuttle people around thus keeping the traffic to a minimum. **If we took away the hotel and put in another 80 to 90 apartments that seems like MORE cars and traffic on a DAILY basis.** I have always loved to walk the streets of historical areas and admire the homes and landscaping. I love being a neighborhood where people feel safe and welcome to do that. I think this town has been burned in the past with projects that only benefited the student. We have a project that will bring some urban development, unique accommodations, restaurants, a few small business, as well as wonderful green space. I have lived next door to this property for over 25

years and it's been used for sixth graders, parks and rec, (lots of cars and people up and down the street) alternative school and buses clogging the street. The grounds and magnificent trees have been ignored and are in sad shape. The building is peeling paint and looks like it is falling down. No one cared about traffic or "unsavory" people then because they weren't next door. I am optimistic about this project. Our other choice was a big square block, multi level student complex that would indeed have been as UGLY as Sanctuary Lofts. San Marcos needs urban dwelling and that means in town. I don't for one second think it will decrease the value of my home and again, I live one door away. I keep hearing infrastructure is an issue although it doesn't seem to be one when we are tearing down buildings all over town and putting up more student housing. I ask you not to be so fearful of something you may not be familiar with. Think of yourself as you travel. Would you prefer big box hotels on the interstate or being in a quaint historical neighborhood where you can walk to things and even enjoy the surroundings? Urban living and boutique accommodations can do very well in this area. I would like to thank David and Mark for trying to show us their vision of what they are wanting to accomplish and for asking for the P&Z vote to be postponed so they can further meet with us. I appreciate your wanting to do a landmark project and I hope you get to do it here.

Sandi Neese
416 Blanco Street

April 6, 2016

Dear Members of the Planning and Zoning Commission:

First, I would like to thank you for your service to the city and citizens of San Marcos. With our rapid growth, your responsibilities and the concomitant pressures that accompany that are greater than ever, and yet you probably feel at times it's a thankless job. Please know that there are many of us who are grateful for the work you perform on behalf of the citizens of San Marcos.

I am writing in regards to the Lindsey Hill Project. I have lived at 400 Blanco Street, at the corner of Blanco and Burleson, for over 35 years, and am one of three property owners who actually live across the street from the property being considered for re-zoning. I love my neighborhood and have wonderful memories of living across from Lamar Middle School. I had hoped that the property would become a public park for this part of town or a site for garden homes, where I might move in the future to stay in my neighborhood. When I learned the school district had sold the property to a Philadelphia developer, I was struck with dread and fear. Then David Lerman and Mark Berins contacted me in the spring of 2015, asking for a meeting to allow them to describe their vision and plan for the property. Because of family matters, I was not able to meet with them until November. They came to my house and met with me and several other neighbors. It was a conversation, with their sharing their plans for the Lindsey Hill Project, and our asking questions and voicing our very serious concerns about the project.

Over the last few months, I have attended the public meetings in the Lamar auditorium and have learned more about the project, as well as the work the developers have done in terms of accessing studies of the demographic population they seek to attract, traffic flow in and around the project, and how they might create a project that is compatible with the neighboring architectural styles and the neighborhood folks themselves. I have been impressed with their willingness to organize meetings over the last 14 months, listen to citizens who are adamantly against the project, and respond with forthrightness and a grace not always shown by our citizens. It is my understanding that they have studied our Master Plan and, in some respects, responding to it through their design.

Am I somewhat anxious about the Lindsey Hill Project? Of course, I am. Sadly, I have no crystal ball. But then, neither do any of us. While I know a lot about the project, the reality of it is not yet with us. Uncertainty is always uncomfortable but I am more uncomfortable with what might turn out to be my "across-the-street" neighbor if this project is denied. I believe that the Lindsey Hill Project might indeed be an asset for our neighborhood, attracting mature adults who want to live in town, able to easily avail themselves of downtown, and contribute to our neighborhood. I was also pleasantly surprised to learn at one of the meetings that the green space surrounding the 2 stands of oak trees makes up 40% of the 4+ acre tract of land.

Your task is a difficult one, I know. You have the information before you to weigh and decide the fate of the Lindsey Hill Project. What I want you to know is that I believe it just might be a good thing for the city of San Marcos. This city is rapidly changing. My hope is that we can all be smart, open-minded, and wise about the decisions we make. Thank you again for your diligence and service.

Fondly,
Dianne Rush Pape

Dianne Rush Pape

To: Commissioners of Planning and Zoning, San Marcos, Texas
From: Amy Meeks
RE: Lindsey Hill Development
Date: April 6, 2016

This letter is in regards to the proposed development at the Lamar school site, currently referred to as Lindsey Hill.

As you are aware, the developers of Lindsey Hill pulled their request for a zoning change from your last meeting's agenda. Many of us living in the surrounding neighborhoods interpreted such action as a positive sign that the developers intended to dialogue with us and incorporate some of our concerns into their final plans for Lindsey Hill, yet, this has not happened.

The plan they initially submitted to the City is the same plan they are currently using in their request for a zoning change. It seems that our opinions and comments, intended by many of us to be helpful, have not been heard and have had zero impact on their plans.

One of my foremost concerns remains the density of the project. I am in favor of the Lamar site being repurposed, and I am in favor of many of the proposed uses for the site, but I am not in favor of all of the proposed uses happening on that plot of land. I believe it would overwhelm the surrounding neighborhoods in a variety of ways, all negative.

Because of this very real possibility, and because we have not yet had any reformulation of the plan, I ask that you deny the zoning request change for Lindsey Hill.

I do appreciate your service to our community and I do thank you for your gift of time.

Respectfully,

Amy M. Meeks

4/6/2016

Planning and Zoning Commission

FSERNA@SANMARCOSTX.GOV

Dear Commission members:

As a concerned citizen, I am voicing my protest and absolute vehemence against the Lindsey Hill Project that is on the table.

There are two points that stand out to me the most, those being that (A) developers Guadalupe LLC states in its Consistency with San Marcos Vision Plan that Lindsey Hill will “catalyze substantial investment and redevelopment of surrounding partially blighted neighborhoods”...As a resident of West Hutchison Street, I take offense at such an ignorant remark made by developers who obviously don’t know what constitutes a “blighted neighborhood”. And we are to trust that they know better than we do about what it is that our town needs? And give them the go ahead to permanently alter one of the oldest districts in town, an area that people come from other states as well as other countries to enjoy and to explore?

And thus enters Point B, The owners of Guadalupe LLC, and of Argeo LLC, David Lerman and Mark Berrins, are both in the business of retail shopping complexes and large scale parking lots i.e LAZ Parking. You only have to visit Mr. Lerman’s website for www.Argeo.com and for LAZ to discover that strip malls and parking lots are the name of their game, constituting the majority of their projects.

I have to ask... what do they really know about greenery, nature preservation, or incorporating buildings into the natural topography ? Just something that might deserve more careful consideration....

Sincerely, Sonya McGill , 329 West Hutchison

To Whom It May Concern:

This letter is in reference to the proposed development, referred to as Lindsey Hill, on the Lamar School property at the corner of Hutchison Street and Ranch Road 12.

My name is Amy Moore Meeks and I currently live in the Belvin Street Historic District. I first moved to the district as a six-year-old child with my parents and lived with them until I left for college. I returned to the district 20 years ago with my two children and remain in the house we purchased at that time.

I like change and usually embrace newness with enthusiasm, especially if such change will help us progress toward a greater good and better life. I believe the proposed Lindsey Hill development can be such an event but only if we walk through the details slowly and conscientiously, having an inclusive conversation with all those involved. This letter is an attempt to extend the conversation to all people, developers as well as citizens of San Marcos, so all ideas and concerns can become part of the dialogue.

The developers of the property, David Lerman and Mark Berins, held one neighborhood meeting two weeks ago (on Monday, February 29, 2016) where they spoke of their intentions for this 5-acre tract of land. It is slated to become a Multi-use property with retail, restaurants, convention facilities, boutique hotel rooms, and one-bedroom apartments for lease. There will also be a multi-level parking garage in the middle of the property as well as an open green space for use by the public.

Change will inevitably come to this location and repurposing the Lamar property seems to be a logical step. I am less concerned about change happening and more concerned about the type of change that may occur.

I wonder if the property can effectively accommodate all of the proposed new uses and enact each of them well. I wonder if the parking that will come with new retail and new restaurants and a visiting public to the green space will become a problem for the surrounding neighbors and neighborhoods. I wonder if the convention center, when in use, will create it's own set of problems. I wonder if this new type of development on one side of Hutchison Street will eventually extend across the street and encompass the remaining houses currently located there. I wonder if one-bedroom apartments averaging 900 square feet will look appealing to university students. I wonder if the traffic on RR12 will disturb/effect/disrupt the green space intended to run parallel to RR12. I wonder if restaurant and retail hours of operation will affect the surrounding neighbors and neighborhoods. I wonder if the finished exterior of the proposed buildings will compliment or distract from the adjoining historic homes and districts. **I wonder if we can create a plan which allows the Lindsey Hill development to become a unique asset within San Marcos and precludes the development from becoming a 5-acre mistake, even though well-intentioned.**

I would like for this development to work and for Lindsey Hill to become a stellar part of the ever-changing, always progressive San Marcos. I applaud the developers for beginning this conversation and believe with continued dialogue we can ascertain many of the answers to the aforementioned questions. But we need time to discuss, consider, and discover the right answers for San Marcos.

Therefore, I will ask that the Planning and Zoning Commission on Tuesday night, March 22, 2016, at their regularly scheduled meeting, table the request for a zoning change asked for by the developers. My hope is that we can continue our conversation with the developers and quickly arrive at viable solutions for Lindsey Hill so this development can proceed.

Respectfully submitted,

Amy M. Meeks

Amy M. Meeks
amymmeeks@yahoo.com

March 14, 2016

Melland, Brandon

From: Serna, Francis
Sent: Wednesday, March 16, 2016 4:13 PM
To: Melland, Brandon
Subject: FW: Proposed Development on the Lamar School Property

From: britaascot@gmail.com [mailto:britaascot@gmail.com] **On Behalf Of** Brita Northcutt
Sent: Wednesday, March 16, 2016 4:13 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>; Brita Northcutt <brita@ascottravel.com>; Cathy Dillon <cri@grandecom.net>; rccbobcats@aol.com
Subject: Proposed Development on the Lamar School Property

To Whom It May Concern,

On behalf of the Heritage Association of San Marcos I wish to express our concern about the possible development on the land into an apartment complex-Hotel Rooms- Restaurant-etc on Lindsey Hill. This property is next to the Historical District an area that the people of San Marcos wish to maintain as a Historical District not a commercial property and certainly not Another Apartment Complex.

The Planning and Zoning Committee is to have a meeting on Tuesday March 22, 2016 to discuss this request and we feel that the discussion should be postponed until several people read the entire proposal (in detail). We as citizens of San Marcos would like to keep our Historical District as it is and not allow developers to come in and destroy what has taken years to build and keep.

It is time that we do not always look at the bottom dollar but at the History of our city.

Your consideration and cooperation of is greatly appreciated. Please take the time to listen to the citizens of San Marcos and not developers from out of state.

Respectfully, On behalf of the Board Members.

Brita Northcutt, President
Heritage Association of San Marcos
400 East Hopkins Street
P. O. Box 1806
San Marcos, Texas 78666

Brita Northcutt

Ascot Travel

512-353-3636 ext. 108 (San Marcos)

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<http://www.ascottravel.com>

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Melland, Brandon

From: Serna, Francis
Sent: Wednesday, March 16, 2016 4:40 PM
To: Melland, Brandon
Subject: FW: proposed Lindsey Hill development at P&Z on Mar 22

From: Camille Phillips [mailto:camille.phillips2@gmail.com]
Sent: Wednesday, March 16, 2016 4:39 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: proposed Lindsey Hill development at P&Z on Mar 22

Dear Ms. Mattingly, planning staff, and members of the Planning and Zoning Commission:

I hope you are well.

The Lindsey Hill development is scheduled to be heard at P&Z on March 22.

Members of the community are interested in what is being proposed for the old Lamar School site and want to hear the presentation about it on Tuesday.

However, this is a large site and hearing about it a few minutes at the meeting is not enough time to grasp the scope of the project.

After the presentation and discussion, I respectfully request that no vote be taken that night and that the item be tabled.

Thank you for your consideration.

Best,
Camille Phillips

From the desk of:
Carl H, Deal III
P.O. Box 753
San Marcos, Texas 78667
512-749-8000
JetSetJoe@GMAIL.com

Tuesday, March 15, 2016

P&Z
San Marcos, Texas

Dear Good Citizens:

As you know, the Lamar Middle School property on Hutchison Street is scheduled for a zoning change to "mixed use", setting in motion the development of a five-story 120 room hotel and 160 unit single bedroom apartment complex. When the struggle over unbridled growth and development first reared its ugly head here - we used to say in a slow country drawl, "Well, if you wanna stay small, you gotta think small". Most people didn't understand what that really meant. We are way past that now and the pressures on the fabric and institutions of our hometown root their demise every day by continued encroachment into the peace and tranquility of our neighborhoods.

Imagine a new hotel and yet another apartment development on the corner of the University and at the very gateway to the Historic District - the obstruction of view, the overflow of traffic and parking, the elevated murmur of growth whistling through the trees as you lounge in your back yard on a sunny afternoon. The folks on Lindsey Street have already fashioned homemade signs in their yards trying to protect the limited space in front of their homes from the daily invasion of students parking close to, yet off of campus.

The worst promise is the promise that is broken. This proposition comes on the heels of the still unhappy ending of the Sessoms Canyon high-rise and the rapidly depreciating, twice flooded apartment complex on the Indian Campgrounds. People should be allowed to make their million bucks - but not without a plan in both design and integration that is in keeping with the history and heritage of this little cow town of cotton fields, Hill Country Springs and Mills. And certainly - another blank check is unacceptable.

A promise made is a debt unpaid and men with good intentions make promises. Only men with good character keep them. Something in this town deserves to earn its seniority and equity. If the preservation of our history and our heritage is among our priorities, development for development's sake on the front doorstep of the Historic District is indeed another nail in the coffin of what we hold dear.

Respectfully Submitted,

Carl H. Deal III

April 5, 2016

Re: Proposed Lindsey Hill Development

To the Members of the San Marcos City Council:

As a resident of the Belvin Street Historical district, I am writing to express what I believe to be an abhorrent and selfish proposal presented by the developers of the proposed Lindsey Hill Development.

To put such a mammoth project made up of approximately 140 apartment units, 120 hotel rooms, a 300+ car parking garage and at least one retail and one restaurant store front is a total misuse and abuse of this beautiful site bordered by the Historic District.

The hotel has been described by the developers as a "Boutique" hotel. All definitions in Webster's Dictionary use the word "small" in all definitions of the word "Boutique". A hotel of this size is surpassed by only one or two other hotels in the city, thus it is not "Boutique" by any definition.

The developers have said that their "Boutique" hotel would be hosting meetings, weddings, etc. The volume of traffic, after said events, exiting from their parking garage onto Burluson St. and subsequently feeding onto Moore St. would create a nightmare. The intersection of Moore and Hutchison Streets, as they stand now often gridlock, especially when backed up from the traffic light at Hopkins St. "Logically", an alternate route for the "guests" would be to shortcut through our beautiful historical neighborhoods. As it is, we have speeders cutting through our streets. To encourage more is not acceptable.

The developers have stated that there were going to be certain restrictions placed on who could rent the apartments. I am not certain, that according to the Fair Housing Act, that such restrictions could be enforced, if in fact such restrictions are at all legal.

I have discussed with neighbors that a better plan for this site would be to build townhomes. They would be individually owned and, if not owner occupied, the landlord would want, presumably, to closely oversee their property thus retaining or increasing its value. Additionally a portion of the land could be set aside as public space to be enjoyed by the citizens of San Marcos.

It is my sincerest hope that the City Council and the Planning and Zoning Board will NOT vote this project through. This development would truly be detrimental to the future of our beautiful city.

Respectively yours,

Cathy Bertron



Cc: Planning and Zoning Commission & Planning and Development Department

Melland, Brandon

From: Serna, Francis
Sent: Wednesday, April 06, 2016 4:58 PM
To: Melland, Brandon
Subject: FW: please put this in the Planning and Zoning commissioner's packets for Tues April 12

Importance: High

From: Cathy Dillon [mailto:cri@grandecom.net]
Sent: Wednesday, April 06, 2016 4:57 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: please put this in the Planning and Zoning commissioner's packets for Tues April 12

Dear Commissioners:

I wrote a letter two weeks ago that capsules very well how my husband and I feel about the proposed Lindsey Hill development. Here are some recent addenda to that original letter.

--In their own Consistency with Vision San Marcos document, there is the remark that "Lindsey Hill has the potential to catalyze substantial investment and redevelopment in surrounding, partially blighted neighborhoods." Really? Since when is our Historic District, which closely surrounds 3 sides of this monolithic proposal, "partially" or even just a tiny bit blighted!! What would look better fronting on Burleson, Blanco, and Hutchison? Shiny new parking lots, maybe?

--Take a look at the developers' Arqeo and LAZ home pages. These people build and maintain parking facilities and commercial shopping centers, with maybe a little bit of "buy it and flip it" mixed in. Mr. Lerman told one of our neighborhood meetings that he wants this to be a legacy development for him, and yet nowhere does this type of development show up in his online presence. I certainly don't see anything that looks like adorable downtown Haddonfield New Jersey, which is the model he espouses. Haddonfield is indeed fabulous, but then, it is all about centuries-old buildings that are walkable and touristy and right downtown. Lindsey Hill is NOT DOWNTOWN. It is in a residential area that in the city's own Master Plan, is listed as an "Area of Stability" which means, it should be, ummm.....stable. And the architects listed on his PDD have a marvelous portfolio of steel and concrete and glass boxes. Not exactly what I'd hope for in this very sensitive very pivotal very visible place.

--I have felt for 8 years that Sanctuary Lofts is just something our inn at 326 W. Hopkins had to endure, with its 75 high Hutchison Street wall blocking out the sun, its all-night parties, endless garbage, tenants clogging our private parking lots, and so forth. But not until I read a specific letter to the editor in last Sunday's paper, did it ever occur to me that perhaps this giant building has had something to do with recurrent flooding of our downstairs apartments. We have three ground-floor apartments, and longtime staff reminds me that on five separate occasions since Sanctuary was finished in 2008, one or more of those units have flooded! We've been here since the 1980's and everything flooded in 1998 too, but flooding just during a normal dumping Texas deluge.....is a new phenomenon!! And, only one of those instances was last year during the Halloween flood. All the others occurred during years that have generally been accepted as a drought period. Don't know why we've never made that connection before, but we didn't. Although, what good would it do to complain? And who would we complain to?

More importantly, what if a 5-story building that is 4 times as long as Sanctuary is, causes repeated flooding at the base of Lindsey Hill, along the 2 blocks of Hutchison and Hopkins that are sitting ducks for all its runoff? Would that make those blocks “blighted”, and thus more attractive for “redevelopment”? Just wondering.

Friends, in your hands rests the very survival of the Historic Districts. If this thing goes in, a lot of beauty and peace and history and charm will go.....out. Maybe not next year, or the next, but let me suggest that it would be far better for your commission to be remembered as the group that saved the Historic District, not the one that launched its eventual destruction. Prayerfully, Cathy Dillon 1000 Burleson Street

My husband and I own a historic hotel/apartment complex (the Crystal River Inn) on one end of San Marcos' historic district, and a historic home near its other end. We have spent 32 years of our lives fiercely loving and protecting and promoting the historic legacy of this town.

Yet we were asleep at the switch when the Sanctuary Lofts project, across the street from the back of our inn, was proposed. We relied on the word of others who said that it would just be a two-story apartment complex surrounding the magnificent and intact old Baptist Church. "How bad could it be?" we thought. "We have no right to dictate what others can do with their property because our area is commercially zoned, we'll just trust the city planners and sit this one out". 2 filthy loud miserable years later, the old Baptist church had been swallowed up by a 5-story apartment box and we were sentenced to nightly loud parties with varying results including at least one death of someone who fell from a 5th floor balcony.

Then came Zelick's, next door to the inn. Having learned a lesson from the Sanctuary experience, this time we talked to the developers themselves, who asked our support for their quiet little garden bar. We gave it, wrongly concluding that it was to be like Palmer's hospitable courtyard. We did not expect the wee hours racket from a pretty place that turned out to appeal to drunken screaming nightowls.... not okay with our clientele who have the right to sleep.

Four years later, we have forged a fragile working relationship with Zelick's owners, who to their credit have become much more proactive and sensitive to our plight. But the real problem in this case was that an OUTDOOR BAR should never have been allowed next to HISTORIC HOUSES-TURNED-INN. It served both properties badly.

We had a chance to sell the inn recently for a lot of money, but we didn't do it because we naively hope that our presence will serve as a buffer against the very real threat that downtown "stuff" will creep into the historic residential side of Moore Street, and eat its way toward its irreplaceable core of magnificent old homes. Indeed, that intact residential area is something that every bit of San Marcos promotional literature touts. It and the river are the very heart and soul of this community.

The Lindsey Hill development will bring commercial and retail and yet another 5-story apartment box and an up-to-120 room hotel (bigger than any hotel currently in town except for the Embassy) and hundreds of cars per day dumped onto historic area streets. Need I say that I am terrified? Not unlike the huge hotel which was proposed to be built up above Aquarena Springs, this huge thing wants to feed off our glorious unique asset, in the process of which I believe they would launch its eventual destruction. The developers state that they really care about preserving and promoting all that is wonderful about the Historic District-into the very gateway of which they want to shoehorn themselves.

Really? Why on earth do we need this? How about: as a community who owes it to our forefathers and our history and our children, we draw a circle drawn around the entire historic residential area, and the city allows nothing and no one to cross that zoning line. Not for any non-residential reason whatsoever.

It is the greatest gift we can give the future of this exploding town.

Melland, Brandon

From: Serna, Francis
Sent: Wednesday, March 16, 2016 4:04 PM
To: Melland, Brandon
Subject: FW: Lindsey Hill project

From: Christine Brown [mailto:christine.brown.mft@gmail.com]
Sent: Wednesday, March 16, 2016 4:04 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: Lindsey Hill project

Dear Commissioners,

Please....don't let this happen.

I left Southern California and chose San Marcos for a reason; let me explain.

Huntington Beach was already a large city when I first moved there more than 30 years ago, but the downtown area at the foot of the pier retained its charm. It had historic buildings and a quaint, old-school surf city feel. Sadly, over the years, the planning and zoning commission sacrificed that charm on the altar of high income development. Down came the old buildings and up went expensive hotels, high rise apartments and condos. Traffic, both pedestrian and auto, became a nightmare. Small businesses were pushed out and the chain stores moved in. Huntington Beach lost any sense of its history and became indistinguishable from any other high end beach city. Great for the city's coffers, but not good for most residents. Careless development made me want to leave.

When I visited San Marcos less than a year ago, I honestly fell in love with this city. The historic districts, with their bygone era homes and tree lined streets, were a balm for my soul. I purchased a home last September, and am now a proud San Marcos resident-for-life.

I beg you to NOT do what Huntington Beach has done. I understand that the city is growing, but it is your duty to see that this growth is directed toward suitable locations. A five-story hotel and dense condominiums at the gateway to the historic district is not a suitable location. The traffic congestion alone should be reason enough to deny this use.

But, of course, there's more than that. A high rise box on the front stoop of our most beautiful historic homes is a Southern California mistake that I hope with all my heart won't be repeated in this beautiful Texas city.

Thank you for your consideration.

Christine Brown

Christine Brown, LMFT
201 Camaro Way

San Marcos, TX 78666
(714) 717-2521

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16 March 2016

ATTN: Francis Serna

RE: Lindsey Hill Project

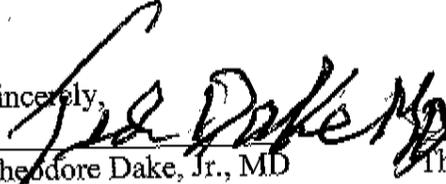
I am writing regards the proposed Lindsey Hill Project. I am a resident of the Historic District as are two other generations of our family, all of whom work and live in San Marcos.

Although we certainly support the growth and development of San Marcos, we do oppose future construction that seems to be benign at start and end up as a "malignant tumor."

This project on the surface may sound good, but considering the scope, size and traffic density it requires more time for all citizens of San Marcos to have adequate information and input regards its appropriateness. It does not give us confidence that what the developer says they would do, is what they will do.

Please consider delaying in this project so more of us citizens can be involved and informed. I do not want this to be like Obama Care, pass it and then we find out what it really involves.

Sincerely,


Theodore Dake, Jr., MD


Thea M Dake, LCSW

Melland, Brandon

From: Serna, Francis
Sent: Tuesday, March 22, 2016 8:21 AM
To: Melland, Brandon
Subject: FW: Lindsey Hill Project

From: Dennis Hunt [mailto:dhunt@grandecom.net]
Sent: Monday, March 21, 2016 5:39 PM
To: smhistorymatters@gmail.com
Cc: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: Lindsey Hill Project

To Whom It *Should* Concern:

Thank you for this opportunity to include our input about this very alarming proposed project. We have lived in San Marcos for over 20 years and moved here for the historic neighborhoods and small town feel. **Historic homes and neighborhoods should be preserved and should be a priority for city planners.**

Growth and development is good for the city and citizens, but any growth should complement the existing footprint of a neighborhood. In the last few years we have seen explosive growth in density with massive apartment complexes and student housing projects that have had detrimental consequences with increased traffic, noise, and a strain on environmental resources for this city. **Don't make Austin's mistakes and out build what the existing neighborhoods and roads can handle. Our historic homes and peaceful neighborhoods will be negatively impacted by the current proposal for this project. We want to live in San Marcos, not Philadelphia.**

This property would be better served by: keeping a good portion of it as greenspace park; building a small over 55+ apartment community (2 stories maximum) so that the residents are able to walk to the grocery, shops, and restaurants in the immediate area; and perhaps adding a minimal number of small retail shops. Density should be kept at a minimum. Our streets throughout the neighborhood have already seen an increase in traffic as drivers are looking for shorter routes to avoid gridlock on RR12, Hopkins and other main streets. This property should be drawing more pedestrian than motor traffic.

We are very upset by this current proposal and want our voices heard. High density building = more profit for the developers and an all-around loss to the homeowners and community. The city should not make a "sweetheart" deal and sell this neighborhood down the river. Thank you.

Dennis and Margaret Hunt
608 Burt Street
San Marcos, Texas 78666

*Edwin F. Kuny
212 Sierra Ridge Drive
San Marcos, TX 78666*

April 4, 2016

Mr. Jim Garber
P&Z
San Marcos, TX

Dear Jim:

This letter is to ask for your vote to deny the change in zoning or in any way allowing proposed development for the area known as Lindsey Hill in San Marcos.

I certainly appreciate your service to the community in serving on the Planning and Zoning Commission I know that it is a time consuming and sometimes thankless job. I believe that you succeeded me on the Neighborhood Commission. This does indicate to me that you are interested in keeping the integrity of our neighborhoods strong. Such a project as planned by the developers of the Lindsey Hill area is in direct conflict with neighborhood integrity. The people who have chosen to live in this wonderful community have done so because we liked the friendly neighborhood feeling, and many have invested in properties and built our homes here for that reason. There are many locations outside of our neighborhoods for high rise/heavy commercial projects.

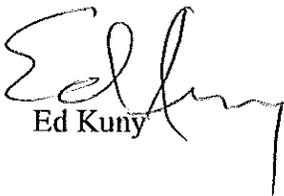
The Lindsey Hill area is especially fragile. It is, if not wholly a part of the Historic District, is fully adjacent to it. The project, as planned, would seriously and detrimentally affect the Historic District and the properties of all of the homeowners within it.

The traffic situations with relation to Moore Street (RR 12), Hutchison, and Burlison Streets will be nearly impossible to negotiate. There are times already when it takes two traffic light rotations for a vehicle to clear the Hutchison/Moore Street and Hunter/Moore Street intersections. The added traffic created by a project such as proposed would increase the problem two or three fold.

I am a member of the Session (Governing Body) of the First Presbyterian Church, just up the street on Hutchison across Moore Street from the area. We just recently invested several thousands of dollars in building a new parking lot on our property due to the heavy traffic and lack of parking on the streets around the church. So, even though the developer plans a parking garage with his project, it is not hard to realize that we will have to take strict measures to insure that our lot is kept available for our congregation rather than full of overflow vehicles from this project.

San Marcos has a rich history and culture that has been too often ignored when developers from outside our community have petitioned the city for their personal financial interests. It is not the mission of the P&Z and other city officials to protect the financial interests of such developers but rather to protect the integrity and financial interests of the citizen property owners who have chosen to make this lovely city our home.

Again I thank you for your service on the very important P&Z Commission and for your interest in keeping our community's neighborhood integrity intact.


Ed Kuny

(512) 353-4339
ekuny@grandecom.net

Ellen T. K. Ault
816 Belvin St.
San Marcos, TX. 78666

Dear P & Z Board,

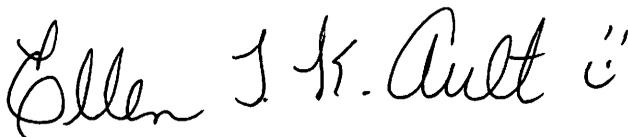
Thank you for serving on this board as I can only imagine how challenging it is at times.

San Marcos is growing and this is very exciting! I want us to grow but as I call it, "grow smart". I am writing over my concerns on the new proposed development Lindsey Hill at the old Lamar building on Hutchison and Ranch Road 12. It sounds like an incredible idea: restaurants, stores, Boutique hotel with over 120 rooms, upscale apartments-164, convention center, parking, and a green space park, all on only 5 acres of land. I want to support and encourage new businesses; I feel this is a bit overzealous and would be better if it was scaled down quite a bit.

For feasibility purposes, I know they said they had done a study and there would be enough parking, but lets be practical and honest- NO way! This development is in the Historic District; therefore, we need to be cautious to uphold the integrity of this area. The Historic District is a unique treasure in San Marcos. In other towns and cities development has not often not had the foresight to protect this precious treasure sighting the need for modernization and forward thinking. We do not want this to happen in San Marcos and must be cognizant of the existing rules surrounding the Historic District.

Again thank for doing your due diligence.

Respectfully Submitted,

A handwritten signature in cursive script that reads "Ellen T. K. Ault" followed by a small flourish.

Ellen T. K. Ault
March 15, 2016

March 18, 2016

Francis Serna
Administrative Coordinator
City of San Marcos
Planning & Development
San Marcos, Texas 78666

Via Email fserna@sanmarcostx.gov

Re: Lindsey Hill Project

To Whom It May Concern,

I am writing to let the City know that my family and I live on Belvin Street and have been living on Belvin Street for the past 24 years. The reason we moved to Belvin Street is due to the charm of the street and the fact that it is in the historical district. I walk the historical district regularly.

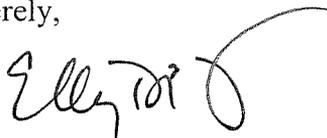
I have some knowledge of the development planned currently for the old Lamar School location on Hutchison Street. My concern is that the project is far too large in scope. We are not opposed to a residential or a boutique hotel use of the property, however, an intense commercial use is not in keeping with our neighborhood. While Bed and Breakfast properties are in the area, this area has never been the site of any intense retail development.

A major concern is that the developers have stated they will only have some 300 parking spaces and this number of spaces does not appear to accommodate the number of hotel rooms, apartments and retail they discussed in their recent presentation to the public. As you know, parking in our City is a premium and our neighborhood already suffers from the influx of student parking that has no place in our neighborhood because of the proximity to the University.

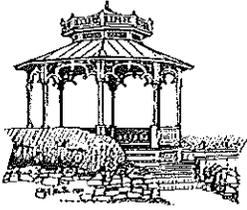
In short, my family and I are not opposed to some development of this space, but the planned development presented thus far will present far more problems for our neighborhood and the City than benefits.

Thank you for your attention to this matter.

Sincerely,



Elly Del Prado Dietz



The Heritage Association of San Marcos, Inc.

Founded 1975

"Preserving the Past for the Present and the Future"

March 15, 2016

Planning and Zoning Commission
City of San Marcos
630 E. Hopkins
San Marcos, TX 78666

Re: Zoning Changes for 500 W. Hutchison Street (Old Lamar School)

Dear Commissioners:

The Board of Directors of the Heritage Association of San Marcos respectfully request that you postpone any zoning changes of 500 W. Hutchison Street until the developer has successfully provided a complete set of plans along with architectural renderings of the facades of this building and the specs for the building.

If you have any questions, please contact me at 512-392-5580.

Thank you in advance,

Robert Cotner
Parliamentarian/Member at Large

Resolution, March 14, 2016

Received 3/17/16 10:18am

J. KAMA DAVIS, ATTORNEY AT LAW

16 March 2016

Francis Serna
Administrative Coordinator
City of San Marcos
Planning & Development
San Marcos, Texas 78666

Via Electronic Mail fserna@sanmarcostx.gov

Re: Lindsey Hill Project

Dear Francis Serna,

I am writing to let the City of San Marcos know that I am highly *opposed* to the Lindsey Hill Project as it is proposed. I have lived in San Marcos for 42 years. I actually went to Lamar Middle School! Now, I have my own private law practice, work full time at Texas State University, and live in San Marcos near the historic district.

I grew up on Hopkins Street, in the historic district. My parents Dr. Karen Brown and Reverend Karl Brown still live in that home. Over the years, as our tiny town has grown, so has the permanent population and the University population. We have seen huge apartment complexes, the expansive outlet mall, and new housing developments go up with lightening speed. Our city simply cannot continue to congest the neighborhoods with retail shops, more large apartment complexes, and developments that require vast space to accommodate their parking needs.

I am absolutely in favor of growth in San Marcos. But, it must be responsible growth that takes into consideration the needs of the permanent residents and the student population. This project could be built elsewhere, especially some place that is not already congested. A different type of development at the proposed site would be appropriate, even. Big business and San Marcos' small town feel are compatible. They just aren't compatible with the current Lindsey Hill Project. Thank you.

Kind regards,

J. Kama Davis

J. Kama Davis

512.757.8695 p
512.757.8696 f

323 West Hopkins, San Marcos, Texas 78666

www.attorneykama.com
judith_kama@hotmail.com

James K. Baker
727 Belvin St.
San Marcos, Texas 78666

April 3, 2016

Planning and Zoning Commission
City of San Marcos
630 E. Hopkins St.
San Marcos, Texas 78666

Dear Commissioners,

I would like to first thank you for your service. The many hours that you sacrifice are to the benefit of San Marcos, and are very important in this rapidly growing city. Planning for development is like planning for surgery. It must be done very carefully to increase the chances for a good result. A botched surgery is not easily corrected, and the salvage results are often not as good as when the first attempt is done well. I appreciate your special care to increase the chances that our city will grow in a good way.

I live on Belvin Street, the fourth house from Scott Street, in very close proximity to the proposed project, and have lived in San Marcos since late 2005.

I am asking you to vote against the currently proposed PPD and ask the developer to go back to the drawing board and give our city a better plan.

I would like to see a residential PDD that will enhance my neighborhood: one with townhouses and apartments.

Being respectful of your time, I would like to briefly record my concerns regarding the proposed Lindsey Hill PPD in expanded list form for quicker reading.

1. *No development is not an option.*

I am excited that we are having proposals for the “dead space” to be developed. Most people seem to recognize the need for that, and that it is not reasonable for the area to be a park. However, **what we need is residential infill: apartments and townhouses, not huge hotels.** Businesses downtown are already having a tough time surviving. Let’s not make it harder for them by bringing in competition.

Single family attached homes (townhouses and row houses) comprise only 1.8% of housing options in San Marcos.¹ This would be an excellent housing option for this area in a transition between single family homes and apartments. The

¹ Vision San Marcos Comprehensive Plan, p. 89.

developer has said that he would like to attract young professionals and those with very young children as well as older adults looking to scale down. Let's get real! Are they going to live next to a huge hotel? Would you? My former neighbors decided to scale down from their home on Belvin Street to something smaller with less yard maintenance. They thought a townhouse would be ideal for them, but they had to move to San Antonio to find an acceptable one. Our housing diversity for single family attached housing is in extremely short supply, almost non-existent. Planners, this is what we need!

2. *The development needs to be done right the first time.*

I have heard people say, "We need to pass what the developer wants because we need to develop this area badly." Instead I say, "We need to develop this area well." The developer has not got it right the first time. After reviewing the PDD, listening to the developers for hours, attempting to have dialogue with them, and after discussing the PPD with city planning staff, I find many problems with the proposed PPD.

3. *We need neighbors.*

The Heritage Neighborhood is model traditional neighborhood district (TND) that new urbanists would give their eyeteeth for. We have all of the elements that new urbanists long for: density, variety of housing types in close proximity to each other, grid-like street pattern with no cul-de-sacs, back streets for garbage, garages in the rear, high percentage of owner-occupied homes who maintain meticulous upkeep for their houses and landscaping, and close proximity to neighborhood grocery store. We have perhaps the most walkable streets in the city and a very large selection of well-maintained historic homes, some just one generation later than W.W. Moon's first cabin. On top of this, we have lots of frequent interaction with our neighbors, frequent parties, a Mardi Gras krewe and more. In short, we are a real community, what is missing so badly in the suburban nation.

What do I want from the development of the old Coronal site (500 W. Hutchison Street)? I want neighbors.

4. *A business is not a neighbor.*

Residential areas surround the Lindsey Hill site on three sides. We live in a neighborhood, not a "business-hood". Mr. Lerman has proposed traditional apartments, which I thank him for, but also proposes a kaleidoscope of inappropriate businesses for the project, inconsistent with the character of the surrounding neighborhood and many goals of the Vision San Marcos Comprehensive Plan (hereafter, "Comprehensive Plan").

The Heritage Neighborhood is not a historic district commodity to infill with quaint coffee shops, art galleries, and antique shops. It is a living, vibrant community of people who care about San Marcos history. It matters!

5. *High density mixed use development is inappropriate in an Area of Stability.*
Because this is a PDD, it may be legally possible to propose a mixed use development, which is very close to a SmartCode development, but this is not following the spirit of the Vision San Marcos masterplan. The City of San Marcos Preferred Scenario Map clearly shows that land area south of Moore Street from Lindsey Street to Hopkins in an Area of Stability². It is not an Area of Change, and is not an area of high intensity, medium intensity, or even low intensity. This means that areas that are developed should maintain the character of the area and not change it.

SmartCode or high-density mixed use developments should not be put into an Area of Stability. After speaking with citizens who helped to develop the Vision San Marcos plan, it is clear that the intention was not to allow PDD loopholes to void the Preferred Scenario Map guidance.

Land Use Goal 2 of the Comprehensive Plan states, “High density mixed use development and infrastructure in the activity nodes and intensity zones including downtown area supporting walkability and integrated transit corridors.”³

I ask you to recognize that Lindsey Hill PDD proposal is a betraying violation of the spirit of the Vision San Marcos Comprehensive Plan.

6. *The Lindsey Hill PDD violates Vision Statement and Goals 1, 3, and 4 of the Neighborhoods and Housing section of the Vision San Marcos Comprehensive Plan.*

The Vision San Marcos Comprehensive Plan (hereafter, “Comprehensive Plan”), requires that the primary guidance for zoning be based upon the Comprehensive Plan: “It follows, therefore, that the first question in any zoning case should be, ‘Does this request comply with the comprehensive plan? The plan’s goals, preferred scenario map, and the matrix provide that first level of guidance.’”⁴ Furthermore, “Zoning is a discretionary act of the part of City Council That discretion is limited, however, by requirements that it not be arbitrary... Making zoning decisions based on the comprehensive plan and the use of objective analysis is essential.”⁵

It follows that significant variances from the Comprehensive Plan should provide reason to deny a zoning request.

The Vision Statement (“We envision San Marcos to have a strong, more comprehensive foundation of safe stable neighborhoods while preserving and protecting the historical,

² <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=17791> (accessed 3-31-2016).

³ Vision San Marcos Comprehensive Plan, p. 43.

⁴ Vision San Marcos Comprehensive Plan, p. 83.

⁵ Vision San Marcos Comprehensive Plan, p. 85.

cultural, and natural identities of those neighborhoods.”⁶) Goal 1 of the section states, “Neighborhoods that are protected and enhanced in order to maintain a high quality of life and stable property values.”⁷ Goal 3 references affordable housing⁸, and Goal 4 states, “Well maintained stable neighborhoods, protected from blight or the encroachment of incompatible land uses.[emphasis added]”⁹

The Lindsey Hill PDD will drastically disrupt the cultural and natural identity of the surrounding Heritage Neighborhood and irreparably damage the socioeconomic diversity of our neighborhood (comprising a violation of the vision statement). A prime characteristic of our neighborhood is the socioeconomic heterogeneity of the neighbors. We have people from all walks of life, among the wealthiest citizens of San Marcos and those of more humble means, artists, architects, physicians, carpenters, laborers, creative sign artists, handymen, professors, students, policemen, all living in close proximity to each other. On my block there is an 1895 moderately large Victorian home next to a 1960’s single level flat, a single level historic home (somewhat modernized after a fire removed the second floor), a single bedroom small rental house (that rents for 375.00 per month), three other garage apartments with students and University teaches, a two story rental house, a Craftsman bungalow, and a large Craftsman home on Hopkins. Down Belvin Street we have the mansion Crookwood, smaller and larger homes, and just across Bishop, we have Habitat for Humanity homes. Around the corner from my house we have townhouses on Scott Street and apartments on Hopkins and San Antonio Streets. We have high proportions of old, young, and middle-aged people in a mixture more diverse than in any other place in San Marcos. If you would try to find a “melting pot” in San Marcos, the Heritage Neighborhood would be it.

People from all of the homes, rental units, and apartments come to Night Out Against Crime, Neighborhood porch parties (Porches of San Marcos), and parades.

The Lindsey Hill PDD will adversely and drastically change the historical, cultural, and natural identities of our neighborhood. We have heterogeneity of all economic classes with housing opportunities of all sorts of houses (in addition to apartments) in our neighborhood. In contrast, the Lindsey Hill PDD will have expensive apartments that will be exclusively be populated by more wealthy tenants. It is my opinion, that the high rent and rigorous economic requirements (quite possibly *de facto* discrimination against minorities) for tenants will lead to an unhealthy economic homogeneity that ironically smart growth proponents rail against.^{10, 11}

⁶ <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=8028> (accessed 3-31-2016).

⁷ <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=8028> (accessed 3-31-2016); Vision San Marcos Comprehensive Plan, p. 44.

⁸ <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=8028> (accessed 3-31-2016); Vision San Marcos Comprehensive Plan Addendum, p. 45.

⁹ <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=8028> (accessed 3-31-2016); Vision San Marcos Comprehensive Plan Addendum, p. 45.

¹⁰ Duany, et al., *The Smart Growth Manual* (Chicago:McGraw Hill, 2010), 5.3.

¹¹ Duany, et al., *Suburban Nation: The Rise of Sprawl and the Decline of the American Dream* (New York: North Point Press, 2000), 46.

The area surrounding 500 W. Hutchison is bounded on three sides by residential houses, owner-occupied and rental, and apartments, and these areas are clearly identified in the Preferred Scenario Map as Areas of Stability. In addition, there are no huge hotels anywhere near the area. Placement of a huge hotel will drastically and irreparably change the character of the area. The Comprehensive Plan, addressing Areas of Stability states, "...any changes, whether new developments, zoning requests, public improvements should be carefully planned and implemented so that the character of the area remains. [underline added]"¹²

The Lindsey Hill PPD violates the Comprehensive Plan by drastically changing the character of the surrounding neighborhood, designated as an Area of Stability.

Unlike Austin, we still have affordable housing and a place for those with more modest economic means right here in our neighborhood. The Lindsey Hill PDD has NO mention or mechanism of affordable housing and will adversely and drastically change the character of our neighborhood with respect to opportunities for housing diversity and affordable housing, a primary goal of smart growth.¹³

This is not just a "feel good" item. The Addendum to the Comprehensive Plan contains the Texas Fair Housing Impediment Analysis.¹⁴ In this report "Section 6: Impediments to Fair Housing and Remedial Actions, Lack of Affordability, Impacts" contains a paragraph of pertinence: "Income diversified neighborhoods and neighborhoods that are accessible to a mix of incomes have shown a greater potential to maintain themselves as a viable community."¹⁵ Obviously, the Heritage Neighborhood meets this definition.

The Addendum's section on remedial action to address lack of affordable housing includes four pages of recommendation for Inclusionary Zoning.¹⁶ It is instructive to review just a few paragraphs from those pages:

"...we also recommend that the City initiate an effort to research and consider one particular policy change, inclusionary zoning, as one alternative means to promoting balanced housing development. Inclusionary zoning has been used in other communities to ensure that some proportion of new housing development is affordable..."¹⁷

"...In practice, these policies involve placing restrictions on 10-30% of new houses or apartments in a given development to make the cost of the housing affordable to lower income households. The mix of "affordable" and "market rate" housing in the same neighborhood is seem as beneficial by many, especially in jurisdictions where housing shortages have become acute...[and] can also be applied when residential planned unit

¹² Vision San Marcos Comprehensive Plan, p. 79

¹³ Duany, et al., *Suburban Nation: The Rise of Sprawl and the Decline of the American Dream* (New York: North Point Press, 2000), 48.

¹⁴ Vision San Marcos Comprehensive Plan Addendum, p. 232.

¹⁵ Vision San Marcos Comprehensive Plan Addendum, p. 278.

¹⁶ Vision San Marcos Comprehensive Plan Addendum, pp. 279-283.

¹⁷ Vision San Marcos Comprehensive Plan Addendum, p. 279.

development zoning is requested.”¹⁸

Furthermore, the Current Conditions Analysis, Technical Report in the Comprehensive Plan Addendum lists as a component of goal 3 of Neighborhoods and Housing section of the Plan (NH G3-03): “Develop an affordable housing program. Ensure that Development Zones contain adequate affordable housing as they develop.”¹⁹

The Lindsey Hill PDD violates goal 3 of the Neighborhoods and Housing section of the Vision San Marcos plan by ignoring affordable housing and disrupting the balanced housing and socioeconomic diversity of our existing neighborhood.

In addition, the Lindsey Hill PDD proposes the placement of a 120 bed hotel with a conference center and bar with alcohol service directly next to an exclusively residential neighborhood. My neighbor has likened the Lindsey Hill PDD to King Kong in the neighborhood. I favor the Stay-Puff Marsh Mellow Man. Both illustrate the absurd ill-proportioned, dominating, out of character nature of this development. The Comprehensive Plan in the section on Land Use, Description of Development Zones, Downtown, states: “Buildings around the square and adjacent to historic neighborhoods will [emphasis added] maintain their current scale.”²⁰ Note that the directive is a requirement (“will”), not an option. The Lindsey Hill PDD will drastically change the current scale of building heights in an area adjacent to historic neighborhoods.

The Lindsey Hill PDD violates goal 4 of the Neighborhoods and Housing section of the Vision San Marcos plan by causing “the encroachment of incompatible land uses.”

Finally, property values are not likely to remain stable next to a hotel. Whether they go up or down, **the Lindsey Hill PDD has a high likelihood of significantly changing property values and is therefore in violation of Goal 1 of the Neighborhoods and Housing section of the Vision San Marcos plan** (“Neighborhoods that are protected and enhanced in order to maintain a high quality of life and stable property values.”²¹)

7. *The Lindsey Hill PDD will exacerbate the downtown traffic problem.*

How so? The hotel to downtown distance does not meet the definition of “walkable”, which is generally accepted by smart growth standards as a five minute walk.²² I walked from Blanco Street and Hutchison to Guadalupe St. and Hutchison, on a beautiful, cool and clear Easter Sunday, and it took me, a healthy young man in tennis shoes, without stopping, nine minutes. Are people going to walk downtown from the hotel in the summer or winter? I seriously doubt it. Most will take their rental cars so they won’t have to wait for a shuttle, which will only

¹⁸ Vision San Marcos Comprehensive Plan Addendum, p. 280.

¹⁹ Vision San Marcos Comprehensive Plan Addendum, p. 376.

²⁰ Vision San Marcos Comprehensive Plan, p. 70.

²¹ <http://www.ci.san-marcos.tx.us/modules/showdocument.aspx?documentid=8028> (accessed 3-31-2016)

²² Duany, et al., *The Smart Growth Manual* (Chicago:McGraw Hill, 2010), 1.5.

exacerbate the downtown parking problem. What we need are hotel rooms *downtown*, not in the Heritage Neighborhood.

8. *The developer has no experience in making this type of development.*

The developers have a pleasant demeanor. However, there is no substitute for experience. Patients will often ask a surgeon, “Have you done this operation before?” If the surgeon cannot answer that he or she has done the operation many times before, then a patient would do well think twice before letting the surgeon experiment on him; the chance of a poor result or complication is increased. It is not acceptable for the surgeon to say, “No, I haven’t... but I want to do a legacy operation... one that my grandchildren will talk about.”

I have grave concerns that the developers have not ever produced a mixed use development like this. I feel that Mr. Lerman was not honest about his experience when questioned directly about this during his second neighborhood meeting. Several people from the audience asked him if he had ever done a similar mixed use project. He replied that he had not exactly done a mixed use project like this, but that he had done all of the components of this project before. To my knowledge he has never developed a large hotel. I am concerned that his lack of experience may lead to a poor outcome if you accept his plan for the PDD.

9. *The developer may “flip” the property.*

It has not escaped the notice of many people that the developers will not invest any capital into the second phase of the project beyond the re-zoning phase, with a reason that it would take millions to do so.

My concern is that the developers may have no intention of actually developing this property but rather plan to sell the re-zoned property to another developer, just as they did with the old post office property on Guadalupe. I suspect that this may be the reason behind the virtual three-ring circus of uses crammed into a 4-acre property bounded on three sides my small neighborhood streets.

A brief survey of the uses requested²³ for this PDD would be comical if the request were not real as the barrel of a shotgun:

- A 120-bed hotel with conference center
- A 200+ bed apartment complex
- Office space
- 17,000 square feet of retail space with up to 8,500 sq ft. of restaurant space
- Microbrewery and microdistillery
- R&D offices and research laboratories (up to and including BSL-2 labs as defined by Centers for Disease Control)
- Fire and police stations

²³ Lindsey Hill PDD Standards Book, p. 77.

If the property is sold after zoning, the next developer will be in a better position to have amendments made to alter the PDD if the use was already in the first PDD to make the project even less favorable to the surrounding neighborhood.

10. *Students? They're not the problem! (And they will be there, anyway.)*

I moved to San Marcos and lived in the Palazzo apartment complex from 11/2005 to 11/2008. I had no idea of any “student problem” that neighborhoods were having until I began paying attention to city government after the previous administration tried to blanket zone the city as SmartCode. My experience at the Palazzo was pleasant, and I would recommend it to any adult. Although the Palazzo had a large student population, the complex was conventional rental (not by the bedroom) and run by three young ladies who were young enough to be in a sorority, but who ran the complex with an iron fist. Tenants could not put towels on the balcony railing to dry without firm warning. Since they demanded mature behavior, they got mature behavior.

My point is that students are not a problem. I love students, or I would live in another town. *The problem is irresponsible landlords who foster immature behavior* (“hey, let’s have an official keg party by the pool!”- Sound familiar?). If a landlord demands mature behavior, then that is what will happen.

I appreciate the developers’ refusal to rent by the bedroom, but I do not appreciate the anti-student prejudices. Frankly, I am embarrassed by them. I think that it is likely that the economic hurdles to renting that the developers are proposing will be ruled as age discrimination by the HUD FHEO Regional Office in Fort Worth, Texas. Apparently, a complaint must be filed before they will rule on it, but I urge you to consider the strong possibility that the Lindsey Hill PDD will certainly have a large student population in the apartment complex if a complaint is registered with HUD and ruled in favor of the tenant applicant. With so close a proximity to the university, would it make sense otherwise?

11. *Light Pollution*

The proposed PDD will require significant lighting around the huge hotel side of the project to enhance pedestrian safety because of the late hours of hotel operation (for example auditorium and conference center events, many of which will be night time events ending as late as 8-11pm at night). This will lead to an adverse increase in the ambient light in the surrounding residential neighborhood. Cars leaving the events (and they will, because you cannot pass a requirement that only hotel guests can attend an event) will pour into the streets, with lights and traffic late into the night. Many will likely travel south down Hutchison and turn on Scott Street to access Hopkins because of the bottleneck at Moore Street, and some will likely continue down Belvin Street in front of my home. The increase in traffic and ambient light will lead to light pollution for my house. Adding to

this will be the neon or LED lighted sign on the huge hotel tower²⁴ that will be on all night long.

This will disrupt the quiet night time character of our neighborhood and interfere with my sleep and my neighbor's sleep, yet another destructive effect of the huge hotel proposed by the Lindsey Hill PDD.

12. *The Huge Hotel will truly be like King Kong in our neighborhood.*

A 120 bed hotel with extra room for conference centers and special events is terribly out of proportion for our neighborhood and will adversely affect the character of our Heritage Neighborhood. Only the Embassy Suite is larger. For a comparison of sizes, please see the [Appendix](#).

13. *Traffic.*

Huge hotels are placed next to interstate access roads or large arteries. Emptying a huge hotel onto the small neighborhood streets not designed for such traffic is not wise and will produce a bottleneck. If this mini-city is placed on the old Coronal site, what will happen? Traffic on Hopkins and Moore Streets: A nightmare! Traffic on Belvin Street: Now you're ruining my street. Gone will be the scenic road for bicyclists and joggers. Up will go the 6 ft. fences with "Private: Keep Out!" signs, and up will go the signs to limit parking. Will we have to totally ruin the street and put in speed bumps? Don't take any more pictures of my street. All you'll see will be the cars.

14. *No Flooding Analysis!!!*

After the Woods Apartments disaster, it is only common sense that an independent and very careful analysis of flooding potential for the surrounding neighborhood and even the commercial areas north of Moore Street is mandatory. I ask you to not only deny this request but to authorize and task the Planning Department to require an independent, expert hydrology study on any future proposal for the area. Not only is there potential for flooding, but the area is in an environmentally-sensitive zone.

Also note that the developer is asking for only 15% non-impervious cover while our Planning Department, without the ability to order an expert hydrology study, recommends 20%.

15. *The hotel and commercial property will likely lead to the loss of historic homes.*

Our traditional neighborhood district is like a fragile ecosystem. Even when a well-meaning person introduces an invasive species into an ecosystem, the intruder can wreak havoc, take over and lead to the death of the native species and replace the native species. This can then lead to the irreversible loss of the ecosystem as previously known, and eventually the extinction of a species. Sometimes an endangered species will need protection to prevent extinction.

²⁴ Lindsey Hill PDD Standards Book, p. 41 (as of 3-30-2016).

History repeats itself. Without adequate protection and care from predatory real estate practices, historic homes, and the neighborhoods they are in, can be replaced by commercial properties, destroying the ambience that attracted the parasitic businesses. The domino effect is real!

It is easy to see the numerous places where this has happened. It is the usual. Only in areas with strong protection for historic properties is this avoided to some degree: places like Charleston and New Orleans. Do any of these large cities have a four-acre mixed use project like this in a residential area? No, they put mixed use downtown. The area south of Moore Street is not downtown.

Will we be seeing the destruction of historic houses and loss of socioeconomic diversity of a neighborhood in the way that Austin is trampling over its jewels?

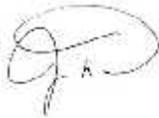
I ask you follow our Vision San Marcos Comprehensive Plan and to protect this fragile ecosystem, the Heritage Neighborhood, a famously successful living traditional neighborhood district that has been part of the soul of our city for over 100 years.

I ask that you vote to reject the current Lindsey Hill PDD so that Mr. Lerman can bring us one that will enhance, rather than dismantle, our neighborhood.

Will my house be here for another 100 years?

It is up to you.

Sincerely,

A handwritten signature in black ink, appearing to read 'J. K. Baker', with a large, stylized flourish at the end.

James K. Baker

Conflict of Interest Disclosure: None, except that I love my neighborhood.

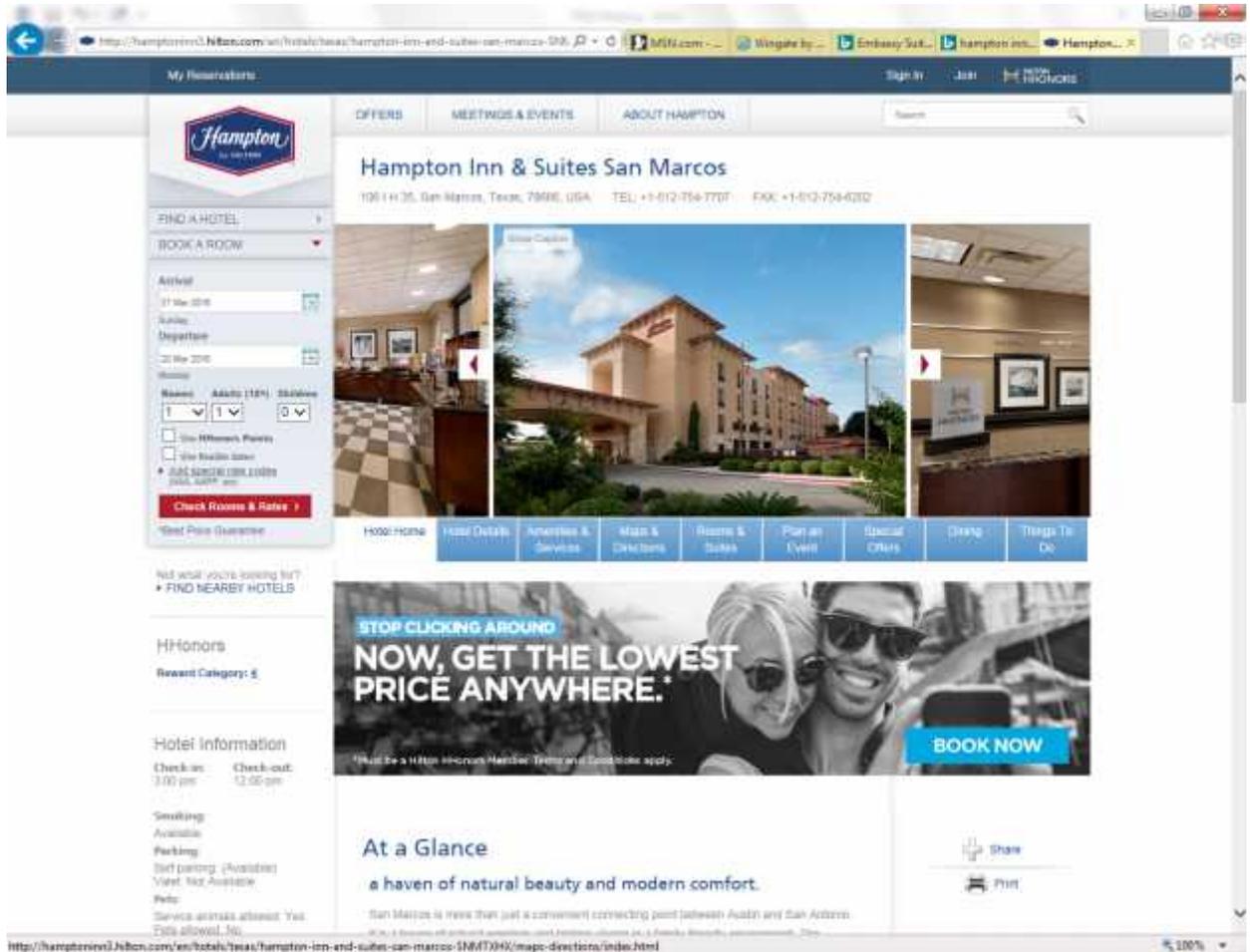
Appendix

Some people have discussed how they like “unique boutique hotels... tucked away in neighborhoods close to town.” Most of those hotels are repurposed older buildings and not new infill. The fact that they are repurposed, older buildings is why they blend in well with the neighborhood. In contrast, this is going to be a new infill, inappropriately sized and architecturally inappropriate to the surrounding residential area.

We should stop using the word “boutique”. This conjures up images of flowers and happiness. Let’s get past emotions and discuss facts. This hotel will be 120 rooms. How does that compare to other San Marcos hotels?

Let’s see on the next few pages:

Hampton Inn hotel on I-35 has 90 guest rooms. The Lindsey Hill hotel will be **25% larger** than the Hampton Inn hotel.



Candlewood Suites at the Wonder World and I-35 80 rooms- the

Lindsey Hill hotel will be **33% larger** than the Candlewood Suites.



Holiday Inn at Bintu Dr. 108 guest rooms- the Lindsey Hill hotel will be

11 % larger than the new Holiday Inn on the I-35 Service Rd.



County Inn Suites on I-35 South 100 guest rooms -- the Lindsey Hill

hotel will be **17% larger** than the Country Inn Suites.



Courtyard Marriott on I-35 South across from the Outlets Malls is 97 rooms.

The Lindsey Hill hotel will be **19% larger** than the Courtyard Marriott hotel.



John H Kuny

123 Riverside Dr

San Marcos, TX

512-557-5533

To planning and zoning and citizens of San Marcos,

Concerning the Lindsey Hill development:

While the old Lamar School is a unique spot for a special development that could benefit all of San Marcos a densely packed over the top multi-multi purpose 5 story box like behemoth (Sanctuary Lofts) in an already tight fit neighborhood is not the answer. First of all isn't it obvious that the existing infrastructure of 1 1/2 lane streets with curb parked cars, crowded traffic light conditions on Moore Street to Hopkins and questionable storm sewer drainage issues coming off the hill is already inadequate.

Of course the developers already have a lot of money in their project but that only brings up the bigger issue of how did it get this far with little hint to the surrounding neighborhoods. Shouldn't people that have lived here for years and have a real stake in San Marcos have a voice in its future. And haven't we seen this too many times already. The Cottages off Craddick that were built to minimal building codes, the on and off again empty concrete superstructure that sits atop the long gone genuine San Marcos icon Rivendell's to the fiasco of the Woods Apartments at Capes Camp come to mind.

The only equation that runs through all of this is mostly outside money forcing their short term vision profit at our long term expense. It's nice to know that San Marcos is the fastest growing 50,000 whatever city but if we can't control it intelligently then it will overwhelm and take over what most of us live here for. (And ask first responders what they think.)

To the Lindsey Hill developers: Nice big idea but if you can't make it work with all aspects of San Marcos and make it work on a smaller scale then maybe it's the wrong plan. How about a little less profit and more maybe space to Terri Hendrix's vision of an arts community for kids and a little more input from the citizens of San Marcos that have to live with the consequences.

Together we all have to draw a line somewhere as to what San Marcos is going to be and to where retail and density end and neighborhoods begin. Moore Street seems like a natural and logical place to do that. We already have the big bully on the hill treating us like doormats to their untaxable kingdom so we don't need outside influences throwing money around at a business opportunity that we all have to pay for. These we told you so's are getting a little old.

Thank you for your consideration, John H Kuny

~~PS Shame on you Scott Gregson, you should apologize, excuse and focus yourself.~~

123 Riverside
SM TX

Karen Tellepsen
826 West Hopkins
San Marcos, TX 78666

To Whom It My Concern:

As a big city gal originally from Houston, I applaud thoughtful, well-researched development. With this in mind, I have a series of questions which I feel need to be addressed in relation to the plans for the property at the old Lamar site. They are:

1. Does the 5 acre tract give the square footage needed to provide the adequate parking for the planned convention center?
2. Do the demographics of San Marcos lend itself to the suggested upscale shops, boutiques, hotel and apartments? I ask this question for the growth of San Marcos is best exemplified by the demographics of the San Marcos Consolidated School District. Does this population shop at upscale stores; stay at boutique hotels; rent top-of-the-line one bedroom apartments? Every venture wants and needs to be successful; thus success is predicated on the audience to which it will appeal. Does this audience exist in San Marcos?
3. The one bedroom apartment with underground parking (this is a must), on site amenities, and services for its tenants would appeal to the young professionals (who abound in Houston) but are not a part of the citizenry of San Marcos.

Looking at this objectively, I have concerns as to the success of this project. The plans should be vetted with all of the above taken into consideration as well as the impact on the neighboring residents. Thank you for your attention to my concerns and those of others .

With respect for your due diligence, I remain,

Karen Tellepsen

Melland, Brandon

From: Serna, Francis
Sent: Thursday, March 17, 2016 4:14 PM
To: Melland, Brandon
Subject: FW: Lindsey Hill Project

-----Original Message-----

From: karl brown [mailto:kwbrown@austin.rr.com]
Sent: Thursday, March 17, 2016 3:37 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Cc: Karl Brown <kwbrown@austin.rr.com>; karen brown <jkbrown@austin.rr.com>
Subject: Lindsey Hill Project

Memo To San Marcos City Council and Planning & Zoning Commission
From: Karl and Karen Brown
RE: Lindsey Hill Project

First of all, thank you for your service and for your decision to remove what's left of Cape's Dam.

We are writing out of our concerns about the Lindsey Hill Project.

We attended the first neighborhood meeting on February 29 with the developers of this project and were pleased with their friendly efforts to explain their vision onsite and to answer questions. After a lengthy session that evening, we left feeling that the developers were more interested in explaining their vision than in receiving real input from the neighborhood.

Among the strengths of the project are: providing enclosed parking; preserving trees; providing a public green space area; appealing by design to attract more permanent residents; and by seeking to be compatible with our historic neighborhood.

Among our main concerns are: the size of the project will definitely add significantly more traffic to an area that is already overwhelmed with cars and trucks - the developers indicated that they want to encourage a lot of traffic on IH-35 that passes by San Marcos daily to come to Lindsey Hill; the five-story apartment section, commercial strip, 300+ parking garage and a 120 room "boutique" hotel are unacceptable as a gateway into our historic district - likely having a domino effect on other development in the district; and finally, the negative impact of this project, when combined with the already existing problems nearby with Sanctuary Lofts and Zelicks, is exponential.

We appreciate the efforts of the City to address some of our issues with the current Hopkins Street Redevelopment Project that is now in the planning stage. Real input from our neighborhood has been sought by our City and is being incorporated into the design. This project, when completed, should be a significant help, especially for those of us living on Hopkins Street. But much more will need to be done to handle present as well as future quality of life concerns in all San Marcos neighborhoods.

We request your continued thoughtful and careful consideration of the Lindsey Hill project in a effective and comprehensive manner. We will be open to continuing the conversation with the developers this Monday as we seek common ground on this important matter. We regret that we will be out of town on March 22, but we hope our views will be heard.

Thank you.

Respectfully,

Karl & Karen Brown
834 W. Hopkins Street
San Marcos, Texas 78666

To: Planning and Zoning Commissioners

FROM: Wayne Kraemer, 733 Belvin Street, San Marcos, Texas

RE: Lindsey Hill Development re-zoning proposal

DATE: March 18, 2016

In April 2013 the City of San Marcos adopted its Comprehensive Plan: Vision San Marcos-A River Runs Through Us. There was over a year of public outreach and involvement to construct and solidify a unified, concise, and thoughtful document describing our concept of sustainable and controlled growth. Now, a development project, The Lindsey Hill Development on the site of the Lamar School at the corner of Hutchison and Moore Street, threatens the very integrity of the democratic process that conceived the Comprehensive Plan.

While I don't completely reject the concept of the development outright, I do have significant reservations about the size and scope of the project and its appropriateness given the Comprehensive Plan and the San Marcos goals described for neighborhoods and housing. A 120-room hotel and a five-story, 150-unit apartment complex (just blocks from Texas State) along with restaurants, shops, and meeting facilities on a historic 5-acre tract adjacent to all of our historic districts seems debilitatingly intrusive. Developers will point to the size of the project as necessary for their profit point, but it is their monetary profit at the expense of our quality of life.

In the preferred scenario of the comprehensive plan, the proposed development in the area bordered by Burlison, Moore, Hutchinson, and Blanco is designated as an "area of stability". According to the city planning office, areas of stability should not see significant zoning changes. The development is huge for the parcel of land it will occupy. To me, it would be more than a significant zoning change. While it is true that there are some small multi-family units within 200 yards of the proposed development, I doubt if all of them combined would equal the scope of this proposed project.

It is offensive to the democratic practice of community input to rush into this request and disturb the stability of the area without an architectural rendering of what the development would look like and a thorough reading of the proposed zoning change.

Additionally, the goals of the city of San Marcos for Neighborhoods and Housing, found on the San Marcos city website, lists goal number one as the protection and enhancement of neighborhoods to maintain quality of life and property values, and goal number four as protecting neighborhoods from encroachment. This project, as currently described, violates both of these goals.

Parking is not sufficient to efficiently and adequately accommodate all of the patrons of the hotel, the apartment tenants, and the clientele for the proposed restaurants and shops discussed in the development plan. The subsequent encroachment of parking in the neighborhood will be both a congestive nuisance as well as a possible safety concern. On street parking will surely be preferred by patrons of the businesses to prevent having to walk from the parking garage and to avoid hassling with the only one entrance and exit from the garage on Burleson Street.

Once patrons or residents do exit onto Burleson Street I suspect that they will exit through the historic district streets of Burleson and Belvin. We already have enough through traffic on these streets. The additional traffic will be problematic thus once again violating the San Marcos goals for neighborhoods.

The San Marcos community survey, listed on the City of San Marcos homepage, indicates that only $\frac{1}{4}$ of our citizens feel the city is planning well for growth. Planning and Zoning will hear a request for re-zoning for the Lindsey Hill Development on Tuesday night, March 22, 2016. Don't let us down again. The Planning and Zoning Commission should vote to table this zoning request until more details are provided and concerned citizens have time to adequately review the proposal. Only through thoughtful and careful deliberation with the developers, we can make this project a jewel and not a blight.

Respectfully submitted,

Wayne Kraemer

Kristen Davis Kline || Brownsville, Texas

16 March 2016

Francis Serna
Administrative Coordinator
City of San Marcos
Planning & Development
San Marcos, Texas 78666

Via Electronic Mail fserna@sanmarcostx.gov

Re: Lindsey Hill Project

Dear Francis Serna,

It has come to my attention that the “Lindsey Hill Project” is being considered as a possible choice for the property currently occupied by the Lamar Middle School. As I understand it, scant written information is available at this time, but I have been briefed by family members who attended a neighborhood meeting last night regarding this project. From what they described, the proposed project sounds similar to the monstrosity that sits where the First Baptist Church used to be, i.e. Sanctuary Lofts, and if such is the case, I am utterly **opposed** to it.

Although I no longer live in San Marcos, I grew up there and consider it my home town. Friends and family still live in and around the nearby historic district, and I visit my childhood neighborhood on a regular basis. It is true that the old school building has become dilapidated and would serve the people of San Marcos best if it were renovated or replaced by something else, but a multi-story apartment complex and parking garage are NOT what a quiet neighborhood needs. Already the streets of San Marcos are congested, and unbridled, galloping growth will serve no one in the community long term.

There is a need for smart growth¹ in Texas communities, San Marcos included. Too many times I have seen the interests of a few take precedence over the many, and the Lindsey Hill Project as I understand it would be no different. The neighborhoods of San Marcos need parks, places of reflection and refuge from the hustle and bustle of daily life. They do not need more traffic, nor rowdy students who will leave San Marcos after they finish university. It seems to me that a green space, community center, or museum would serve the entire community better than yet another profit-driven multi-plex.

I await more information about this project and urge City officials to carefully consider the re-purposing of the Lamar Middle School property.

Kind regards,



Kristen D. Kline

¹<http://www.smartgrowthamerica.org/what-is-smart-growth>

March 16, 2016

Dear San Marcos Planning and Zoning Commission,

I respectfully request that the proposed development plan known as Lindsey Hill be given more time before it is voted upon. A development of this scope will have a huge impact on its surrounding neighborhoods and it is not wise to rush into a decision for all concerned.

There are several facets to this project that need to be fully explained and time given to be fully explored and understood.

Thank you for your consideration,

LeAnne Smith

San Marcos, TX 78666

Melland, Brandon

From: Serna, Francis
Sent: Wednesday, March 16, 2016 5:00 PM
To: Melland, Brandon
Subject: FW: Lindsey Hill development

From: Marianne Moore [mailto:marimoore2001@yahoo.com]
Sent: Wednesday, March 16, 2016 4:56 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: Fw: Lindsey Hill development

[Sent from Yahoo Mail on Android](#)

On Wed, Mar 16, 2016 at 4:54 PM, Marianne Moore
<marimoore2001@yahoo.com> wrote:

Dear Frances Serna,

Please pass my comments along to the powers that be.

I write concerning the Lindsey Hill development. Just what we need: another multi-story (5-story? seriously?) apartment building in the middle of an established neighborhood. And an historic neighborhood at that. Three things scream out to me.

1. Must every sector of San Marcos be home to multi-level apartments which house students? And who else would be living in one-bedroom apartments three blocks from campus? Apartments and student housing is swallowing up our town.
2. Traffic is already gridlocked most times of day at Hutchinson, Moore and Hopkins Streets. Just how are hundreds more cars supposed to navigate these intersections?
3. Our historic districts are the character of our town. They should be OFF LIMITS to any large construction projects like this.

Please rethink this project which will be the beginning of the end of the charm of San Marcos.

Sincerely,
Marianne Moore
2930 Summit Ridge Drive
San Marcos TX 78666
512-665-6048

[Sent from Yahoo Mail on Android](#)

Melland, Brandon

From: Serna, Francis
Sent: Wednesday, April 06, 2016 8:26 AM
To: Melland, Brandon
Subject: FW: Lindsey Hill comments

Importance: High

From: R. Neill Hadder [mailto:neill.hadder@gmail.com]
Sent: Wednesday, April 06, 2016 7:29 AM
To: Planning_Info <planning_info@sanmarcostx.gov>
Subject: Lindsey Hill

My property at 615 W. Hutchison St. stands 44 paces from the corner of Blanco and W. Hutchison St, making me one of the home owners most directly affected by redevelopment of Lamar School.

George Orwell's novel 1984 gave us the concept of "Doublethink," a system of thought control in which "war is peace" and "ignorance is strength." Now we can add the following:

"Boutique," as in "boutique hotel" – largest commercial structure anywhere in central San Marcos, violating the height restrictions agreed upon by the community in the Comprehensive Plan.

"Integrate with the community" – tower with LED lights on said hotel beaming into my bedroom, 17,000sqft of retail, parking pushed into the already-stressed surrounding neighborhood, and 4-5 story apartment building looming over single-family homes.

"Poet's corner" – insufficient substitute for the 20% landscaping required by the land use code, and no assessment of water runoff effects of the increase in impermeable cover (my yard consistently receives torrents of flood water running down W. Hutchison St. each time it rains, and the sidewalk in front of Lamar School is buried by several inches of mud each time).

"Jewel box" – Attempting to make a fortune off a supposedly naive City council and P&Z commission by imposing Northeastern-style dense urbanism that is completely alien, undesired, and unnecessary in the slated "buffer" region between the historic district and downtown. They weren't aware that this town had learned better.

The Lindsey Hill concept initially sounded promising to many of us. A true "boutique" hotel of 30-40 rooms, along with living areas suitable for retirement down-sizing and designed to discourage a "critical mass" of student occupancy, along with a restaurant, would be perhaps the best use for these 5 acres. Although faculty and other professionals have no desire to live in one-bedroom apartments (I graduated from college for a reason), an 800 sqft apartment provides space for an 8'x10' home office or guest room (with no dedicated closet) and could be attractive to professionals while posing an awkward space for students. Any town homes built on the property would need to be restricted to older residents. Otherwise, there is no way to stop them from being purchased and rented out by realtors; and, Kissing Tree makes this type of development less useful to the City than something that would serve people aged 30-50. If the developers are not interested in scaling down the hotel and apartment complex to meet the requirements of the Comprehensive Plan and to fit the human scale of our community, however, then they turn out to be not at all the kind of developers for which we have been waiting.

Neill Hadder

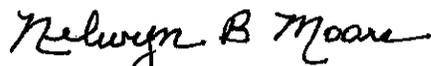
My name is Nelwyn Moore and I have lived in the historic district on Belvin Street since 1964 when my husband and I bought the home where I still reside. We raised our two children here and they attended Lamar when it was used as a middle school for the SMCISD. Those of us who live in close proximity to Lamar have seen many changes in its' use over the years. When I first arrived in San Marcos in 1947 as a 17 year old freshman attending Southwest Texas Teachers College the Lamar building was being used by the San Marcos School District to educate students of all ages. All of those buildings which were related to the property's original use, the Coronal Institute, were destroyed in 1949 so a new high school could be built. The building currently standing on the Lamar property is that high school.

The recounting of that small amount of history is to say I realize change is a necessary part of growth and one which often means we are a thriving community. I do understand the need to repurpose the structures and land on the Lamar property and I look forward to some of the new uses proposed by the developers. However, I do worry that such an extensive list of functions (hotel, apartments, parking garage, convention facilities, park land) will not serve the property or the surrounding neighborhoods well. The abundance of good ideas is a great start to transforming Lamar into a viable, thriving part of our community but reality calls for us to engage in thorough discussion and consideration of the facts at hand. In my opinion, it will be this type of discussion that will provide us with the most direct route, and perhaps the only route, to sound decisions.

I look forward to seeing the Lamar property repurposed and used wisely and well for all of San Marcos. It is, after all, our duty as the current generation to make the best, most measured decisions of which we are capable.

At this time I believe we should pause the development process of the Lamar property, not stop it or negate it, and continue our inclusive conversation with the developers, the surrounding neighborhoods and the city. This is our best chance of getting it right.

Sincerely,

A handwritten signature in cursive script that reads "Nelwyn B. Moore". The signature is written in black ink and is positioned above the printed name.

Nelwyn B. Moore

Melland, Brandon

From: Serna, Francis
Sent: Friday, March 18, 2016 12:25 PM
To: Melland, Brandon
Subject: FW: Lindsey Hill

From: Robert Bermea [mailto:rbermea@gmail.com]
Sent: Friday, March 18, 2016 12:06 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: Lindsey Hill

To the zoning and planning commission:

As a resident in the neighborhood of the Lindsey Hill Project, I would like for more time to be allowed to fully understand the nature and scope of the project before being acting upon by the Zoning and Planning Commission and being sent forward to the City Council.

During the Feb 29th presentation, the neighborhood residents were presented with 'concepts' of the project but few details.

I feel the following issues need to be fully addressed and understood by both the project developer and neighborhood residents.

Naturally many concerns of the neighborhood residents were raised such as:

- Parking for development residents, hotel and business customers. Personally I don't think there will be enough and we will have "Lindsey Hill" patrons parking through out our neighborhood.
- Additional traffic that will be generated through the neighborhood. There's enough congestion on Blanco and Hutchinson right now as it is. I think 'how traffic will be addressed' needs to be part of the proposal being put forward.
- Feasibility of have the majority of the apartments 1 bedrooms.
- Nature of the business that will go into commercial aspects of the project. We don't need establishments that serve alcohol in our neighborhood.
- Fitting in with the nature of the historical district adjacent to the project.

A concern that was not addressed at the meeting but that I have is that the height of the project 4 - 5 stories would be so out of character with the neighborhood that it would 'stand out from the rest of the structures in the area.

Thank you for allowing me present my concerns.

Robert Bermea
712 Maury
San Marcos

Melland, Brandon

Subject: FW: lindsey hills project

From: tina simek [mailto:tina_simek@yahoo.com]

Sent: Wednesday, March 16, 2016 2:53 PM

To: Serna, Francis <FSerna@sanmarcostx.gov>

Cc: Cathy Dillon <cri@grandecom.net>

Subject: lindsey hills project

This is my letter for input on the traffic issues this project will create.

Sent from my iPhone

Begin forwarded message:

From: tina simek <tina_simek@yahoo.com>

Date: March 5, 2016 at 7:40:54 AM CST

To: Daniel Guerrero <daniel@guerrero.com>, "jprather@sanmarcostx.gov"

<jprather@sanmarcostx.gov>, "jcitycouncil@gmail.com"

<jcitycouncil@gmail.com>, "lprewitt@sanmarcostx.gov"

<lprewitt@sanmarcostx.gov>, "jhughson@sanmarcostx.gov"

<jhughson@sanmarcostx.gov>, "sgregson@sanmarcostx.gov"

<sgregson@sanmarcostx.gov>, "mderrick@sanmarcostx.gov"

<mderrick@sanmarcostx.gov>

Subject: lindsey project

Reply-To: tina simek <tina_simek@yahoo.com>

Hello City Council and Mayor Gurerro,

I am Tina Simek, a resident of the historic district on Scott and Burleson Street. I am concerned, as well as all the neighborhood attendees at the developer's meeting in the Lamar Annex on 2/29, about the traffic this project will create. I think and would like your help, to consider keeping all entrances, exits, deliveries and all traffic services off of Burleson Street. This could help keep some of the traffic from cutting through our residential neighborhoods. Also, I can't help but imagine the nightmare for OUR city this current plan will create with their only access from Burleson and how this will work with the intersection on Moore and Burleson and the current mess Moore Street has already? Lindsey project has three good commercial streets to use, please keep them off of Burleson. Other than that I am pleased with the developer's intent and ideas, and am glad this project will hopefully be a big step up from The Vistas or The Woods.

Sincerely, Tina Simek

p.s. I will be at the meeting on March 15.

Melland, Brandon

From: Serna, Francis
Sent: Monday, March 28, 2016 8:42 AM
To: Melland, Brandon
Subject: FW: Letter to Daily Record

From: William Agnew [mailto:wagnew@austin.rr.com]
Sent: Sunday, March 27, 2016 4:09 PM
To: Serna, Francis <FSerna@sanmarcostx.gov>
Subject: Letter to Daily Record

The letter below ran in the Daily Record on Sunday and I was advised to send a copy to you. For whatever consideration it may merit by P&Z, here it is.

Sent from [Mail](#) for Windows 10

Among all of the verbiage on the Record's opinion page concerning the proposed Lindsey Hill project, two items seem particularly notable. One is the reference in Wayne Kraemer's letter to the "area of stability" status of the block in which this project would be located. This block sits in the historic district which presently consists of mostly single family residences (some of them historic homes; many of them beautifully restored), some relatively small apartment buildings in unobtrusive locations, and four scattered, very small commercial buildings. It doesn't take a genius to see that Lindsey Hill would be like King Kong among its neighbors, both in terms of its size and its capacity to be destructive to the neighborhood. The second item was the owner of the Crystal River Inn's mention of that establishment's travails at the hands of Zelick's. While not a perfect analogy, the Crystal River Inn/Zelick's saga is instructive in one sense. In that case the interests of out of town opportunists have prevailed over those of an established San Marcos business, one which had worked hard to make itself a part of the San Marcos scene. The Lindsey Hill situation exhibits this same tension – out of town interests versus the residents of a neighborhood with a very well defined culture and ambience. I have been in San Marcos since 1992 and too often I have seen the city accommodate out of town money in this type of situation. I believe the first obligation of the city's various commissions and City Council should be to protect that which is already here. The siren song of "growth" should not be entertained at the expense of existing interests. In the medical profession they have a principle which seems relevant in the case of the Lindsey Hill and similar proposals. "First, do no harm."

William A Agnew
716 Belvin St.
San Marcos, Tx 78666
(512) 353-3189

Great points pro-project (because they're going to develop SOMETHING):

We know our city is growing and we hope to work with you on this project.

1. Our community is thrilled that you are going to build apartments that are not (installment loan) by-the-bed rentals.

Concerns & Suggestions:

Our community has a few concerns and suggestions if we are to work together on this project. Unfortunately, the developers building this industrial multi-use project in our small, quant neighborhood comes with a high cost.

1. Our small community's **infrastructure and roads are built for neighborhood use, NOT industrial use.**

Lindsey Hill will be bringing in hundreds of people who will use these resources.

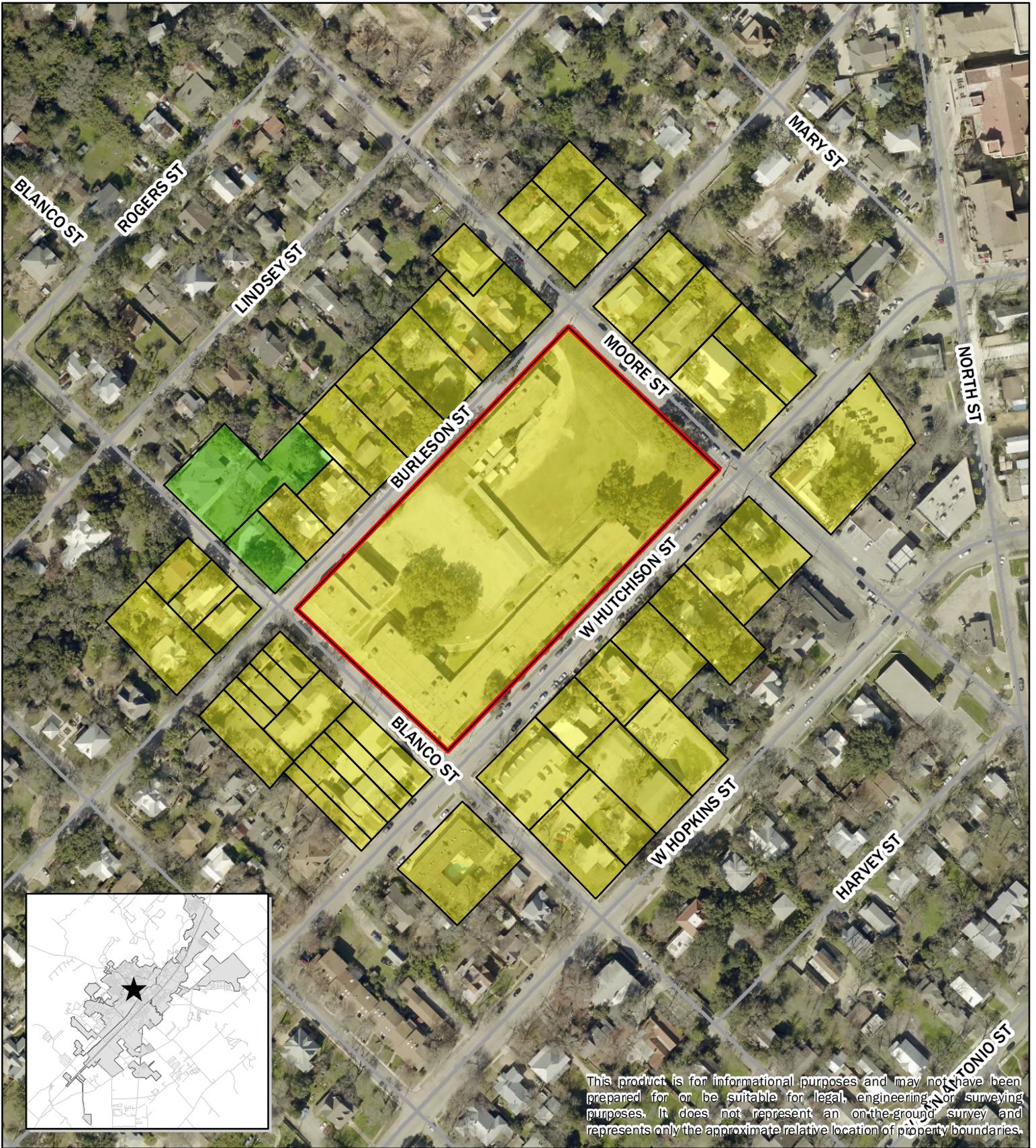
*You should pay an **IMPACT FEE** as part of your permit fee before construction begins.* This will put the cost on you, the people who will gain financially from this project.

Otherwise, the **tax payers will have to shoulder the burden and cost** of putting in traffic lights, updating old pipes, sewage and electrical systems, putting up speed bumps to "slow" traffic, and reconfiguring certain roads to go one-way

2. The multi-use project is too much.
3. **Build more apartments, but leave out the hotel.** Put in shops but not office buildings. There has to be a happy medium.
4. Lindsey Hill will **lower our neighborhood property values.** It will no longer be a historic small-town district.
5. Your **hotel will cause our small historic neighborhood to be glutted** during University events... **student orientation, commencement, graduation.**
6. You claim that having the hotel and shops in your project will bring business TO our downtown area and will give San Marcos' local, downtown shops more business. In reality, they will be taking customers AWAY from our local, true boutique, small businesses.

Kana D. D. C.

7. You haven't addressed what you will do to protect the patients and facilities for our special needs' citizens whose homes surround two sides of this project.
8. We've discussed parking and traffic ad nauseum. Remember this will affect us all during construction even more.
9. Hopkins is already like a speeding highway. Now similar traffic will be diverted to Belvin, San Antonio, Hutchison, Moore, Burleson and other "side streets".
10. Hopkins is undergoing "reconstruction". It will not primarily curb traffic, but will provide larger side-walks. Your project will defeat this purpose.
11. Your studies on parking and traffic give results indicating that the flow will not be largely impacted by the Lindsey Hill Project. If our developers from Houston and New Jersey want to stay here for a month, you will find that the reality of traffic and parking are very different from the study results!
12. Having a large hotel (smaller ONLY to Embassy Suites), shops, offices, and apartments smack dab in the middle of a beautiful historic district family NEIGHBORHOOD will diminish the quality of life we have.



PDD-15-02
Lindsey Hill Planned Development District
500 W Hutchison St
Map Date: 4/7/2016





City of San Marcos

630 East Hopkins
San Marcos, TX 78666

Legislation Text

File #: ID#16-182, **Version:** 1

AGENDA CAPTION:

Hold a public hearing, staff presentation and discussion on the 10-Year Capital Improvements Program (FY 2017-2026)

Meeting date: April 12, 2016

Department: Engineering/CIP

Funds Required:

Account Number:

Funds Available:

Account Name:

CITY COUNCIL GOAL:

COMPREHENSIVE PLAN ELEMENT(s):

BACKGROUND:

FY 2017-2026 10 Year CIP Project List

Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
*Possible Bond Projects															
Multi	507	Airport KDA & JDA Utilities & Drainage	Extension of utilities and drainage improvements along Taxiways Kilo & Juliet to provide for development sites.	Wastewater					\$ 140,000		\$ 1,400,000				
				Water					\$ 140,000		\$ 1,400,000				
				Drainage					\$ 140,000		\$ 1,400,000				
Multi	524	Airport Taxiway System, Ramp Rehab Design and Drainage Master Plan	PCI analysis in 2013 indicates failure of pavement on various taxiways and ramps. TxDOT recommends an analysis and rehab based upon its results. Drainage will also be a component to insure pavement life. First phase of the project will be for the study and design. Second phase of the project will be for construction over multiple years. This amount reflects the City's 10% project match.	Drainage			\$ 15,000								
				General			\$ 50,000								
Multi	183	Bishop Street Improvements	Drainage, Water (per master plan) and Wastewater improvements along Belvin & S. Bishop to improve drainage issues downstream along San Antonio Street. Includes a sidewalk on Bishop between Belvin and Hopkins. The needs on this project will be determined with the Hopkins Street Imp PER. Design funds in 2019, construction in 2021.	Water				\$ 20,000		\$ 80,000					
				Wastewater				\$ 40,000		\$ 160,000					
				General				\$ 200,000		\$ 800,000					
				Drainage				\$ 220,000		\$ 980,000					
Multi	587	Bishop Street Sidewalk Connection	Construct a 5' wide sidewalk along one side of Bishop from Prospect to Belvin. Install a bus stop and bus shelter at Belvin. The PER will determine the costs of widening the road for the addition of dedicated bike lane going uphill from Belvin to Prospect. Power pole relocation will be required. General funds may be offset by any available fee in lieu funds.	General	\$ 250,000	\$ 200,000									
				Drainage	\$ 25,000	\$ 75,000									
				Water		\$ 15,000									
				Wastewater		\$ 10,000									
				Electric		\$ 100,000									
Multi	325	Briarwood Water Improvements	Replace undersized mains approx. 1500 LF. Will include drainage improvements along the railroad track. Design in 2019, Construction in 2020.	Water				\$ 50,000	\$ 300,000						
				Drainage				\$ 50,000	\$ 200,000						
Multi	27	Cheatham Street and Blanco River WL Bore Imps	Construct 1,400 ft. of 12" of water line from CM Allen to Mill Race along Cheatham. Connects to previous improvement at Mill Race. Add 70' of sidewalk on South Side of Cheatham St from end of existing sidewalk on end of west side of Riverside. Add sidewalk on Downstream side of Cheatham St. to connect Mill Race and San Marcos River bridge sidewalks. Install 2000' of 12" Waterline on Downstream side of Blanco River at IH-35 due to damage caused by "All Saints Day" Flood. Previous funding of \$100k is for design. 2017 funds \$1.7M for construction and \$150k to reimburse for borrowed	General		\$ 200,000									
				Water	\$ 100,000	\$ 1,650,000									
Multi	199	Chestnut Street Improvements	Construct new sidewalks along Chestnut St. from Holland to Acorn; including a portion of underground electric, due to the narrow right-of-way. Replace wastewater from Ridgeway to N.LBJ and the water from Holland to N.LBJ. Mill and overlay from Holland to N.LBJ.	Wastewater		\$ 110,000		\$ 360,000							
				Electric		\$ 120,000		\$ 400,000							
				Water		\$ 150,000		\$ 500,000							
				General		\$ 375,000		\$ 1,230,000							
				Drainage		\$ 15,000		\$ 50,000							
Multi	597	Coers Drive Improvements	In 2016, City Council approved \$500K in drainage funds with the Old RR 12 project to assist with drainage for areas impacted by this project. A PER was completed this year (2015) which identified Coers Drive as an area impacted by the Old RR 12 project. The Coers Drive Improvements will be constructed in advance of the Old RR 12 project and will use \$200k in drainage funds from Old RR12. Coers Drive Improvements will regrade the ditch along Coers from Owens towards Old RR12. Replace 2 drainage crossings on Coers with larger pipe. Enclose culvert at Owens and Coers; headed North. Replace water and sewer within project limits due to anticipated conflicts. Add Sidewalk from Old RR12 to Owens. This project does not include pavement reconstruction. High Priority.	General		\$ 100,000									
				Water		\$ 275,000									
				Wastewater		\$ 300,000									
				Drainage	\$ 200,000	\$ 150,000									
				Electric		\$ 20,000									
Multi	453	Columbia Improvements	Improvements to existing wastewater line on Columbia at Hazelton to correct existing sag. Replacement likely needed from North of Hazelton to Prospect, 1,900lf. 4x4 box culvert crossing at Hazelton also needs to be replaced.	Drainage	\$ 25,000		\$ 100,000								
				Wastewater	\$ 125,000		\$ 725,000								
Multi	31	Comprehensive Plan	Implement items in Vision San Marcos; 2016 funds will be used toward the Land Development Code (Code SMTX) rewrite.	Electric	\$ 83,000	\$ 33,000		\$ 100,000							
				General	\$ 83,000	\$ 33,000		\$ 100,000							
				Water	\$ 83,000	\$ 33,000		\$ 100,000							
Multi	39	Disaster Recovery Infrastructure (every 5 yrs.)	Upgrade recovery system due to age	General		\$ 166,666				\$ 166,666					
				Wastewater		\$ 166,666				\$ 166,666					
				Electric		\$ 166,666				\$ 166,666					
Multi	609	Downtown Master Plan Update	Update of the Downtown Master Plan adopted in 2008 including an implementation plan, support of CodeSMTX strategies, parking management & implementation & underground utility plan/implementation.	General			\$ 100,000								
				Electric			\$ 100,000								
				Water			\$ 100,000								
Multi	45	Downtown Reconstruction Ph. II	Reconstruction to implement goals of Downtown Master Plan. Includes street, drainage, water, wastewater, underground electric and telecom as required. Design funds in 2019, Construction funds in 2021. Estimated costs for approximately 6 blocks. Final location will be established with Council direction.	Water				\$ 100,000		\$ 1,000,000					
				Wastewater						\$ 1,000,000					
				Drainage				\$ 300,000		\$ 3,000,000					
				Electric				\$ 300,000		\$ 3,000,000					
				General				\$ 500,000		\$ 5,000,000					

FY 2017-2026 10 Year CIP Project List

Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026		
Multi	582	E. Aquarena Springs Drive Reconstruction	This is a safety project to reconstruct E. Aquarena Springs Drive at the curve to super elevate the road to decrease accidents in this area. The road is currently super elevated in the wrong direction. This project will also address drainage issues in the area. This project is currently being looked at for possible design by a City Engineer. If it is determined that City staff can make these improvements, then the 2019 funds will be used for materials and the 2021 funds will be removed.	General				\$ 100,000		\$ 500,000							
				Drainage				\$ 50,000		\$ 200,000							
Multi	284	Engineering -CIP Department Funding	Funding for Engineering and Capital Improvements Department Staff. City is in the last years of transitioning funding from debt to operating.	Drainage	\$ 58,618	\$ 26,847											
				General	\$ 146,545	\$ 67,117											
				Wastewater	\$ 381,018	\$ 67,117											
Multi	69	Fiber Optic Infrastructure Expansions/Maintenance	Purchase the required fiber optic equipment to expand fiber ring; funding provided by General, Wastewater, and Electric Funds. Locations for expansion include the Airport, Gary Sports Complex, Surface Water Treatment Plant, Electric Substations, and Nature Center & Convention Visitor Bureau	Electric	\$ 240,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 34,000			
				General	\$ 240,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 34,000		
				Wastewater	\$ 240,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 80,000	\$ 34,000		
Multi	611	Franklin Water Tank/Monopole Antenna	Removal of the Franklin water tank and replacement of a monopole antenna. Cost estimate for the removal of the tank is around \$400k due to the location. Antenna cost is estimated at \$500k. Removal of tank will allow additional area within cemetery. Currently the City's public safety radio system is located on the Tower.	General		\$ 500,000											
				Water		\$ 400,000											
Multi	90	Hopkins Street Improvements Project (Bishop to Moore)	Reconstruct street, water, wastewater, drainage and electric infrastructure pursuant with Complete Streets ordinance, as well as replace the waterline on Belvin from Scott to Johnson (approx. 2,500lf) to strengthen the supply in the area; TxDOT will participate. Electric funding amount is for pole replacement only. \$3,000,000 of the current funding is from CAMPO funds.	Electric	\$ 400,000		\$ 2,500,000										
				Water			\$ 1,500,000										
				Wastewater			\$ 1,250,000										
				Drainage	\$ 2,500,000		\$ 1,000,000										
				General	\$ 3,000,000												
Multi	594	Hopkins Street Improvements Project from Moore to Guadalupe	Reconstruct street, water, wastewater, drainage and electric infrastructure pursuant with complete streets ordinance on Hopkins Street from Moore St. to Guadalupe. Project will coordinate with future project #598 to widen Comanche @ Hopkins	General					\$ 300,000		\$ 3,000,000						
				Water					\$ 130,000		\$ 1,300,000						
				Wastewater					\$ 130,000		\$ 1,300,000						
				Drainage					\$ 100,000		\$ 1,000,000						
				Electric					\$ 200,000		\$ 2,000,000						
Multi	588	Hutchison Street Parking Lot	Provide 12 downtown parking spaces and community seating area. Possible location for a transportation hub. Utilities will be needed if such desired hub moves forward.	General	\$ 85,000	\$ 75,000											
				Water		\$ 20,000											
				Wastewater		\$ 30,000											
				Electric		\$ 25,000											
Multi	96	Hwy 123 12" Water AC Line Replacement	Replace existing AC water line along Hwy 123 between IH35 to just past DeZevala 5,400 feet. Design funds in 2018, construction funds in 2019. Will coordinate with proposed TxDOT IH-35 & SH 123 intersection improvements to start design in 2016 with construction in 2018.	Water			\$ 250,000	\$ 2,500,000									
				Electric			\$ 25,000	\$ 75,000									
Multi	600	Kingswood and Sherwood Imps	Use trenchless technology to rehabilitate existing wastewater lines along Kingswood and Sherwood. Approx 1800 LF total. Reconstruction of streets and drainage. The existing water line is needed due to the line being undersized and does not provide fire coverage. Electric is currently looking at the area to determine if work needs to be done. Design begins in 2019 with construction in 2021.	General				\$ 70,000		\$ 700,000							
				Water				\$ 20,000		\$ 200,000							
				Wastewater				\$ 20,000		\$ 200,000							
				Drainage				\$ 30,000		\$ 300,000							
Multi	105	Leah Drive	Construct 12" water line in extension of Leah from Cottonwood Pkwy to Civic Center Loop, approx. 700 LF. Includes 50% participation in the estimated cost for road and culvert construction with the development of the City property.	Water		\$ 50,000	\$ 250,000										
				Drainage		\$ 40,000	\$ 360,000										
				General		\$ 20,000	\$ 200,000										
Multi	563	Linda Drive Improvements	Replace or rehabilitate the 18" sewer main that runs from LS#1 to the McCoy's headquarters. Total length of pipe is approximately 7,000'. The current pipe has a decomposing inner liner, which if left unattended could leave us open to a catastrophic failure under Hwy 80 or any other point along the route of the pipe line. This is a Gifford Hill concrete cylinder pipe that was initially installed and used as a force main to transport waste to the old trickling filter treatment facility located on Davis Lane. Water and street improvements to be included	Wastewater				\$ 200,000		\$ 2,000,000							
				Water				\$ 70,000		\$ 700,000							
				General				\$ 150,000		\$ 1,500,000							
Multi	559	Long Street Realignment	Realign Long Street at SH-80 with Cheatham and the traffic signal. This will provide a 4-way intersection at the current signalized intersection. Assumes donation of ROW by Wendy's. PER underway this year.	General		\$ 250,000	\$ 550,000										
				Drainage		\$ 50,000	\$ 200,000										
				Electric		\$ 25,000	\$ 75,000										

FY 2017-2026 10 Year CIP Project List

Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026		
Multi	111	Main Lift Station Force Main & Reclaimed Water Imps	Construct additional force main (approx. 4000 LF) and replace existing force main from main lift station to wastewater treatment plant. Project will also include a new bulk reclaimed water station at the plant entrance and the extension at the reclaimed waterline to Cape road to serve the park and development. \$1.3M was approved in 2014 CIP but was never funded. \$200k was approved in 2015 CIP. \$1.1M was approved in 2016 CIP	Water	\$ 900,000												
				Wastewater	\$ 400,000	\$ 2,700,000											
Multi	509	Mockingbird Hills Subdivision Imps	Total Street reconstruction and positive drainage on Cloverleaf, Mockingbird, Parkview, Lacey and Suncrest. Improve existing asbestos-cement aged water lines; approx. 3,200 ft. in all streets except Parkview.	Water					\$ 200,000		\$ 900,000						
				General					\$ 700,000		\$ 5,100,000						
Multi	132	Municipal Services Complex Expansion/Relocation	Relocate and consolidate city facilities; With the FY15 first year funds of \$150k, conduct an assessment to expand City Facilities at the current City Hall location, relocate all Public Services (SMEU, PS, Fleet, Parks) to a new combined location, and make necessary repairs to the current fleet maintenance area. The second year of funds approved in FY 16, \$1.8M - will begin the concept and design phase for the improvements. The third year of funds in 2017 \$4M - will provide funding for land. The final year of funding in 2018, \$12M - will be for construction.	Electric	\$ 600,000	\$ 1,300,000	\$ 4,000,000										
				General	\$ 750,000	\$ 1,300,000	\$ 4,000,000										
				Wastewater	\$ 300,000	\$ 700,000	\$ 2,000,000										
				Water	\$ 300,000	\$ 700,000	\$ 2,000,000										
Multi	136	Network Infrastructure	Replacement of network infrastructure equipment; funding provided by General, Water, and Electric Funds	General		\$ 150,000											
				Water		\$ 150,000											
				Electric		\$ 150,000											
Multi	464	Old Ranch Road 12 Bike & Ped & Widening Project	Addition of bike and pedestrian facilities, as well as a center turn lane to improve mobility and increase safety along Old RR 12 between Craddock and Holland. Replacement of utilities and drainage improvements at problem locations within project limits will also be addressed. \$200K drainage funds will be moved to Coers Drive #597. The remaining \$300K will be used to make improvements at Sara Street and to improve the drainage scope on Old RR12. Previously approved funding is from FY2014 and FY2016 funds. High Priority.	Electric			\$ 350,000										
				Drainage	\$ 700,000		\$ 500,000										
				Wastewater	\$ 300,000		\$ 450,000										
				Water	\$ 800,000		\$ 900,000										
				General	\$ 2,420,000		\$ 1,800,000										
Multi	593	Old RR12/Moore Street Reconstruction	Reconstruct roadway section from Hopkins to Blanco Street with new curb, sidewalk, drainage, traffic signal and intersection upgrades, retaining walls and roadway surface. Project will reduce localized flooding, improve pedestrian facilities and traffic movements.	General							\$ 250,000		\$ 2,200,000				
				Drainage							\$ 250,000		\$ 1,100,000				
				Electric							\$ 200,000		\$ 2,000,000				
Multi	372	Pat Garrison Improvements from Comanche to Guadalupe	New waterline from Guadalupe to the dead-end past Comanche, include road and sidewalk improvements in this area and the possible realignment of Pat Garrison at Fredericksburg. Continue the WW improvements from Vistas 2, upsize the 6" WW line to 8" WW line in the alley and continue down Pat Garrison from the dead end to Comanche. May include electrical improvements from overhead to underground. Currently in preliminary design with \$300k in prior funding.	Electric		\$ 50,000	\$ 500,000										
				Water	\$ 250,000	\$ 15,000	\$ 150,000										
				General		\$ 50,000	\$ 1,000,000										
				Wastewater		\$ 10,000	\$ 100,000										
Multi	596	Ramsay St. Reconstruction	Ramsay St. from N. LBJ to the dead end is in need of total reconstruction with new curb and gutter and sidewalks. Due to unstable soils it is going to need soil stabilization. Ramsay St. holds a score of around 10-15 for a PCI Index. 1,050 LF of water line will be replaced. Wastewater will be replaced if in conflict. Drainage will be needed.	General					\$ 150,000		\$ 700,000						
				Drainage						\$ 50,000		\$ 400,000					
				Wastewater						\$ 25,000		\$ 75,000					
				Water						\$ 50,000		\$ 200,000					
Multi	482	Reclaimed Water System Expansion Ph. 1 (formerly Water Reuse System)	Expansion of the reclaimed water system. Phase I - 12,400 linear feet of 16-inch diameter pipeline between current reclaimed main and CM Allen Parkway. Design will also cover improvements to pump station and reservoir design to increase capacity. TX State will participate in costs. WW and Reclaimed Water on Durango South to De Zavala. Reclaimed Water and Water on Staples from De Zavala to Durango will be replaced using \$375k of Wallace Addition Funds. 1 of the 16" Waterline crossings from project #99 - IH-35 Waterline Crossings will be replaced at McKie St. A bore from #544 Eastside Interceptor project with a 24" WW line will be complete and water imp from #508 McKie and Lee Water Project, from IH35 to Love will be completed. Also includes a bore with 3 - 6" PVC pipe and 6- 4" PVC pipe for electric and telecom. Funding through 2027 for additional phases.	Water	\$ 375,000	\$ 1,925,000		\$ 200,000	\$ 2,300,000						\$ 500,000		
				Wastewater		\$ 700,000											
				Electric		\$ 500,000											
Multi	173	River Ridge Extension to Post Road	Extend roadway to include bicycle lanes and sidewalks to provide accessibility to IH35, approx. 3,100 lf with railroad overpass. Possible bond project.	Drainage					\$ 200,000		\$ 800,000						
				General					\$ 3,200,000		\$ 6,300,000						

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Multi	419	Sessom/Academy Intersection Improvements	The intersection improvements at Sessom and Academy will include physical characteristics such as geometry as well as traffic operations and pedestrian safety. Various geometric improvement options will be developed as part of the PER and preferred option will be designed and constructed to improve traffic operations and pedestrian safety. Drainage improvements along Sessom Drive from Yale to Comanche to abandon a storm drain through private property will be included. Improvements and cost participation will be coordinated with development and Tx State and with all Drainage Master Plan projects.	General		\$ 100,000		\$ 1,000,000							
				Drainage	\$ 100,000			\$ 500,000							
Multi	272	Sessom Drive Bike/Ped and Peques Improvements	Improve bike and pedestrian access along Sessom Dr from Aquarena Springs Dr to North LBJ and realign the State/Peques Intersection. Campo funds and construction in 2017. Additional funding for design and sidewalk widening associated with the SMEU underground conversion on this project. High Priority.	Electric	\$ 80,000	\$ 500,000									
				General	\$ 700,000	\$ 200,000									
				Drainage	\$ 650,000										
Multi	521	Sessom Improvements from N LBJ to Comanche	Install 3,000 lf of 12" wastewater mostly by bore along Sessom between N LBJ and Loquat. In addition; bore sewer along Peachtree and Canyon Rd. Design in first funding year; acquisition in second funding year; construction in third funding year. To be constructed with 10' Multi-use path within the same limits and Sessom Creek Remediation Sites 9 & 10 as identified by RPS. Water quality assessment and analysis, stream erosion assessment and recommendations for remediation from the outfall of Sessom Creek into the San Marcos River to approximately Peachtree Street. Pre-funding this project with \$125k from Sessom Drive Bike/Ped and Peques Imps #272, will be refunded back with 2017 funds. Wastewater is High Priority.	Wastewater		\$ 450,000	\$ -		\$ 2,200,000						
				General		\$ 150,000	\$ 350,000		\$ 720,000						
				Drainage		\$ 30,000			\$ 200,000						
Multi	36	Uhland Road Improvements	Alleviate drainage problems along County and Uhland Road by installing a storm sewer system and upgrading water and wastewater mains. Install 1800lf 12" waterline and 1000lf of wastewater line along Uhland road from IH 35 to the 1300 block of Uhland.	Wastewater					\$ 30,000		\$ 150,000				
				Water					\$ 32,000		\$ 280,000				
				General					\$ 150,000		\$ 1,000,000				
				Drainage					\$ 150,000		\$ 850,000				
Multi	234	Victory Gardens Neighborhood Improvements Ph. II - South Section	Reconstruct deteriorating streets, drainage, electric, water and wastewater infrastructure	Water					\$ 200,000		\$ 1,500,000				
				General					\$ 400,000		\$ 3,000,000				
				Wastewater					\$ 200,000		\$ 1,500,000				
				Electric					\$ 200,000		\$ 1,500,000				
				Drainage					\$ 300,000		\$ 2,000,000				
Multi	281	Victory Gardens Subdivision Ph. I - North Section	The project will replace old streets, drainage system, water and wastewater lines in the north section of the Victory Gardens Subdivision to improve reliability of service. Wider sidewalks are proposed along Patton and Roosevelt as well as 5' sidewalks throughout the subdivision to improve overall pedestrian connectivity. Overhead electric and telecommunication lines will be converted to underground along Patton and Roosevelt and decorative lighting will also be installed along these streets. A photometric study will dictate where new lighting will be installed throughout the subdivision as well. Pedestrian crosswalks and electronic crossing indicator poles are also proposed at the intersection of Guadalupe and Roosevelt. Construction to begin in 2017. High Priority	General	\$ 2,000,000										
				Water	\$ 1,900,000	\$ 600,000									
				Wastewater	\$ 2,600,000	\$ 400,000									
				Drainage	\$ 500,000	\$ 2,000,000									
				Electric	\$ 1,500,000										
General Fund Subtotal						\$ 4,016,783	\$ 8,130,000	\$ 3,280,000	\$ 5,850,000	\$ 7,246,666	\$20,930,000	\$ 80,000	\$ 2,280,000	\$ 34,000	\$ -
General Fund Subtotal without potential Bond Projects						\$ 2,716,783	\$ 4,130,000	\$ 2,780,000	\$ 2,250,000	\$ 2,246,666	\$11,630,000	\$ 80,000	\$ 2,280,000	\$ 34,000	\$ -
Water Fund Subtotal						\$ 5,983,000	\$ 5,150,000	\$ 3,490,000	\$ 3,422,000	\$ 1,280,000	\$ 6,280,000	\$ -	\$ -	\$ -	\$ 500,000
Wastewater Fund Subtotal						\$ 5,723,783	\$ 4,605,000	\$ 600,000	\$ 3,005,000	\$ 1,606,666	\$ 6,505,000	\$ 80,000	\$ 80,000	\$ 34,000	\$ -
Drainage Fund Subtotal						\$ 2,386,847	\$ 2,175,000	\$ 1,200,000	\$ 1,490,000	\$ 4,480,000	\$ 8,200,000	\$ -	\$ 1,100,000	\$ -	\$ -
Electric Fund Subtotal						\$ 3,069,666	\$ 7,605,000	\$ 880,000	\$ 480,000	\$ 3,246,666	\$ 3,780,000	\$ 80,000	\$ 2,080,000	\$ 34,000	\$ -
Airport	491	Airport Landscaping	Improve landscaping around Airport facility. Will implement new standards developed by airport management for future private projects. This project is not eligible for a TxDOT match.	General		\$ 80,000									
Airport	513	Airport - Relocation of Electric Vault	Relocate electric vault out of prime development area with access to ramp and utility infrastructure	General					\$ 40,000						

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Airport	605	Airport - Relocation of WWII Tower	Rehabilitation and possible relocation of the old WWII Tower as entryway feature. The airport has one of the few remaining WWII towers. Currently the tower is not safe to access the viewing platform and needs to be assessed for possible lead paint removal/asbestos mitigation and needed repairs. Design in 2018. Construction in 2019.	General				\$ 14,000	\$ 70,000						
Airport	606	Airport - FM 110 Connection Drive	New Roadway to connect the airport to FM 110. Design funds in 2018, Construction funds in 2020. TxDOT has a 50/50 match on new roadways, with a cap of \$100k	General			\$ 100,000		\$ 1,000,000						
Airport	159	Airport Routine Maintenance Grant Match	50/50 match with TxDOT for routine airport maintenance; move into operating cost in 2019	General	\$ 50,000	\$ 50,000	\$ 50,000								
Airport	518	Airport RSDA Hangars	Phase 2 of City owned T-hangars	General							\$ 60,000				
Airport	520	Airport Runway 17-35 Extension	Extension of Runway 17-35 to over 6000 ft. in order to accommodate larger aircraft at the airport	General								\$ 510,000			
Airport	607	Airport Wildlife Hazard Assessment	City 10% match for assessment of airport for wildlife hazards. TxDOT has already received the federal grant funds for this project.	General		\$ 10,000									
Comm Svs	12	Animal Shelter - Phase II, Design, Construction	Expand current facilities to meet growth and continue to serve as a regional facility. The existing 9,000 sq. ft. will be renovated and an additional 10,500 sq. ft. will be added, as well as a 2 acre dog park. Conceptual and design in first funding year; construction in second funding year.	General					\$ 750,000	\$ 5,500,000					
Comm Svs	473	Arts District to enhance performing and visual space	Multiple locations to implement the Arts Master Plan- in a district; planning implementation in first funding year; design in second funding year; construction in third funding year	General											\$ 100,000
Comm Svs	471	Athletic Field Synthetic Turf	10-year plan to replace all natural turf at all city fields with synthetic turf.	General					\$ 2,000,000	\$ 2,000,000	\$ 2,000,000	\$ 2,000,000	\$ 2,000,000	\$ 2,000,000	
Comm Svs	411	Blanco River Village City Park	City park for neighborhood on dedicated land. Property for park split by non-owned land. CIP /Dev. working on acquisition of land. Bigalow (developer) has committed \$100K for clubhouse. Property is currently land locked.	General							\$ 200,000				
Comm Svs	472	Capes Camp Improvement and Dam	Determine removal or renovation of Capes Dam, Thompson's Island which was acquired by the City through parkland dedication; first funding year will provide minor improvements; second funding year will provide analysis and permit; third funding year will provide long-term improvements. High Priority	General	\$ 250,000	\$ 1,000,000									
Comm Svs	475	City Facilities HVAC Replacement	HVAC Replacement for City Facilities at multiple locations.	General					\$ 150,000						
Comm Svs	169	City Facility Parking Lots	Repave/restripe all City parking facilities based upon condition, with drainage improvements as needed. Will coordinate with project #358 - Downtown SmartCode. Funding for this project is split over 3 years; 2017, 2020 and 2021.	General		\$ 300,000			\$ 150,000	\$ 150,000					
Comm Svs	526	City Facility Renovations	Renovate / construct -- Activity Cntr (Restrooms, flooring); Municipal Bldg. (first floor training room, restrooms, hallways, lobby and utility billing); Public Services Bldg. (restrooms, hallways, flooring, ceilings); City Hall (foundation repair, restrooms); Grant Harris (foundation repair, restrooms, lobby, offices); Library (Restrooms, painting, flooring); Dunbar (Restrooms, flooring); Fleet Maintenance Bay; Roofs -- (Municipal Bldg., Public Services, Grant Harris, SMEU, Library gutter system, Metal roof)	General	\$ 200,000	\$ 200,000	\$ 150,000	\$ 150,000							
Comm Svs	610	City Facility Renovations - Police	Current PD 911 Center. Internal Renovations. Reconfigure the call center area to allow for separation of call takers and radio operators. Renovate existing wiring to eliminate massive amount of unused wiring under current sub-floor. - \$1M Update funding amount based on CAPCOG participation funding. Constructed in 1973, the Police Facility HVAC system consists of a chilled water system, supplying chilled or heated water to 6 different air handlers throughout the building. Because of the system age, it runs continuously, resulting in extreme utility costs as well as numerous expensive repairs. This project will replace the HVAC system, primarily the air handlers, with new, more efficient digitally controlled units that can be shut down at non-use or non-peak times. - \$750k Rebuild Police Department parking & driver training facilities, with drainage improvements as needed. Also includes replacement of perimeter fencing. \$650k Remaining funds are for misc. renovations to the PD facility including interrogation area, ADA storefront, kitchen rehab, office expansion and restructure.	General		\$ 5,000,000									

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Comm Svcs	542	City Hall Reconstruction	Public/Private/Partnership (PPP) to renovate City Hall Site. Must occur after Municipal Service Complex Relocation (project 132). Approx. 70,000 sf building	General			\$ 1,500,000	\$ 35,000,000							
Comm Svcs	438	City Park - ADA Playground	Improve City Park to insure ADA compliance.	General					\$ 100,000	\$ 1,000,000					
Comm Svcs	336	Gary Softball Complex Renovation-Existing Fields	Renovate existing softball fields at Gary Complex. Improvements needed to update the 30 year old facility and meet ADA requirements. Design in first funding year; construction in second funding year.	General	\$ 250,000				\$ 2,500,000						
Comm Svcs	106	Library Expansion & Renovation	Increase library size by approximately 29,000 sf & renovate existing portion of building; design in first funding year; construction in second funding year	General				\$ 1,100,000	\$ 13,500,000						
Comm Svcs	590	Nature Center	Provide funding to build a Nature Center for San Marcos in Purgatory Park. This is a project with County participation. First year of funding is for design with construction in 2020 and additional work in park in 2021.	General				\$ 300,000	\$ 2,500,000	\$ 250,000					
Comm Svcs	449	Park Development of Undeveloped Park Land	Approximately 400 acres to be improved over 4 years	General					\$ 500,000	\$ 500,000	\$ 500,000	\$ 500,000			
Comm Svcs	550	Purgatory Creek Preserve Park - Trail Head	This project will provide resources to construct a Trail off of McCarty lane for a Park Ranger Facility, parking area and construction of trails that will connect the La Cima development to Purgatory Creek Preserve park.	General								\$ 1,500,000			
Comm Svcs	168	Regional Tennis Center	Build a 16 court tennis center that will allow the city the ability to host USTA regional, state and national championship tournaments. design in first funding year; construction in second funding year	General						\$ 400,000	\$ 4,000,000				
Comm Svcs	189	San Marcos Youth Baseball Complex	Provide new, up-to-date facility at a location along Highway 80. Design funds in 2025, construction funds split over 3 years starting in 2026.	General										\$ 900,000	\$ 3,000,000
Comm Svcs	589	Senior Citizens Center	Provide funding to build an additional or larger Senior Citizens Center for San Marcos.	General					\$ 300,000	\$ 2,000,000	\$ 250,000				
Comm Svcs	476	Sheltered Bus Stops	In 2012 the City became a Small Urban Transit District. The City contracts with CARTS to provide transit service. CARTS has 160 stops, and only 4 are covered. This project will improve bus stops throughout the city based on the Strategic Plan completed in 2014. The cost will cover improvements to approx. 4 shelters and 3 pads per year.	General	\$ 160,000			\$ 80,000		\$ 80,000	\$ 80,000				
Comm Svcs	221	Swimming Pool Complex - East Side	New swimming pool complex. Location is unknown on east side of the interstate. First funding year is for acquisition and design. Second funding year is for construction.	General						\$ 750,000	\$ 3,000,000				
Eng	616	Cemetery Land Acquisition	The City's cemetery is nearing capacity. Acquisition of adjacent property or the development of an additional cemetery is required. Funding will come from cemetery perpetual fund and general fund.	General		\$ 500,000	\$ 500,000								
Eng	598	Comanche/Hopkins Intersection Improvements	Widen Comanche Street to add a dedicated Left turn lane onto Hopkins St. to improve safety and intersection capacity to alleviate the congestion. Modify the traffic signal to accommodate new geometry. Acquisition will be needed.	General							\$ 50,000		\$ 500,000		
Eng	614	Downtown Property Acquisition	Acquire future site downtown for potential parking areas as recommended in Downtown parking implementation plan. Acquiring a site now will ensure future location.	General		\$ 750,000									
Eng	477	Guadalupe/LBJ Improvements	Improvements on Guadalupe/LBJ for implementation of two-way or other operational improvements including; outreach, additional infrastructure at railroad crossings, traffic counts, analysis of existing and future infrastructure roadway assignments. This funding does not include total reconstruction of the streets, only signals and striping of the roadway.	General	\$ 275,000		\$ 2,000,000								
Eng	595	Hays Street/Hwy 123 Intersection Imps	Widen Hays Street at the intersection of Staples Road to improve safety and intersection capacity. Modify the traffic signal to accommodate new geometry. Re-evaluate the project with the completed Transportation Master Plan.	General					\$ 6,000		\$ 200,000				

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Eng	480	Hopkins Sidewalk Widening CM Allen to Thorpe	Install 10'+ multi-use sidewalk along Hopkins from CM Allen - Thorpe Lane Identified in Sidewalk Analysis performed by Staff. Coordinate with expansion of library. Add crosswalks at City Hall and Riverside Drive. Add second left turn lane on Southbound Charles Austin at Hopkins - 2018 funding of \$100k for this turn lane. Approved funding for this project is \$75,000 in 2016 and \$2.5M in CAMPO funds (which will be funded in 2018 by CAMPO).	General	\$ 2,575,000		\$ 100,000								
Eng	275	IH 35 River Road Underpass PER	PER to reconstruct IH 35 Main lanes at River Road to create vehicular underpass between River Road and Riverside drive	General					\$ 150,000						
Eng	37	MLK/LBJ Intersection Traffic Signal & Crosswalk Improvements	Design and installation of traffic signal and crosswalk improvements. Complete this with the next phase of a downtown project.	General					\$ 342,000						
Eng	157	Purgatory/Willow Creek Trail Corridor	Connect trail in Willow Creek and extend to Purgatory Park at Hunter Road. Design will begin in 2018 with phased construction in 2019 and 2021.	General			\$ 250,000	\$ 1,000,000		\$ 1,000,000					
Eng	415	Reconstruction of Downtown Alleys	Reconstruct the alleys between LBJ and Guadalupe from Hopkins to University, approx. 700 LF	General					\$ 500,000						
Eng	195	SH 21 San Marcos River Bridge Prel Engr Rpt	Preliminary engineering report to construct a four-lane bridge across the San Marcos River to extend SH 21 south as an alternative regional reliever for IH 35.	General					\$ 200,000						
Eng	230	Transportation Master Plan	Update Transportation master plan to maintain a five year interval insuring capital improvements that meet growth.	General						\$ 400,000					\$ 400,000
Eng	583	Transportation Oversize	Provide an annual funding source for the City to participate in transportation projects with new development. This will allow for the construction of facilities beyond the proportional impact of new development.	General	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000
Fire	72	Fire Department Relocation Station #2	Relocation of Fire Station #2 located at the intersection of Academy and Holland to the area of Wonderworld Dr. and Old RR12. Station 2 was opened in 1954 as a combination fire station/residential duplex. It is functionally and structurally inadequate. Relocating the station would improve emergency coverage and decrease response times to that area of the City. The apparatus bay is too small to adequately house modern fire apparatus and the living areas are insufficient. Broaddus and Associates classified this as the 2nd worst City Facility. The new station will be built to accommodate a fire engine or ladder truck, an ambulance, a brush truck, and either a specialized response vehicle (Haz-mat or heavy rescue) or a reserve fire apparatus. Timing of this project will depend on La Cima Development.	General	\$ 600,000		\$ 4,750,000								
Fire	403	Fire Department Replacement Brush Truck #2 (52-319)	Replace Brush Truck #2: Unit # 52-319, 2003 Ford F-550, 4-wheel drive. This apparatus is 11 years old and has 49,391 miles. Station 2	General					\$ 200,000						
Fire	102	Fire Department Replacement Ladder Truck (52-300)	Replacement of Ladder Truck at Station 5	General			\$ 1,320,000								
Fire	62	Fire Department Replacement Engine (52-614)	Replace Fire Engine. 2007 Smeal Pumper at Station 2	General					\$ 785,000						
Fire	601	Fire Department New Station District #8	A new Station in the Highpoint/Trace development which donated land and \$1.5-\$2M towards construction. Timing will be based upon the Highpoint/Trace Development. Design funding in first year and construction funding in second year. The station is necessary to meet response times.	General				\$ 350,000	\$ 3,000,000						
Fire	444	Fire Department New Engine District # 8	New Engine for the new station at Highpoint/Trace Development. Apparatus needs to be in the same year as the construction, will take 1 year to build	General					\$ 750,000						
Fire	494	Fire Department New Station District# 11	Station at Hwy 80 and SH 21, per fire station master plan priority #2. Land and design funds in 2018, construction and engine funds in 2019.	General						\$ 800,000	\$ 4,750,000				
Fire	495	Fire Department New Engine District # 11	New Ladder Truck for Station at Hwy 80 and SH 21. Apparatus needs to be in the same year as the construction, will take 1 year to build.	General						\$ 1,400,000					
Fire	79	Fire Department New Station District #6 Mall	Future Fire Station at the Outlet Mall. Land in 2018, architect design in 2020, construction in 2021.	General						\$ 600,000		\$ 400,000	\$ 5,000,000		
Fire	442	Fire Department New Engine District #6 Mall	Fire Department Engine for new Mall Fire Station - contingent of the Mall Fire Station being constructed. Apparatus needs to be in the same year as the construction, will take 1 year to build	General									\$ 1,100,000		

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Fire	8	Fire Department New Station District #10 (Airport)	Future Fire Station. 50/50 match - joint-use fire station located on airport property along SH 21.	General										\$ 400,000	\$ 5,000,000
Fire	603	Fire Department New Apparatus District #10 (Airport)	New Apparatus either a Truck or an Engine for the Airport Station.	General											\$ 1,100,000
Fire	604	Fire Department New Aircraft Rescue Firefighting Truck District #10 (Airport)	New Aircraft Rescue Firefighting Truck for the Airport Station, can possibly buy a used/refurb one	General											\$ 500,000
Public Safety	490	700 mhz Radio's replacements for Public Safety	Public Safety will need to move to this: other City depts. will be on the 900mhz	General			\$ 1,200,000	\$ 370,000							
Public Safety	30	Coban Digital Mobile Video	The Police Department purchased Coban Digital Video units for marked patrol cars in early 2007. These 78 units now have expired warranties and need to be repaired or replaced. \$8,500 per unit - includes camera and recording unit. Install cost not included in est. cost per unit. \$150k covers 16 units which must be replaced every 5 years due to being out of warranty. Convert to operational after 2017.	General	\$ 500,000	\$ 150,000			\$ 500,000					\$ 500,000	
General Fund Grand Total						\$12,156,783	\$ 20,150,000	\$41,744,000	\$35,943,000	\$22,776,666	\$37,520,000	\$ 5,090,000	\$10,980,000	\$ 3,934,000	\$10,200,000
General Fund Grand Total Without Potential Bond Projects						\$ 5,856,783	\$ 9,900,000	\$ 4,794,000	\$14,793,000	\$ 9,476,666	\$23,220,000	\$ 5,090,000	\$10,980,000	\$ 3,934,000	\$10,200,000
Water	24	Centerpoint Rd 12" Water Main Extension	Install a 12" water main 4,600 LF on Centerpoint Road from the existing 12" water main at the Masters School to the 6" water main just South of Old Bastrop Hwy. Design in 2018, construction in 2020. Project will use impact fees, if available.	Water			\$ 200,000		\$ 1,300,000						
Water	556	Comanche Well Improvements	Drill additional well at the Comanche Tank or Spring Lake site to replace the capacity lost with Spring Lake Well #1. 1000-2000 gpm.	Water									\$ 240,000	\$ 1,200,000	
Water	538	Deerwood Water Improvements	Design replacement of undersized water mains throughout the Deerwood (approx. 16,000 LF) neighborhood. All water on Whitetail Dr, Deerwood Dr, Hunter Ridge and the 3"-4" on Quail Run from Hunter Rd the existing 12". The existing mains are undersized and deteriorating and require weekly maintenance. This area is not in the City limits but is in the water CCN. Design funds in 2019, construction in 2021.	Water				\$ 650,000		\$ 2,500,000					
Water	91	Hopkins Water Line Replacement - LBJ to CM Allen	Replace 8" waterline on Hopkins from LBJ to CM Allen with a 12" waterline and replace a 2" waterline in Alley, approx. 1,300 LF.	Water							\$ 500,000				
Water	581	Hunter Water Main Extension from Harmons Way to H&H Industry Park	Extend the 12" water main 4,300 LF on Hunter Road from Harmons Way to H&H Industry Park as identified in the Alan Plummer Associate, Inc. Study titles "Harmon's Way - Oakridge Supply Line Phase III Recommendation" Dated 04-09-2015 (alternative 3).											\$ 300,000	\$ 1,300,000
Water	3	Hwy 21 12-inch Water Line Extension	Extend the 12" water line in SH 21 approximately 9600lf from the airport entrance to William Pettus Road to accommodate future planned development along SH-21, including expansion of businesses at the airport	Water					\$ 500,000	\$ 3,000,000					
Water	98	IH 35 Water McCarty-Centerpoint	Construct water line along west side of I35 from N of McCarty to Centerpoint 6,000 feet	Water										\$ 300,000	\$ 1,500,000
Water	99	IH 35 Waterline Crossings	Replace existing, deteriorated IH35 waterline crossings with new 16" crossings. Some crossings are currently closed because they have broken; other crossings are undersized. 6 Waterline Crossings total will be split into 2 phases for construction. Phase 1 in 2019 will include Cottonwood Crossing, Woodcrest Crossing and Marshall Crossing. Phase 2 in 2020 will include Riverside Crossing, Long Crossing and County Crossing.	Water		\$ 400,000		\$ 750,000	\$ 1,200,000						
Water	505	IH 35S Water Improvements - Clovis south across creek	Replace existing 12" water line South of Clovis Barker. Replacement of 1400 lf where break occurred and line is closed.	Water		\$ 100,000		\$ 500,000							
Water	506	IH 35S Water Improvements - Hwy 123 to Wonder World	Reconstruct water line along east side of IH 35 from the IH 35 waterline crossing (just north of Parker) to Wonder World, approx. 5300 LF. Design funds in 2018, construction in 2020.	Water			\$ 400,000		\$ 1,500,000						
Water	100	IH 35S Water Wonder World to Clovis and Civic	Construct water line along east side of IH 35 from Clovis Barker to Wonder World, approx. 5500 LF.	Water						\$ 250,000	\$ 1,700,000				
Water	522	Lazy Lane Water Improvements	Replacement of undersized water mains in the Sleepy Hollow (approx. 8,000 LF on Lazy Lane) neighborhood. The existing mains are undersized and deteriorating and require weekly maintenance. This area is not in the City limits but is in the water Design in 2019, Construction in 2020.	Water				\$ 325,000	\$ 1,600,000						

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Water	424	Lime Kiln Water Line Oversize	Oversize line to 12" approx. 4500 ft. from Post Rd to Windemere Rd	Water						\$ 500,000					
Water	117	McCarty Tank Improvements	Improve existing pumps to boost water to the 936 pressure plane.	Water					\$ 100,000	\$ 1,000,000					
Water	508	McKie and Lee Street Water Replacement	Design replacement of undersized water mains on McKie from Craddock to Mariposa and on Lee from McKie towards Guadalupe in advanced of the 2017 Mill and Overlay for this area.	Water	\$ 100,000	\$ 500,000									
Water	612	Midway Tank	Removal of the Midway tank. This tank is no longer in service.	Water		\$ 150,000									
Water	539	Mill Street Reconstruction	Reconstruct Mill St from Uhland to approx. 1,400 ft northeast. Due to rapid growth in the area and increased traffic, the street is failing. Will include water/drainage/new fire hydrants. \$2.2M in multi funds was approved in FY2016, additional \$250k water funds needed in 2017.	Water	\$ 2,200,000	\$ 250,000									
Water	142	Old Bastrop Hwy Water McCarty to Posey	Construct 16" line in Old Bastrop from McCarty to Centerpoint 6,000 lf and a 12" line from Centerpoint to Posey 10,000 lf. Design and acquisition funding in 2019, to include \$1,000,000 of impact fees. Construction in 2021, to include \$4,000,000 of impact fees.	Water				\$ 1,000,000		\$ 4,000,000					
Water	592	Primrose Way Water	Upsize existing 2" water line to 8" water line 820 lf. Construct 1020 lf of new 8" water line and tie to existing 8" water line located at the Southwest corner of the Master's School parcel. Easements needed along Primrose Way from Horace Howard Dr to the end of the Master's School private drive. This is an area within the City's water CCN where the number of connections exceeds TCEQ requirements. This will also provide a loop feed for fire protection.	Water		\$ 70,000	\$ 345,000								
Water	613	Rattler Road Water Line Extension	Complete 12" Water main loop to provide a second feed to Cottonwood subdivision. Rattler road from Old Bastrop to Hwy 123. Currently we serve the subdivision with only one 16" water main along Hwy 123. Not having the ability to feed from a different source can become an issue if we have breaks or shutoffs on Hwy 123. Approx. 2500 lf	Water					\$ 50,000		\$ 500,000				
Water	166	Red Sky Water Improvements	Replace undersized 2" mains with 8" mains on Horace Howard/Dachshund St. Approx. 3,820. LF.	Water			\$ 200,000	\$ 750,000							
Water	206	Soyars Storage Tank #2	Replace the existing Soyars Storage Tank with a 1,000,000 gallon tank approximately 90 feet tall	Water					\$ 150,000	\$ 1,800,000					
Water	208	Stagecoach to McCarty Water Tank	Construct line from Summit Ridge to McCarty Tank. Parallel existing 12" with 16", approx. 5,900 lf	Water					\$ 300,000	\$ 1,900,000					
Water	430	Staples Rd 12" Water Line	Construct 7000 LF of new 12" line in Staples Road from existing 12" water line at Lago Vista to Old Bastrop Road. Within the project limits, we will swap Crystal Clear Water Supply Corporation customers into our CCN. This project is required per the City's agreement with Crystal Clear to swap portions of CCN's and to strengthen the City's water supply.	Water	\$ 500,000		\$ 2,000,000								
Water	231	Trunk Hill Tank	Construct a 500,000 gallon tank north of Quail Run/Sleepy Hollow Neighborhood	Water							\$ 1,790,000				
Water	248	Water Improvements	Minor engineering projects to repair waterlines	Water	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Water	249	Water Main Oversizing	Funds for oversizing water mains in conjunction with development	Water	\$ 150,000	\$ 150,000		\$ 150,000		\$ 150,000		\$ 150,000		\$ 150,000	
Water	250	Water Master Plan	Evaluate water system current and future needs and opportunities based on updated growth patterns	Water						\$ 500,000					
Water	251	Water Pump Station Improvements	Systematic repair, replacement and upgrade of water pump stations	Water	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Water	288	Water Supply - HCPUA	COSM Water Supply. Acquiring future water supplies through participation with Hays Caldwell Public Utility for Carrizo Wilcox aquifer water	Water	\$ 4,305,000		\$ 5,300,000	\$ 24,000,000		\$ 36,500,000					
Water	247	Water System Improvements	On-going effort to replace, repair, and add water valves and hydrants throughout system, and make emergency replacements	Water	\$ 100,000	\$ 100,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Water Fund Grand Total						\$ 8,003,000	\$ 13,845,000	\$ 31,065,000	\$ 9,122,000	\$ 47,530,000	\$ 9,430,000	\$ 600,000	\$ 690,000	\$ 2,400,000	\$ 3,750,000
Impact Fee Eligible Water Projects Total						\$ -	\$ 200,000	\$ 1,000,000	\$ 1,450,000	\$ 6,300,000	\$ 1,790,000	\$ -	\$ -	\$ -	\$ -

FY 2017-2026 10 Year CIP Project List

Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Wastewater	568	Airport 10-inch/12-inch Wastewater Main	This project consists of constructing new 10-inch/12-inch wastewater mains, to convey municipal airport flows into the existing City owned 18-inch wastewater main on Railroad Ave. WWMP#19	Wastewater					\$ 335,000	\$ 1,700,000					
Wastewater	569	Basin SM-04 Sanitary Sewer Evaluation	This project consists of manhole inspections and wastewater line smoke testing in the SM-04 flow monitoring basin. WWMP#20	Wastewater				\$ 25,000							
Wastewater	579	Basin SM-06 Sanitary Sewer Evaluation	This project consists of manhole inspections and wastewater line smoke testing in the SM-06 flow monitoring basin. WWMP#34	Wastewater										\$ 105,000	
Wastewater	570	Basin SM-07 Sanitary Sewer Evaluation	This project consists of manhole inspections and wastewater line smoke testing in the SM-07 flow monitoring basin. WWMP#21	Wastewater					\$ 55,000						
Wastewater	576	Blanco River Village Lift Station (LS #21) and City Softball Fields Lift Station (LS #47) Decommissioning	This project WWMP#31 consists of decommissioning the Blanco River Village Lift Station, located north of the Airport Hwy near Rush Haven and the City Softball Fields Lift Station, located along the Airport Hwy south of Arnold Ave. This project also consists of the construction of 8-inch gravity mains to convey wastewater flow upstream of the decommissioned lift stations to the proposed 24-inch wastewater main (WWMP #12).	Wastewater									\$ 145,000	\$ 720,000	
Wastewater	422	Brown Terrace Wastewater Lift Station 20	Upsize lift station. Development dependent	Wastewater					\$ 50,000						
Wastewater	578	Browne Terrace Lift Station (LS #20) Rehabilitation	This project consists of rehabilitating Lift Station #20 on Browne Terrace, between Viola St. and Burseson St. WWMP#33	Wastewater									\$ 25,000	\$ 125,000	
Wastewater	425	Care Inn Wastewater Lift Station 5	Rehab lift station	Wastewater					\$ 100,000						
Wastewater	59	East IH 35 WW Oversize	Participate in the oversize of wastewater extensions with proposed developments such as Trace and Gas Lamp, east side of IH 35 to Old Bastrop Hwy. Funds will come from Impact Fees.	Wastewater				\$ 400,000							
Wastewater	545	East Purgatory Creek 15-inch/18-inch Wastewater Interceptor	This project consists of replacing the existing 12-inch wastewater main with a 15-inch wastewater interceptor from the corner of Martin Luther Kind Blvd. & Nance St. to the corner of Shady Lane & Centre St. Then replacing the existing 12-inch wastewater main with a 18-inch wastewater interceptor from Shady Lane & Centre St. to the proposed 18-inch wastewater interceptor from WWMP #4 (Victory Gardens Neighborhood) across the railroad tracks at the intersection of Patton and Roosevelt.	Wastewater					\$ 350,000		\$ 1,800,000				
Wastewater	544	East Side Interceptor 36-inch Wastewater	This project consists of either replacing or rehabilitating the existing 36" wastewater interceptor East and parallel to IH-35 from the Luciano Flores vicinity northward to the San Marcos River. WWMP#3. This project will continue the wastewater improvements installed with the Willow Springs Creek Channel/Wastewater Improvement Project and the Reclaimed Water System Expansion Project.	Wastewater	\$ 150,000		\$ 300,000								
Wastewater	547	Ed JL Green Dr. 10-inch Wastewater Main	This project consists of replacing the existing 8-inch wastewater main with a 10-inch wastewater interceptor along Ed JL Green Dr. from Sessom to Alta Vista	Wastewater			\$ 85,000	\$ 415,000							
Wastewater	575	Gary Job Corp Lift Station (LS #46) Decommissioning	This project consists of decommissioning Lift Station #46, located along Railroad Ave. south of Arnold Ave. by constructing a new 18-inch, 21-inch, and 24-inch wastewater main to reroute flow to the Highway 80 Lift Station. WWMP#30	Wastewater									\$ 850,000	\$ 4,010,000	
Wastewater	423	Gary Job Corp LS 46 Rehab	Rehab lift station	Wastewater					\$ 300,000						
Wastewater	564	Hemphill Creek 12-inch, 24-inch, 27-inch Wastewater Main	This project consists of constructing a new 12-inch, 24-inch, 27-inch wastewater mains from State Highway 21 & Harris Hill Rd. along Hemphill Creek to the proposed Highway 80 Lift Station (WWMP #11). This project also consists of tying in the existing 8-inch force main from the Gary Job Corp Lift Station and 6-inch force main from the Blanco River Village Lift Station into the proposed 24-inch wastewater main near the intersection of State Highway 21 and Old Bastrop Highway. WWMP#12	Wastewater									\$ 1,400,000	\$ 7,000,000	
Wastewater	572	Highway 80 Lift Station Expansion	This project consists of expanding the Highway 80 Lift Station firm capacity to 2.50 MGD from 1.25 MGD. WWMP#23	Wastewater											\$ 130,000

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Wastewater	555	Highway 80 Lift Station and 10-Inch Force Main	This project consists of constructing a new lift station with an initial firm pumping capacity of 1.25 MGD and a wet well capacity to handle the 2035 peak flow of 5.0 MGD. The proposed lift station will be located at Highway 80 and CR 102 and includes a 10-inch force main that discharges directly to the existing wastewater treatment facility. WWMP#11 Design in 2019, Construction in 2020.	Wastewater				\$ 1,000,000	\$ 5,000,000						
Wastewater	414	Hwy 21 Wastewater Extension & Lift Station	Replace and improve wastewater lines at the Airport and plan for future growth	Wastewater					\$ 550,000	\$ 2,800,000					
Wastewater	97	IH 35 Ellis to Wonderland WW Improvements	Use trenchless technology to rehabilitate existing wastewater line along IH35 from Ellis to Wonder World. Existing lines on Texas will be rehabilitated as well. Approx. 5000 LF total. 6" Wastewater line along Texas needs to be upsized to 8" lines and the 8" Wastewater line along IH35 needs to be upsized to a 12" line according to the Wastewater Master Plan.	Wastewater			\$ 200,000		\$ 2,000,000						
Wastewater	427	Main Lift Station (LS #1) Expansion	This project consists of replacing the pumps at the Main Lift Station to expand the firm pumping capacity to 23 MGD to meet 2025 peak wastewater flows. Install variable frequency drive for pump 1. WWMP#17	Wastewater				\$ 300,000	\$ 1,700,000						
Wastewater	565	Ladybird & Fenway Wastewater Main Replacement	This project consists of replacing existing 10-inch wastewater main upstream of the N. LBJ Drive Lift Station with a 15-inch/12-inch wastewater main from Craddock St. & Lady Bird Ln. along Fenway Loop to the N. LBJ Drive Lift Station. WWMP#15. Design in 2019 and construction in 2021.	Wastewater				\$ 250,000		\$ 1,200,000					
Wastewater	207	SSES & Repairs	Continue systematic sanitary sewer evaluation study and correction of defects throughout the City. Phase 1: SSES EARZ Project (Veramendi, Nevada and Misc. EARZ lines) funding with 2015 and 2016 funds. (Est - \$1.7m) Phase 2: San Antonio/Armstead/MLK Project (est - \$500,000) and SSES Defect Repairs Project (est - \$460,000) funding with 2017 funds.	Wastewater	\$ 1,828,500	\$ 609,500	\$ 609,500	\$ 625,000	\$ 625,000	\$ 625,000	\$ 625,000	\$ 625,000	\$ 625,000	\$ 625,000	\$ 625,000
Wastewater	573	Upper Blanco River 24-Inch Wastewater Transfer Interceptor	This project consists of installing a 24-Inch wastewater interceptor starting southeast of LS #23 to the proposed 24-inch wastewater interceptor (WWMP #12) at State Highway 21. This project also includes the construction of a 12-inch force main that will convey wastewater flow from LS #23 to the proposed 24-inch wastewater interceptor. WWMP#24	Wastewater							\$ 950,000		\$ 4,700,000		
Wastewater	525	Wallace Addition - Water and Wastewater Improvements	Replace existing aged and sagging wastewater line in alley from Cape to Laredo; approx. 600 ft. Replace existing AC aged water lines in Juarez and Staples; approx. 5,100 ft. Valves are a problem due to them being broken or non-existent at prime locations. Streets Dept. will come in after for mill and overlay. Previously approved funds include \$200k water in 2015, \$1.1M water and \$250k wastewater in 2016. An additional \$75k in wastewater is needed. A portion of this project will be completed with the Reclaimed Water Project - \$375k water funds have been moved for this portion.	Wastewater	\$ 1,550,000	\$ 75,000									
Wastewater	258	Wastewater Collection Improvements	Minor operation projects to repair or replace deteriorating wastewater infrastructure, add cleanouts, install monitoring equipment, etc.	Wastewater	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Wastewater	244	Wastewater Improvements	Minor engineering projects to repair or replace deteriorating wastewater infrastructure, add cleanouts, install monitoring equipment, etc.	Wastewater	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Wastewater	245	Wastewater Lift Station Improvements	Operational systematic upgrade of existing wastewater lift stations.	Wastewater	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000	\$ 150,000
Wastewater	246	Wastewater Master Plan	5 year update; evaluate system needs and identify future projects	Wastewater					\$ 400,000						
Wastewater	546	West Purgatory Creek 18-inch Wastewater Interceptor	This project consists of replacing the existing 12-inch wastewater main with a 18-inch wastewater interceptor from the corner of San Antonio St. & Pitt St. to the existing 24-inch wastewater interceptor at Ellis St. Developer will design the project and we will construct.	Wastewater	\$ 350,000	\$ 1,725,000									
Wastewater	571	WWTP 4.0 MGD AADF Expansion	This project consists of expanding the existing wastewater treatment facility from an annual average daily flow (AADF) of 9.0 MGD to an AADF of 13.0 MGD, while increasing the peak 2-hour capacity from 31.0 MGD to 41.0 MGD. WWMP#22	Wastewater						\$ 3,000,000		\$ 17,500,000			
Wastewater Fund Grand Total						\$ 8,583,283	\$ 6,249,500	\$ 3,665,000	\$14,920,000	\$11,381,666	\$ 9,380,000	\$18,655,000	\$ 3,575,000	\$13,069,000	\$ 1,205,000
Impact Fee Eligible Wastewater Projects Total						\$ -	\$ -	\$ 400,000	\$ -	\$ -	\$ 950,000	\$ -	\$ 4,700,000	\$ -	\$ -

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Drainage	602	Clarewood Drainage	Install drainage infrastructure on Clarewood Drive between Hwy 80 and Bugg Lane to reduce street flooding. Also need to address Wastewater needs in this area.	Drainage					\$ 30,000		\$ 200,000				
Drainage	33	Cottonwood Creek Detention Study	Study feasibility of constructing regional detention structure along/within Cottonwood Creek to improve water quality	Drainage					\$ 150,000						
Drainage	358	Downtown Smartcode Water Quality Plan Implementation	Water quality features downtown from study in 2015. \$500,000 over 5 years starting in 2017. Locations include City Hall Parking Lot, Fish Hatchery/Grant Harris Bldg., Dunbar Park, Hopkins St at San Marcos River - South and Hopkins St at San Marcos River - North. Will coordinate with project 169 - City Parking Lot Imp.	Drainage		\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000					
Drainage	53	Drainage Master Plan Update	Update the City's Drainage Master Plan, including the Airport area to maintain a five year interval insuring capital improvements that meet growth	Drainage	\$ 65,000					\$ 300,000	\$ 600,000				
Drainage	558	Farris Street Drainage	Install drainage infrastructure per RPS study to increase drainage capacity and prevent localized flooding in a residential neighborhood. Site #17 on the Comprehensive Watershed Master Plan Field assessment. Need an H/H analysis to size the roadway and/or SD pipe. 2018 funding is for H/H Analysis. Design will be in 2020 and construction in 2021.	Drainage			\$ 50,000		\$ 50,000	\$ 200,000					
Drainage	254	Girard/Earle Drainage Improvements	Improvements to neighborhood drainage at the intersection of Girard and Earle. Removal of four-sided area inlet and installation of a 48" RCP headwall. Replace existing 48" CMP with 60" RCP. Design funding in 2020 construction funding in 2021.	Drainage					\$ 50,000	\$ 200,000					
Drainage	591	Hilltop Drainage	Install new curb inlet and piping and replace curb and gutter, sidewalk, driveways, and roadway to alleviate flooding of houses on the 1200 block of Hilltop, in the cul-de-sac only.	Drainage			\$ 30,000	\$ 200,000							
Drainage	599	Hughson Drive Drainage Improvements	Install a 24" RCP in Hughson from Old RR12 to the outfall just past Craddock. Install an inlet near 405 Hughson to capture excess flows which are jumping the driveway at this address. See Old RR we Bike & Ped PER by Half for more details.	Drainage						\$ 100,000		\$ 300,000			
Drainage	141	NRCS Reservoir #5 Expansion Prel Engr Rpt	Study the feasibility of expanding regional detention structure on main stem of Purgatory Creek.	Drainage					\$ 125,000						
Drainage	551	River Road Drainage Improvements	Construct drainage improvements on River Road approximately 220 feet South of Aquarena Springs Drive to prevent flooding in the area. May need to purchase an easement for this project.	Drainage		\$ 200,000									
Drainage	177	River Road RR Underpass - Drainage Improvements	Construct drainage improvements at River Road and the railroad underpass to provide street, drainage, & safety improvements to low-water crossing.	Drainage						\$ 200,000	\$ 1,000,000				
Drainage	190	Schulle Creek Culvert Imp	Replace culverts to provide greater service and reduce overtopping and provide area for trash collection before detention pond on east side of LBJ.	Drainage					\$ 150,000	\$ 1,000,000					
Drainage	210	Stormwater System Improvements	Annual program for minor unplanned drainage improvements & repairs to address localized deficiencies and problems in the drainage system.	Drainage	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000	\$ 90,000
Drainage	255	William Pettus Drainage Improvements	Reconstruct William Pettus Rd low-water crossing. 2015 Design funds will include a preliminary engineering report to determine if future road improvements will be included in construction.	Drainage	\$ 250,000	\$ 200,000									
Drainage Fund Grand Total						\$ 2,976,847	\$ 2,445,000	\$ 1,590,000	\$ 2,235,000	\$ 6,670,000	\$10,090,000	\$ 390,000	\$ 1,190,000	\$ 90,000	\$ 90,000
Electric	40	Customer Extensions - New Service	Purchase of transformers to keep up with demand of growth	Electric	\$ 450,597	\$ 458,000	\$ 465,000	\$ 471,179	\$ 478,247	\$ 485,421	\$ 492,702				
Electric	383	Electric Master Plan	Conduct a Master Plan for the City of San Marcos Public Services Electric Utility	Electric		\$ 400,000									
Electric	384	HT-10 Reconductor	Increase capacity, reduce losses, improve voltage levels as recommended by the Electric System Master Plan. This project is concurrent with HT T-1 Transformer Replacement and will replace existing lines and poles.	Electric	\$ 200,000	\$ 250,000									
Electric	385	LP-550 Reconductor	Upgrade existing lines on Post Road between Claremont and Uhland, for increased load capacity as recommended in the Electric System Master Plan.	Electric			\$ 240,000								
Electric	405	Pole Replacement - Elect Power poles	Annual CIP for the replacement of electric poles within the City's electric service area.	Electric	\$ 225,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000	\$ 75,000
Electric	386	SM-170 Reconductor	This project addresses the rehabilitation of San Marcos feeder SM-180. This will include upgrading of the existing 4/0 all-aluminum conductor to 336.4 thousand circular mils (wire gauge) conductor. Located between Bugg Lane and Hwy 80.	Electric	\$ 367,000	\$ 195,000									

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Category	Project ID	Project Name	Project Description	Funding Source	Previously Approved Funding	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026
Electric	615	SMEU Back Up Generator Installation	Replace existing back up generator in the SMEU equipment yard with new, higher capacity generator, automatic transfer switch. The current generator is not adequate to power the entire facility in the event of widespread power outage. The Electric Utility must remain operational during emergency conditions such as the recent floods.	Electric		\$ 95,000									
Electric	212	Street Light Replacement Program	A 5- year program to replace street light and lighting fixture extensions with energy saving LED fixtures to ensure all street lights meet dark sky requirements. The project started in 2013 and will continue through 2017.	Electric	\$ 1,600,000	\$ 400,000									
Electric	388	Upgrade Redwood Substation Transformer RW-T2	Upgrade transformer to accommodate system growth. Project includes circuit switcher and substation screen wall	Electric			\$ 3,075,000								
Electric	389	Upgrade Redwood Substation Transformer RW-T3	Upgrade transformer to accommodate system growth. Project includes circuit switcher. Replace power transformer, circuit switcher, distribution bus and feeder exits.	Electric		\$ 2,500,000									
Electric	232	URD Cable Replacement	Required maintenance and replacement of Underground equipment. The project began in 2014 with \$100,000 and will continue through 2021.	Electric	\$ 200,000	\$ 200,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000					
Electric Fund Grand Total						\$ 7,642,666	\$ 11,560,000	\$ 1,526,179	\$ 1,133,247	\$ 3,907,087	\$ 4,347,702	\$ 155,000	\$ 2,155,000	\$ 109,000	\$ 75,000